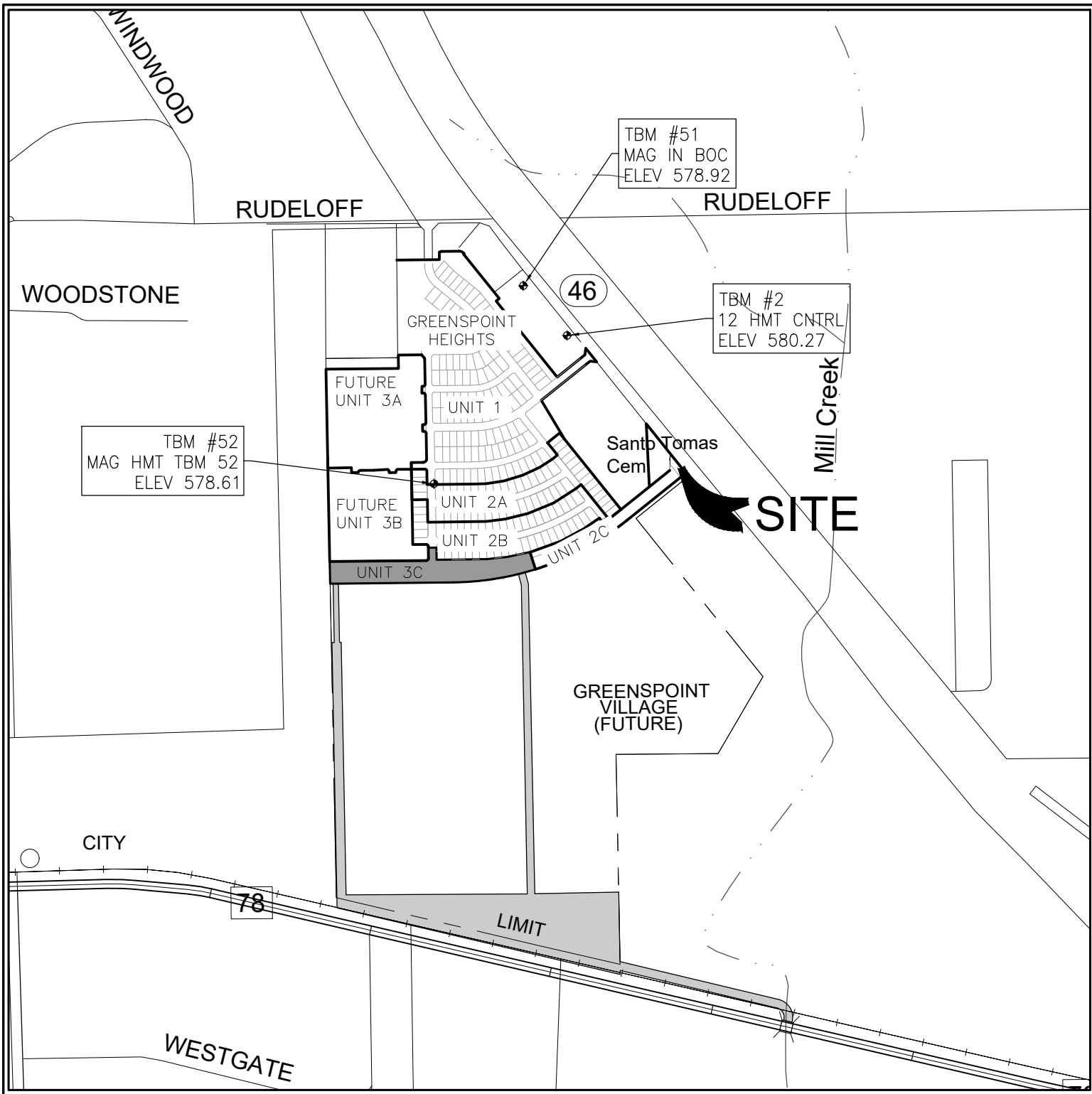


Drawing Name: N:\\_Projects\032 - Fred Heimer\032.067 - Greenspoint Heights Unit 3C (Hecan Parkway)\CDA\0001\032.067\_T0001\_COVER.dwg User: joshuak Sep 20, 2024 - 9:48am



PROJECT LOCATION MAP

SCALE: N.T.S.

PROJECT BENCHMARK

SITE TBM #1  
SET MAG IN BOC  
N: 13768910.77  
E: 2282810.97  
ELEV: 580.27

SITE TBM #2  
SET MAG IN BOC  
N: 13769281.41  
E: 2282484.62  
ELEV: 578.92

GENERAL NOTES:

- ALL RESPONSIBILITY FOR THE ADEQUACY OF THESE PLANS REMAINS WITH THE ENGINEER OF RECORD. IN ACCEPTING THESE PLANS, THE CITY OF SEGUIN MUST RELY UPON THE ADEQUACY OF THE WORK OF THE ENGINEER OF RECORD.
- IF CONSTRUCTION HAS NOT COMMENCED WITHIN TWO-YEAR OF CITY APPROVAL FOR CONSTRUCTION, THAT APPROVAL IS NO LONGER VALID.
- THE MOST CURRENT EDITIONS OF THE CITY OF SAN ANTONIO STANDARD SPECIFICATIONS AND THE TEXAS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION OF HIGHWAYS, STREETS AND BRIDGES SHALL FOLLOWED FOR ALL CONSTRUCTION EXCEPT AS AMENDED BY THE CITY OF SEGUIN STANDARD DETAILS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL CONTACT THE CITY SEGUIN TO SET A PRE-CONSTRUCTION MEETING.
- A 48-HOUR ADVANCED NOTIFICATION IS REQUIRED FOR ALL INSPECTION REQUESTS.  
4.1 ALL INSPECTIONS ARE TO BE CALLED IN AT 830-401-2414 OR,  
4.2 FAXED IN AT 830-386-2513
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO SEE THAT ALL TEMPORARY AND PERMANENT TRAFFIC CONTROL DEVICES ARE PROPERLY INSTALLED AND MAINTAINED IN ACCORDANCE WITH THE PLANS AND LATEST EDITION OF THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. IF THE NEED ARISES, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES MAY BE ORDERED BY THE ENGINEERING REPRESENTATIVE AT THE CONTRACTOR'S EXPENSE.
- DRAINAGE IMPROVEMENTS SUFFICIENT TO MITIGATE OFFSITE IMPACT OF CONSTRUCTION MUST BE COMPLETED AND IN PLACE PRIOR TO INCREASING RUNOFF FROM THE SITE.
- NO PORTION OF THE SUBDIVISION IS LOCATED WITHIN ANY SPECIAL FLOOD HAZARD AREA (100 YR. FLOOD), AS DEFINED BY THE GUADALUPE COUNTY, TEXAS, FIRM PANEL NUMBER 48187C0260F EFFECTIVE DATE 11/02/2007, AS PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY.
- THIS PROJECT IS NOT LOCATED WITHIN THE EDWARDS AQUIFER RECHARGE ZONE.
- GAS UTILITIES ARE NOT INCLUDED IN THE CIVIL CONSTRUCTION PLANS. FINAL GAS UTILITY DESIGN SHALL BE APPROVED BY THE CITY FOR WORK WITHIN PUBLIC RIGHT-OF-WAY.

# TxDOT SIGNAL DESIGN

## GREENSPPOINT HEIGHTS UNIT 3C

### SEGUIN, TEXAS

### CIVIL SITE CONSTRUCTION PLANS

BUTTE LAND PARTNERS  
130 S. SEGUIN AVENUE, STE 109  
NEW BRAUNFELS, TEXAS 78130

October 2023

ALL RESPONSIBILITY FOR THE ADEQUACY OF THESE PLANS REMAINS WITH THE ENGINEER OF RECORD. IN ACCEPTING THESE PLANS, THE CITY OF NEW BRAUNFELS MUST RELY UPON THE ADEQUACY OF THE WORK OF THE ENGINEER OF RECORD.

Mallory N. Scates  
P.E. Registration No. 134012

PREPARED BY:



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HMTNB.COM  
P(830)625-8555\*F(830)625-8556  
TBPE FIRM F-10961  
TBPLS FIRM 1053600

NOTE TO CONTRACTOR:

BY THE ACT OF SUBMITTING A BID FOR THIS PROPOSED CONTRACT, THE BIDDER WARRANTS THAT THE BIDDER, AND ALL SUBCONTRACTORS AND MATERIAL SUPPLIERS HE INTENDS TO USE HAVE CAREFULLY AND THOROUGHLY REVIEWED THE DRAWINGS, SPECIFICATIONS AND ALL OTHER CONTRACT DOCUMENTS AND HAVE FOUND THEM COMPLETE AND FREE FROM ANY AMBIGUITIES AND SUFFICIENT FOR THE PURPOSE INTENDED. THE BIDDER FURTHER WARRANTS THAT TO THE BEST OF HIS OR HIS SUBCONTRACTORS' AND MATERIAL SUPPLIERS' KNOWLEDGE, ALL MATERIALS AND PRODUCTS SPECIFIED OR INDICATED HEREIN ARE ACCEPTABLE FOR ALL APPLICABLE CODES AND AUTHORITIES.

THE LOCATION OF ALL EXISTING UTILITIES SHOWN ON THESE PLANS HAS BEEN BASED UPON RECORD INFORMATION ONLY AND MAY NOT MATCH LOCATIONS AND/OR DEPTHS AS CONSTRUCTED. THE CONTRACTOR SHALL CONTACT EACH OF THE INDIVIDUAL UTILITIES FOR ASSISTANCE IN DETERMINING EXISTING UTILITY LOCATIONS AND DEPTHS PRIOR TO BEGINNING ANY CONSTRUCTION. CONTRACTOR SHALL FIELD VERIFY LOCATIONS OF ALL UTILITY CROSSINGS PRIOR TO BEGINNING ANY CONSTRUCTION.

GREENSPPOINT HEIGHTS UNIT 3C  
CIVIL SITE CONSTRUCTION PLANS

HMT # 032.067







ION GENERAL NOTES

AND CONSTRUCTION WILL PROVIDE FOR PRESERVING ALL EXISTING FEATURES IN OR NEAR OF WAY BEING AFFECTED BY THE WIDENING. THIS INCLUDES BUT IS NOT LIMITED TO, Y, GATE SET-BACK, RELOCATION OF ELECTRONIC PRIVATE PROPERTY GATES, MAILBOX, BOXES AND SUPPORTS, CATTLE GUARDS, ROADWAY SIGNING, EXISTING RIP-RAP OR OTHER ION CONTROL FEATURES, DIVERSIONARY BERMS, SWALES, DITCHES, AMOUNT AND 7 DRIVEWAY FLARES AND DRIVEWAY CENTERLINE PROFILE, METAL BEAM GUARD FENCE AND ETC. EXISTING DRIVEWAY CULVERTS AND SAFETY END TREATMENTS IF EFFECTED BY G WILL BE RECONSTRUCTED TO PRESERVE EXISTING FRONT SLOPE RATES. THE ITEMS THAT EFFECT EXISTING PRIVATE PROPERTY ACCESS, MAIL DELIVERY, ETC. IS THE 7 THE DEVELOPER. THE WRITTEN CONCURRENCE OF ANY EFFECTED PROPERTY OWNERS FOR FECTING THEIR DRIVEWAYS OR MAILBOX TURNOUTS MUST BE OBTAINED AND PROVIDED TXDOT DRIVEWAY PERMITS BEING ISSUED.

I STATE RIGHT OF WAY, THE DEVELOPER IS RESPONSIBLE FOR COORDINATION OF, OBTAINING O COMPLYING WITH ANY AND ALL STATE AND FEDERAL REGULATORY AGENCIES AND ALL , RULES AND REGULATIONS PERTAINING TO THE REGULATION OF DRAINAGE, PRESERVATION SOURCES, NATURAL RESOURCES AND THE ENVIRONMENT. THE DEVELOPER IS RESPONSIBLE IF THE PROJECT IS IN AN ENVIRONMENTALLY SENSITIVE AREA SUCH AS WITHIN THE NTRIBUTING ZONE OF PROTECTED AQUIFERS, AND ACT IN ACCORDANCE WITH ALL RESOURCE ONS.

C2P OR WPAP ON FILE WITH TCEQ, THE DEVELOPER IS RESPONSIBLE FOR AMENDING TXDOT'S S TCEQ APPROVAL AND PROVIDING TXDOT WITH THE APPROVED AMENDED PERMIT. THE WILL ADDRESS THE RELOCATION OF ANY TXDOT PERMANENT BMP'S INCLUDING VEGETATIVE AT MAY BE IMPACTED BY WORK DONE WITHIN TXDOT ROW. "

OT HAVE A C2P OR WPAP ON FILE WITH TCEQ, ANY PERMANENT BMP'S INCLUDING 3 3 STRIPS, THAT MAY BE REQUIRED IN ORDER TO TREAT ADDITIONAL IMPERVIOUS COVER ROW WILL BE LOCATED IN PRIVATE PROPERTY AND THE DEVELOPER WILL PROVIDE TXDOT 1 TCEQ APPROVAL OF THE ADDITIONAL IMPERVIOUS COVER."

AY NOT OPERATE UNDER RESOURCE AGENCY ENVIRONMENTAL CLEARANCE OF A PREVIOUS T PROJECT, BUT WILL BE REQUIRED TO OBTAIN SEPARATE RESOURCE/ENVIRONMENTAL CE.

EAS OR MATERIAL SOURCE AREAS RESULT FROM THIS PROJECT, THE CONTRACTOR IS LOW THE REQUIREMENTS OF THE TEXAS AGGREGATE QUARRY AND PIT SAFETY ACT. IN QUESTED THAT THESE AREAS NOT BE VISIBLE FROM ANY HIGHWAY ON THE STATE SYSTEM.

LS REMOVED AND NOT REUSED AND DETERMINED TO BE SALVAGEABLE SHALL BE STORED CT LIMITS AT AN APPROVED LOCATION OR DELIVERED UNDAMAGED TO THE STORAGE YARD. ORERLY DISPOSE UNRECOVERABLE MATERIALS IN ACCORDANCE WITH LOCAL, STATE, AND TIONS. DEFACE TRAFFIC SIGNS SO THAT THEY WILL NOT REAPPEAR IN PUBLIC AS SIGNS.

EXISTING WITHIN STATE RIGHT OF WAY ARE THE NATURAL RESOURCES OF THE STATE AND D. IN THE EVENT THAT TREES MUST BE REMOVED, TXDOT WRITTEN PERMISSION WILL BE NCE AND WILL IDENTIFY THE SPECIFIC TREES BY SPECIES, DIAMETER AND LOCATION TO BE DEVELOPER WILL BE FINED FOR ANY UNPERMITTED REMOVAL OF TREES.

T THAT THERE ARE AREAS OF PUBLIC ROW DEDICATION RESULTING FROM THE PLATTING EA WITHIN THE PUBLIC ROW DEDICATION DOES NOT PASS INTO TXDOT OWNERSHIP AS A ING. HOWEVER, THE DEVELOPER WILL REMOVE ANY OLD FENCING, GATES AND UNSIGHTLY N THE AREA OF THE ROW DEDICATION, LEAVING IT IN AN AESTHETICALLY PLEASING AREA OF ROW DEDICATION WILL NOT BE MOWED OR OTHERWISE MAINTAINED BY TXDOT. I OF TREES IN THE AREA OF ROW DEDICATION, THE TREES WILL FIRST BE EVALUATED IN 4 THE REQUIREMENTS OF LOCAL TREE PROTECTION ORDINANCES AND THE WRITTEN 1 THE LOCAL JURISDICTION WILL BE PROVIDED TO TXDOT.

PER WILL MAINTAIN AT THE PROJECT SITE, AND MAKE AVAILABLE UPON REQUEST, COPIES OF IVIRONMENTAL PLANS AND PERMITS RELATING TO WORK IN STATE RIGHT OF WAY.

GINNING GRADING ACTIVITY THE CONTRACTOR WILL SET AND MAINTAIN ROADWAY STATIONING, MARKS, STAKES TO ESTABLISH LINES, SLOPES, GRADES AND CENTERLINES.

IN STATE RIGHT OF WAY WHICH BECOME STEEPER THAN 3:1 AS A RESULT OF THE WORK WITH 4" THICK REINFORCED CONCRETE RIPRAP AND BE TREATED WITH METAL BEAM GUARD 1 ENTAIL ADDITIONAL RIP-RAP BEYOND THAT SHOWN IN THE PLANS.

ERWISE SHOWN ON THE PLANS, WHERE EXISTING CONCRETE RIP-RAP IS REMOVED, MODIFIED TO REMOVE OR TO REPAIR, SAW-CUT PRIOR TO REMOVAL AND THE NEW FORMED TO MATCH THE EXISTING LINES AND GRADES OF THE EXISTING RIP-RAP AND WILL THE EXISTING RIP-RAP WITH #3 BARS ON 12" CENTERS. THE DOWEL BARS WILL BE : WITH EPOXY MEETING TXDOT REQUIREMENTS. THE MINIMUM EMBEDMENT LENGTH IS 9 PLIES TO ANY TYPE OF CONCRETE RIP-RAP INCLUDING METAL BEAM GUARD FENCE OR OW STRIPS.

ERICHTER (830) 609-0707 NEW BRAUNFELS, TRAVIS YOUNG (830) 303-0130 SEGUIN, CHAD 430 BOERNE, MARK ANDREWS (830) 393-3144 FLORESVILLE, TXDOT MAINTENANCE OFFICE ED BY THE CONTRACTOR 48 HOURS PRIOR TO WORK OCCURRING IN STATE RIGHT OF WAY.

OF WAY WILL NOT BE USED AS AN AREA FOR CONTRACTOR PARKING OR FOR STAGING THE RIALS OR EQUIPMENT.

TROL AND CONSTRUCTION BARRICADES WILL MEET THE REQUIREMENTS OF THE TEXAS MUTCD.

CTOR WILL PROVIDE ADVANCE NOTIFICATION TO THE ENGINEER OF IMPENDING/UPCOMING LANE L TEMPORARY AND/OR PERMANENT LANE, RAMP, CONNECTOR, FRONTAGE, SHOULDER, R, ETC. CLOSURES OR DETOURS.

ADJOINING PROPERTY MUST BE MAINTAINED AT ALL TIMES.

ERWISE NOTED IN THE PLANS AND/OR AS DIRECTED BY THE AREA ENGINEER OR REVISOR, DAILY LANE CLOSURES SHALL BE LIMITED ACCORDING TO THE FOLLOWING

ENANCE SUPERVISOR AND/OR AREA ENGINEER APPROVAL REQUIRED. (WITH UNIFORMED OFF CEMENT OFFICERS).

ES: MAINTENANCE SUPERVISOR AND/OR AREA ENGINEER APPROVAL REQUIRED.

JSURES OR ROADWAY CLOSURES WILL BE PERMITTED FOR THE FOLLOWING KEY DATES EVENTS:

MBER 15 AND JANUARY 1.  
EFORE THANKSGIVING THRU THE SUNDAY AFTER THANKSGIVING.  
D SUNDAY BEFORE MEMORIAL DAY AND LABOR DAY.  
SUNDAY WHEN JULY 4 FALLS ON A FRIDAY OR MONDAY.

WILL THE ROADWAY TRAVEL WAY BE BLOCKED

RES WILL ONLY BE PERMITTED WITH 48 HOUR PRIOR APPROVAL OF THE TXDOT MAINTENANCE E CLOSURES WILL BE PERMITTED ONLY BETWEEN 9:00 A.M. AND 4:00 P.M. MONDAY

:1 (H:V) TEMPORARY SAFETY SLOPE OF STABLE COMPACTED MATERIAL WILL BE REQUIRED : STATE HIGHWAY EDGE OF PAVEMENT AT ALL TIMES DURING NON WORKING HOURS.

DE OF THE ROADWAY WILL BE OPEN TO CONSTRUCTION AT A TIME. WORK WILL BE PAVEMENT EDGES BACKFILLED ON ONE SIDE OF THE ROAD BEFORE WORK WILL BEGIN ON THE E ROADWAY.

PAVING AND SEAL COAT OPERATIONS SHALL PROCEED IN THE DIRECTION OF TRAFFIC.

NT EDGE DROP-OFFS BETWEEN 1 AND 2 INCHES IN HEIGHT WILL HAVE CW 8-11 WARNING MENT EDGE DROP-OFF 2 INCHES OR GREATER WILL HAVE A 3:1 COMPACTED SAFETY SLOPE : CW 8-11 SIGNS PLUS CHANNELIZING DEVICES. PAVEMENT EDGES WILL BE SHOULDERED UP EMBANKMENT MATERIAL AND 4 INCHES OF TOPSOIL AS SOON AS POSSIBLE AFTER PAVING IS E SIDE OF THE ROAD BEING WIDENED.

NG OF SUBGRADE IS REQUIRED AND SHALL BE WITNESSED BY TXDOT PRIOR TO PLACEMENT UCTURE UNLESS OTHERWISE APPROVED BY THE TXDOT MAINTENANCE SUPERVISOR. THE PROOF-ROLLING OF SUBGRADE IS NOT SUPERSEDED BY ANY OTHER REQUIREMENTS OF ANY GEOTECHNICAL REPORT.

E BASE WILL HAVE A MINIMUM PLASTICITY INDEX OF 4.

23. ALL COURSES OF ASPHALTIC CONCRETE PAVEMENT (REGARDLESS OF TYPE) WILL BE PLACED WITH A ASPHALT PAVING EQUIPMENT MEETING THE REQUIREMENTS OF TXDOT ITEM 320, "EQUIPMENT FOR ASPHALT CONCRETE PAVEMENT"; UNLESS OTHERWISE APPROVED BY THE MAINTENANCE SUPERVISOR.

24. ALL SURFACE AGGREGATES WILL MEET THE REQUIREMENTS OF TXDOT FRICTION CLASSIFICATION "B" WILL MEET PG BINDER GRADE 70-22.

25. ALL SURFACE ASPHALT CONCRETE PAVEMENT WILL BE UNDER-SEALED WITH A ONE COURSE SURFACE TREATMENT.

26. ALL ASPHALTIC CONCRETE PAVEMENT USED IN BASE COURSES WILL BE TYPE "A" OR "B" AND WILL MEET PG BINDER GRADE 64-22.

27. ALL PAVEMENT WIDENING INCLUDING SHOULDERS WILL MATCH THE EXISTING PAVEMENT CROSS SLOPE.

28. ALL PAVEMENT MARKINGS WILL BE TYPE I THERMOPLASTIC (100 MIL) WITH UNDER-SEAL MEETING THE REQUIREMENTS OF TXDOT ITEM 666, REFLECTORIZED PAVEMENT MARKINGS. THE CONTRACTOR WILL PLACE GUIDE MARKS IN ACCORDANCE WITH ITEM 666 AND WILL MAKE ARRANGEMENTS FOR TXDOT INSPECTION OF THE PAVEMENT MARKING LAYOUT PRIOR TO PLACEMENT OF STRIPING. EQUIPMENT USED FOR THE PLACEMENT OF STRIPING WILL MEET THE PRODUCTION REQUIREMENTS OF ITEM 666 UNLESS OTHERWISE APPROVED IN ADVANCE BY THE TXDOT MAINTENANCE SUPERVISOR.

29. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH PROPOSED PAVEMENT MARKINGS WILL BE LIGHTLY GROUND IN A MANNER THAT DOES NOT DAMAGE THE PAVEMENT SURFACE, TO REMOVE ANY PAVEMENT MARKING ACCUMULATION, AND WILL BE COVERED WITH A STRIP SEAL OF 18" MINIMUM WIDTH, CONSISTING OF PRECASTED GRADE 5, FRICTION CLASS B AGGREGATE.

30. ALL MATERIALS AND CONSTRUCTION METHODS USED IN STATE RIGHT OF WAY WILL MEET TXDOT SPECIFICATIONS. THIS SUPERSEDES ALL OTHER SPECIFICATIONS IN THE PLANS.

31. ALL TURN LANE CONCRETE PAVEMENT IN STATE ROW WILL MEET THE REQUIREMENTS OF TXDOT ITEM 360 CLASS P CONCRETE AND WILL BE BATCHED AT CONCRETE PLANTS HAVING A CURRENT APPROVED MIX DESIGN. CLASS P CONCRETE SHALL HAVE 7 AND 28 DAY COMPRESSIVE STRENGTH OF 3200 PSI AND 4400 PSI RESPECTIVELY.

32. WHEN WIDENING EXISTING CONCRETE PAVEMENTS, JOINTS IN THE NEW PAVEMENT WILL MATCH JOINTS IN EXISTING PAVEMENT AND CURB.

33. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT TXDOT APPROVED MATERIALS, MIX DESIGNS, APPROVED SOURCE AREAS AND PERSONNEL CAN WALK ON THE CONCRETE WITHOUT DAMAGING THE SURFACE REGARDLESS OF TIME OF DAY OR WEATHER CONDITIONS. STAND-BY POWER DRIVEN CONCRETE SAWS WILL BE PROVIDED DURING THE SAWING OPERATION. CURING COMPOUND WILL BE RE-APPLIED TO THE SAWED JOINT IMMEDIATELY UPON SAWING THE JOINT.

34. SAWING OF CONTRACTION/CONSTRUCTION JOINTS IN CONCRETE PAVEMENT WILL BE ACCOMPLISHED AS PERSONNEL AS PERSONNEL CAN WALK ON THE CONCRETE WITHOUT DAMAGING THE SURFACE REGARDLESS OF TIME OF DAY OR WEATHER CONDITIONS. STAND-BY POWER DRIVEN CONCRETE SAWS WILL BE PROVIDED DURING THE SAWING OPERATION. CURING COMPOUND WILL BE RE-APPLIED TO THE SAWED JOINT IMMEDIATELY UPON SAWING THE JOINT.

35. GUARDRAIL SGT'S WILL BE TYPE 3 UNLESS OTHERWISE APPROVED BY THE TXDOT MAINTENANCE SUPERVISOR. GUARDRAIL MOW STRIP PLACED ADJACENT TO OTHER CONCRETE RIP-RAP WILL BE SEPARATED BY A FORMED CONSTRUCTION JOINT.

36. ANY CONCRETE CURB TO BE REMOVED WILL BE SAW-CUT AT THE LIMITS OF REMOVAL AND BE REMOVED ENTIRELY. SLICING THE TOP PORTION OF THE CURB OFF AND LEAVING REMAINING PORTION OF CURB IN PLACE IS UNACCEPTABLE.

37. ANY DAMAGE TO TXDOT FACILITIES WILL BE REPAIRED AT NO EXPENSE TO THE STATE, TO TXDOT'S SATISFACTION.

38. SIDEWALKS PLACED IN THE HIGHWAY RIGHT-OF-WAY WILL BE A MINIMUM WIDTH OF FIVE FEET OR COMPLY WITH THE MORE STRINGENT WIDTH AS REQUIRED BY CITY ORDINANCE AND WILL MEET ALL OTHER REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT. PEDESTRIAN RAMPS WILL BE PROVIDED AT STREET AND DRIVEWAY INTERSECTIONS AS SHOWN ON THE CURRENT STATE STANDARD FOR PEDESTRIAN FACILITIES. COLOR CONTRAST AND TEXTURING OF PEDESTRIAN RAMPS WILL BE PLACED AT STREET INTERSECTION RAMPS ONLY AS SHOWN ON THE CURRENT STATE STANDARD FOR PEDESTRIAN FACILITIES. PEDESTRIAN RAMPS AT DRIVEWAY INTERSECTIONS WILL NOT RECEIVE ANY COLOR CONTRAST OR TEXTURING. METAL PLATING FOR SIDEWALK BRIDGES WILL MATCH THE TYPICAL WIDTH OF THE APPROACH SIDEWALK. HIS MAY RESULT IN A WIDTH THAT IS GREATER THAN SHOWN IN THE STANDARD DETAILS INCLUDED IN THE PLANS.

39. THE CONTRACTOR WILL USE BEST MANAGEMENT PRACTICES (BMP'S) TO MINIMIZE EROSION AND SEDIMENTATION IN THE STATE RIGHT OF WAY RESULTING FROM THE PROPOSED CONSTRUCTION. RE-VEGETATION OF DISTURBED AREAS WILL BE COMPLETED IN ACCORDANCE WITH TXDOT STANDARD SPECIFICATIONS. PERMANENT VEGETATIVE COVER MUST ACHIEVE 70% COVERAGE PRIOR TO PROJECT ACCEPTANCE. SOIL RETENTION BLANKETS MAY BE REQUIRED TO PREVENT EROSION OF TOPSOIL PRIOR TO VEGETATION RE-ESTABLISHMENT

40. PRIOR TO SEEDING OR RE-VEGETATION THE FRONT SLOPES WILL BE SHOULDERED UP WITH TOPSOIL TO ELIMINATE ANY PAVEMENT EDGE DROP-OFF.

41. MUD TRACKED ONTO THE ROADWAY FROM THE SITE WILL BE IMMEDIATELY REMOVED TO THE SATISFACTION OF TXDOT.

42. IT WILL BE THE DEVELOPER/OWNER'S RESPONSIBILITY TO CLEAN OUT, TO THE STATE'S SATISFACTION, ANY DRAINAGE STRUCTURE OR STORM SEWER SYSTEM THAT BECOMES SILTED AS A RESULT OF THEIR OPERATIONS.

43. THE ADJUSTMENT OF ANY UTILITIES IN STATE RIGHT OF WAY OR ADJACENT PRIVATE EASEMENT WILL BE THE RESPONSIBILITY OF THE DEVELOPER/OWNER'S.

44. THE CONTRACTOR IS RESPONSIBLE FOR PLACING AND MAINTAINING EXISTING SIGNS ON TXDOT APPROVED TEMPORARY MOUNTS UNTIL PERMANENT SIGNS ARE PLACED.

45. THE FINAL PLACEMENT OF PERMANENT SIGNS WILL BE COORDINATED PRIOR TO PLACEMENT WITH THE LOCAL TXDOT MAINTENANCE SUPERVISOR.

46 FOR WORK WITHIN THE STATE RIGHT OF WAY WHERE REMOVAL OF MATERIALS OR DEBRIS WITHIN THE CONSTRUCTION LIMITS AND NOT INCORPORATED IN THE FINISHED ROADWAY SECTION OF RIGHT OF WAY, WILL BE DISPOSED OF IN A MANNER ACCEPTABLE TO THE MAINTENANCE SUPERVISOR AT NO EXPENSE TO THE STATE. MATERIALS THAT ARE NOT DETERMINED TO BE SALVAGEABLE BY THE MAINTENANCE SUPERVISOR BECOME THE PROPERTY OF THE CONTRACTOR FOR PROPER DISPOSAL AT THEIR EXPENSE. MATERIALS DETERMINED TO BE SALVAGEABLE WILL BE RETURNED TO THE STATE AND DELIVERED TO THE LOCATION AS DETERMINED BY THE MAINTENANCE SUPERVISOR.

47. REGARDLESS OF ERRORS AND OMISSIONS IN INFORMATION PROVIDED IN THE PLANS OR CROSS-SECTIONS THE PERMITEE IS RESPONSIBLE FOR PROVIDING FOR POSITIVE DRAINAGE OUTFALLS WITHIN AND OFF THE LIMITS OF THE PROJECT.

48. (FOR WORK IN CITY OF NEW BRAUNFELS) ALL TRAFFIC SIGNALS ON THE STATE HIGHWAY SYSTEM WITHIN THE NEW BRAUNFELS CITY LIMITS, WITH THE EXCEPTION OF SIGNALS ON IH 35, ARE THE RESPONSIBILITY OF THE CITY OF NEW BRAUNFELS AND THE CITY OF NEW BRAUNFELS WILL PERFORM CONSTRUCTION INSPECTION. CONTACT GARRY FORD, P.E. AT (830) 221-4645, 48 HOURS PRIOR TO THE NEED FOR ANY INSPECTIONS. ALSO WHEN NON-TRAFFIC SIGNAL WORK IS BEING PERFORMED WITHIN 400 FEET OF AN EXISTING SIGNALIZED INTERSECTION, FLASHING BEACON OR SCHOOL ZONE FLASHER OR OTHER TYPE OF SIGNAL, IF WITHIN THE CITY OF NEW BRAUNFELS AREA OF RESPONSIBILITY CONTACT GARRY FORD, P.E. TO DETERMINE/VERIFY THE LOCATION OF LOOP DETECTORS, CONDUIT, GROUND-BOXES, ETC. FOR ALL OTHER LOCATIONS, CONTACT TXDOT REPRESENTATIVE, MIKE GARZA, AT (210) 615-6028, E-MAIL IS MIKE.GARZA@TXDOT.GOV. THE CONTRACTOR IS RESPONSIBLE FOR REPAIR OR REPLACEMENT OF ANY SIGNAL EQUIPMENT DAMAGED BY CONSTRUCTION OPERATIONS. THE METHOD OF REPAIR OR REPLACEMENT SHALL BE PRE-APPROVED AND INSPECTED. DEPENDING ON THE TYPE AND EXTENT OF THE DAMAGE, THE ENGINEER RESERVES THE RIGHT TO PERFORM THE REPAIR OR REPLACEMENT WORK AND THE CONTRACTOR WILL BE BILLED FOR THIS WORK. WHEN WORKING NEAR AERIAL ELECTRICAL LINES OR UTILITY POLES, COMPLY WITH FEDERAL, STATE AND LOCAL REGULATIONS.

49. (FOR AREAS OTHER THAN CITY OF NEW BRAUNFELS) WHEN NON-TRAFFIC SIGNAL WORK IS BEING PERFORMED WITHIN 400 FEET OF AN EXISTING SIGNALIZED INTERSECTION, FLASHING BEACON OR SCHOOL ZONE FLASHER OR OTHER TYPE OF SIGNAL, CONTACT TXDOT REPRESENTATIVE, MIKE GARZA, AT (210) 615-6028, E-MAIL IS MIKE.GARZA@TXDOT.GOV. THE CONTRACTOR IS RESPONSIBLE FOR REPAIR OR REPLACEMENT OF ANY SIGNAL EQUIPMENT DAMAGED BY CONSTRUCTION OPERATIONS. THE METHOD OF REPAIR OR REPLACEMENT SHALL BE PRE-APPROVED AND INSPECTED. DEPENDING ON THE TYPE AND EXTENT OF THE DAMAGE, TXDOT RESERVES THE RIGHT TO PERFORM THE REPAIR OR REPLACEMENT WORK AND THE CONTRACTOR WILL BE BILLED FOR THIS WORK. WHEN WORKING NEAR AERIAL ELECTRICAL LINES OR UTILITY POLES, COMPLY WITH FEDERAL, STATE AND LOCAL REGULATIONS.

TRAFFIC CONTROL GENERAL NOTES

1.ALL BARRICADES, SIGNS, AND TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL CONFORM WITH THE LATEST REVISION OF "THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES CTMUTCD", AND MUST BE MAINTAINED AS DIRECTED.

2.INSTALL ALL BARRICADES, SIGNS, AND TRAFFIC CONTROL DEVICES AS SHOWN AND IN ACCORDANCE WITH THE TXDOT STANDARD TOP, BC AND WZ SHEETS, OR AS DIRECTED. ALL SIGNS, BARRICADES, AND SUPPORTS SHALL BE NEW.

3.REFER TO "BC," "TCP," AND "WZ" STANDARD SHEETS FOR PLACEMENT AND SPACING OF TRAFFIC CONTROL DEVICES. LOCATION OF DEVICES MAY BE SHIFTED AS NEEDED TO FIT EXISTING FIELD CONDITIONS, AS APPROVED.

4.THE REQUIRED SIGNAGE SHOWN AT EACH INTERSECTION MUST BE PROPERLY INSTALLED AND APPROVED PRIOR TO BEGINNING WORK AT SUCH INTERSECTION.

5.ADDITIONAL BARRICADES, SIGNS, AND TRAFFIC CONTROL DEVICES OTHER THAN THOSE SHOWN OR SPECIFIED MAY BE REQUIRED FOR THE SAFE MOVEMENT OF TRAFFIC THROUGH THE PROJECT. PAYMENT FOR ALL SUCH ADDITIONAL SIGNS BARRICADES, OR TRAFFIC CONTROL DEVICES WILL BE CONSIDERED SUBSIDIARY TO THE BID ITEM "BARRICADES, SIGNS AND TRAFFIC HANDLING."

6.TRAFFIC MUST PASS THROUGH THE PROJECT IN COMFORT AND SAFETY DURING NON-WORK HOURS, AND NO EQUIPMENT OR MATERIAL WILL BE LEFT IN A POSITION THAT IN THE OPINION OF THE ENGINEER, WILL CONSTITUTE A HAZARD OR ENDANGER TRAFFIC.

7.COVER EXISTING TRAFFIC SIGNS THAT PROVIDE CONFLICTING INFORMATION TO THE DRIVER DURING CONSTRUCTION AS DIRECTED UNTIL SUCH TIME THAT A CONFLICT NO LONGER EXISTS. THIS SHALL BE CONSIDERED SUBSIDIARY TO THE BID ITEM ITEM "BARRICADES, SIGNS AND TRAFFIC HANDLING."

8.THE WORK SITE SHALL BE CONTINUALLY MONITORED TO ENSURE THAT ALL TRAFFIC CONTROL MEASURES ARE OPERATING EFFECTIVELY AND THAT ALL DEVICES USED ARE VISIBLE, IN GOOD CONDITION, AND KEPT CLEAN.

9.THE CONTRACTOR SHALL PROVIDE SAFE ACCESS TO AND FROM ALL DRIVEWAYS AT ALL TIMES AND IN ALL WEATHER CONDITIONS DURING THE CONSTRUCTION OF THIS PROJECT.

10.ANY REQUEST TO ALTER THE SEQUENCE OF WORK OR TRAFFIC CONTROL PLAN MUST BE SUBMITTED TO THE ENGINEER FOR CONSIDERATION OR WRITTEN APPROVAL PRIOR TO IMPLEMENTATION. ANY MAJOR MODIFICATIONS TO THE SEQUENCE OF WORK RECOMMENDED BY THE CONTRACTOR MUST INCLUDE ANY CHANGES TO AFFECTED BID ITEMS, IMPACTS TO TRAFFIC, AND EFFECT ON OVERALL PROJECT COST, SCHEDULE, ETC. IF THIS APPROVAL IS IMPLEMENTED, THE CONTRACTOR WILL BE RESPONSIBLE FOR DEVELOPING DETAILED PLAN SHEETS TO BE SEALED BY A TEXAS LICENSED PROFESSIONAL ENGINEER FOR INCLUSION WITH THE CHANGE ORDER. ANY ADDITIONAL COSTS RELATED TO THE CHANGES IN THE SEQUENCE OF WORK WILL BE BORNE ENTIRELY BY THE CONTRACTOR. THE CONTRACTOR CANNOT PROCEED WITH ANY CONSTRUCTION OPERATIONS BASED ON A REVISED STEP / SEQUENCE UNTIL WRITTEN APPROVAL IS OBTAINED FROM THE ENGINEER. IF AT ANY TIME DURING CONSTRUCTION THE CONTRACTOR'S PROPOSED PLAN OF OPERATION FOR HANDLING TRAFFIC DOES NOT PROVIDE FOR SAFE AND COMFORTABLE MOVEMENT OF TRAFFIC, THE CONTRACTOR WILL IMMEDIATELY CHANGE THEIR OPERATION TO CORRECT THE UNSATISFACTORY CONDITION.

11. ANY TEMPORARY LANE CLOSURES NEEDED WILL BE IN ACCORDANCE WITH THE APPROPRIATE TOP STANDARD SHEETS.

12. INSTALLATION OF THE TRAFFIC SIGNAL UPGRADE SYSTEM SHALL INTERFERE AS LITTLE AS POSSIBLE WITH TRAFFIC. NO TRAFFIC-CARRYING LANES MAY BE CLOSED AFTER THE HOURS OF 7 AM OR BEFORE 6 PM UNLESS OTHERWISE APPROVED. FLAGGERS AND/OR WARNING DEVICES, AS APPROVED, SHALL BE USED FOR BLOCKING TRAFFIC-CARRYING LANES. PEDESTRIAN TRAFFIC MUST BE SAFELY ROUTED AROUND AND THROUGH THE WORK AREA, IN ACCORDANCE WITH THE TMUTCD.

13. PORTABLE ELECTRONIC CHANGEABLE MESSAGE BOARDS SHALL BE PLACED AS DIRECTED PRIOR TO IMPLEMENTING LANE CLOSURES OR CHANGES IN TRAFFIC PATTERNS. NOTIFY THE ENGINEER OF UPCOMING MAJOR TRAFFIC CHANGES AT LEAST 7 DAYS IN ADVANCE OF THE WORK BEING DONE IN ORDER TO PROVIDE ADEQUATE TIME TO NOTIFY THE MEDIA.

14.THE CONTRACTOR MAY BE REQUIRED TO HIRE OFF-DUTY POLICE OFFICERS AND/ OR POLICE CRUISERS TO ASSIST IN TRAFFIC CONTROL DURING CONSTRUCTION OPERATIONS THROUGHOUT THE COURSE OF THIS WORK. PAYMENT FOR THE OFF-DUTY POLICE OFFICERS AND/ OR POLICE CRUISERS SHALL BE IN ACCORDANCE WITH ITEM 9.

15.THE CONTRACTOR SHALL MAINTAIN ADEQUATE OPERATIONS DURING NIGHT TIME HOURS. A BY THE ENGINEER PRIOR TO CONSTRUCTION. WILL NOT BE PAID FOR DIRECTLY BUT WILL ITEMS.

16.THE CONTRACTOR IS REQUIRED TO NOTIFY THE TEXAS 811 CALL CENTER PRIOR TO PERFORMING ANY OPERATIONS INVOLVING DIGGING, BORING, EXCAVATION, OR DRILLING.

SUGGESTED SEQUENCE OF CONSTRUCTION:

TRAFFIC SIGNAL INSTALLATION

1. INSTALL ADVANCE WARNING SIGNS AND PROJECT PERIMETER BARRICADING IN ACCORDANCE WITH THESE PLANS, BC SHEETS, AND TMUTCD..

2. INSTALL SIGNS, BARRICADES AND TRAFFIC CONTROL DEVICES PER STANDARDS.

3. CONSTRUCT SIGNAL INSTALLATION.

4. THE CONTRACTOR SHALL NOTIFY THE TXDOT TRAFFIC SIGNAL SHOP PRIOR TO BEGINNING CONSTRUCTION AT ANY INTERSECTION OR MAKING ANY TYPE OF ADJUSTMENT TO ANY TRAFFIC SIGNAL INTERSECTION OR SYSTEM. THE TXDOT TRAFFIC SIGNAL SECTION IS RESPONSIBLE FOR TRAFFIC SIGNAL OPERATIONS AT ALL DIAMOND INTERSECTIONS AND IS THUS RESPONSIBLE FOR PROGRAMMING OF TRAFFIC SIGNALS AT THESE INTERSECTIONS. THE TXDOT AUSTIN DISTRICT TRAFFIC SIGNAL DEPARTMENT IS RESPONSIBLE FOR THE PROGRAMMING, TIMING, AND COORDINATION FOR TRAFFIC FLOW FOR THE TRAFFIC SIGNALS AT ALL OTHER INTERSECTIONS.

TRAFFIC CONTROL GENERAL NOTES

1.ALL BARRICADES, SIGNS, AND TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL CONFORM WITH THE LATEST REVISION OF "THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES CTMUTCD", AND MUST BE MAINTAINED AS DIRECTED.

2.INSTALL ALL BARRICADES, SIGNS, AND TRAFFIC CONTROL DEVICES AS SHOWN AND IN ACCORDANCE WITH THE TXDOT STANDARD TOP, BC AND WZ SHEETS, OR AS DIRECTED. ALL SIGNS, BARRICADES, AND SUPPORTS SHALL BE NEW.

3.REFER TO "BC," "TCP," AND "WZ" STANDARD SHEETS FOR PLACEMENT AND SPACING OF TRAFFIC CONTROL DEVICES. LOCATION OF DEVICES MAY BE SHIFTED AS NEEDED TO FIT EXISTING FIELD CONDITIONS, AS APPROVED.

4.THE REQUIRED SIGNAGE SHOWN AT EACH INTERSECTION MUST BE PROPERLY INSTALLED AND APPROVED PRIOR TO BEGINNING WORK AT SUCH INTERSECTION.

5.ADDITIONAL BARRICADES, SIGNS, AND TRAFFIC CONTROL DEVICES OTHER THAN THOSE SHOWN OR SPECIFIED MAY BE REQUIRED FOR THE SAFE MOVEMENT OF TRAFFIC THROUGH THE PROJECT. PAYMENT FOR ALL SUCH ADDITIONAL SIGNS BARRICADES, OR TRAFFIC CONTROL DEVICES WILL BE CONSIDERED SUBSIDIARY TO THE BID ITEM "BARRICADES, SIGNS AND TRAFFIC HANDLING."

6.TRAFFIC MUST PASS THROUGH THE PROJECT IN COMFORT AND SAFETY DURING NON-WORK HOURS, AND NO EQUIPMENT OR MATERIAL WILL BE LEFT IN A POSITION THAT IN THE OPINION OF THE ENGINEER, WILL CONSTITUTE A HAZARD OR ENDANGER TRAFFIC.

7.COVER EXISTING TRAFFIC SIGNS THAT PROVIDE CONFLICTING INFORMATION TO THE DRIVER DURING CONSTRUCTION AS DIRECTED UNTIL SUCH TIME THAT A CONFLICT NO LONGER EXISTS. THIS SHALL BE CONSIDERED SUBSIDIARY TO THE BID ITEM ITEM "BARRICADES, SIGNS AND TRAFFIC HANDLING."

8.THE WORK SITE SHALL BE CONTINUALLY MONITORED TO ENSURE THAT ALL TRAFFIC CONTROL MEASURES ARE OPERATING EFFECTIVELY AND THAT ALL DEVICES USED ARE VISIBLE, IN GOOD CONDITION, AND KEPT CLEAN.

9.THE CONTRACTOR SHALL PROVIDE SAFE ACCESS TO AND FROM ALL DRIVEWAYS AT ALL TIMES AND IN ALL WEATHER CONDITIONS DURING THE CONSTRUCTION OF THIS PROJECT.

10.ANY REQUEST TO ALTER THE SEQUENCE OF WORK OR TRAFFIC CONTROL PLAN MUST BE SUBMITTED TO THE ENGINEER FOR CONSIDERATION OR WRITTEN APPROVAL PRIOR TO IMPLEMENTATION. ANY MAJOR MODIFICATIONS TO THE SEQUENCE OF WORK RECOMMENDED BY THE CONTRACTOR MUST INCLUDE ANY CHANGES TO AFFECTED BID ITEMS, IMPACTS TO TRAFFIC, AND EFFECT ON OVERALL PROJECT COST, SCHEDULE, ETC. IF THIS APPROVAL IS IMPLEMENTED, THE CONTRACTOR WILL BE RESPONSIBLE FOR DEVELOPING DETAILED PLAN SHEETS TO BE SEALED BY A TEXAS LICENSED PROFESSIONAL ENGINEER FOR INCLUSION WITH THE CHANGE ORDER. ANY ADDITIONAL COSTS RELATED TO THE CHANGES IN THE SEQUENCE OF WORK WILL BE BORNE ENTIRELY BY THE CONTRACTOR. THE CONTRACTOR CANNOT PROCEED WITH ANY CONSTRUCTION OPERATIONS BASED ON A REVISED STEP / SEQUENCE UNTIL WRITTEN APPROVAL IS OBTAINED FROM THE ENGINEER. IF AT ANY TIME DURING CONSTRUCTION THE CONTRACTOR'S PROPOSED PLAN OF OPERATION FOR HANDLING TRAFFIC DOES NOT PROVIDE FOR SAFE AND COMFORTABLE MOVEMENT OF TRAFFIC, THE CONTRACTOR WILL IMMEDIATELY CHANGE THEIR OPERATION TO CORRECT THE UNSATISFACTORY CONDITION.

11. ANY TEMPORARY LANE CLOSURES NEEDED WILL BE IN ACCORDANCE WITH THE APPROPRIATE TOP STANDARD SHEETS.

12. INSTALLATION OF THE TRAFFIC SIGNAL UPGRADE SYSTEM SHALL INTERFERE AS LITTLE AS POSSIBLE WITH TRAFFIC. NO TRAFFIC-CARRYING LANES MAY BE CLOSED AFTER THE HOURS OF 7 AM OR BEFORE 6 PM UNLESS OTHERWISE APPROVED. FLAGGERS AND/OR WARNING DEVICES, AS APPROVED, SHALL BE USED FOR BLOCKING TRAFFIC-CARRYING LANES. PEDESTRIAN TRAFFIC MUST BE SAFELY ROUTED AROUND AND THROUGH THE WORK AREA, IN ACCORDANCE WITH THE TMUTCD.

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16.THE CONTRACTOR IS REQUIRED TO NOTIFY THE TEXAS 811 CALL CENTER PRIOR TO PERFORMING ANY OPERATIONS INVOLVING DIGGING, BORING, EXCAVATION, OR DRILLING.

290 S. CASTELL AVE., STE. 100  
NEW BRAUNFELS, TX 78130  
TBPE FIRM F-10961  
TBPLS FIRM 1053600



**HMT**  
ENGINEERING & SURVEYING

GENERAL NOTES  
(2 OF 2)  
GREENSPONT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

| REVISION                 | DATE        |  |  |  |  |  |  |  |  |
|--------------------------|-------------|--|--|--|--|--|--|--|--|
| NO.                      | DESCRIPTION |  |  |  |  |  |  |  |  |
| DATE: September 22       |             |  |  |  |  |  |  |  |  |
| DRAWN BY: ZAH            |             |  |  |  |  |  |  |  |  |
| DESIGNED BY: JMP         |             |  |  |  |  |  |  |  |  |
| REVIEWED BY: JMP         |             |  |  |  |  |  |  |  |  |
| HMT PROJECT NO.: 032.067 |             |  |  |  |  |  |  |  |  |
| SHEET                    |             |  |  |  |  |  |  |  |  |
| C0.03                    |             |  |  |  |  |  |  |  |  |


Y OF TRAFFIC SIGNAL ITEMS

|          |                                       |                                      |                                      |  |  |  |  |                           |   |   |   |  |                                |                                       |                                   |                                       |                                   |
|----------|---------------------------------------|--------------------------------------|--------------------------------------|--|--|--|--|---------------------------|---|---|---|--|--------------------------------|---------------------------------------|-----------------------------------|---------------------------------------|-----------------------------------|
| N        | 416                                   | 416                                  | 618                                  | 618  | 618  | 620  | 620  | 620                       | 620                                       | 621   | 624                                       | 680  | 682                            | 682                                   | 682                               | 682                                   | 682                               |
|          | 6004                                  | 6006                                 | 6023                                 | 6070   | 6030                                       | 6007                                       | 6008                                       | 6009                      | 6010                                      | 6005  | 6002                                      | 6002   | 6001                           | 6002                                  | 6003                              | 6004                                  | 6005                              |
|          | DRILL<br>SHAFT<br>(36 IN)             | DRILL<br>SHAFT (48<br>IN)            | CONDT<br>(PVC)(SCH 40)<br>(2")       | CONDT<br>(PVC)(SCH 40)<br>(3")                 | CONDT<br>(PVC)(SCH 40)<br>(3")<br>(BORE)   | ELEC<br>CONDR<br>(NO. 8)<br>BARE           | ELEC<br>CONDR<br>(NO. 8)<br>INSULATED      | ELEC CONDR<br>(NO.6) BARE | ELEC CONDR<br>(NO. 6)<br>INSULSLATED      | TRAY<br>CABLE (4<br>CONDR) (12<br>AWG)      | GROUND<br>BOX TY A<br>(122311)<br>W/APRON | INSTALL<br>HWY TRF SIG<br>(ISOLATED)           | VEH SIG SEC<br>(12") LED (GRN) | VEH SIG SEC<br>(12") LED (GRN<br>ARW) | VEH SIG SEC<br>(12") LED<br>(YEL) | VEH SIG SEC<br>(12") LED<br>(YEL ARW) | VEH SIG SEC<br>(12") LED<br>(RED) |
|          | LF                                    | LF                                   | LF                                   | LF   | LF   | LF   | LF   | LF                        | LF  | EA  | EA  | EA   | EA                             | EA                                    | EA                                | EA                                    | EA                                |
| IT<br>RD | 27                                    | 22                                   | 62                                   | 274  | 1395                                       | 1257                                       | 888  | 50                        | 113                                       | 444   | 6   | 1  | 6                              | 3                                     | 6                                 | 3                                     | 6                                 |
|          | 27                                    | 22                                   | 62                                   | 274  | 1395                                       | 1257                                       | 888  | 50                        | 113                                       | 444   | 6   | 1  | 6                              | 3                                     | 6                                 | 3                                     | 6                                 |
| N        | 682                                   | 682                                  | 682                                  | 684  | 686  | 686  | 686  | 690                       | 6001                                      | 6025  | 6027                                      | 6292   |                                |                                       |                                   |                                       |                                   |
|          | 6006                                  | 6050                                 | 6060                                 | 6033   | 6039                                       | 6051                                       | 6059                                       | 6131                      | 6001                                      | 6002  | 6008                                      | 6003   |                                |                                       |                                   |                                       |                                   |
|          | VEH SIG SEC<br>(12") LED (RED<br>ARW) | BACKPLATE W/<br>REFL BRDR (4<br>SEC) | BACKPLATE W/<br>REFL BRDR (3<br>SEC) | TRF SIG CBL<br>(TY A) (14<br>AWG) (7<br>CONDR) | INS TRF SIG PL<br>AM (S)1 ARM<br>(36') LUM | INS TRF SIG PL<br>AM (S)1 ARM<br>(48') LUM | INS TRF SIG PL<br>AM (S)1 ARM<br>(55') LUM | INSTALL BBU<br>SYSTEM     | PORTABLE<br>CHANGEABLE<br>MESSAGE<br>SIGN | RADAR<br>PRESENCE<br>DETECTOR<br>COMM CABLE | GROUND<br>BOX<br>(PREPARE)                | RVDS<br>(PRESENCE<br>AND ADVANCE<br>DETECTION) |                                |                                       |                                   |                                       |                                   |
|          | EA                                    | EA                                   | EA                                   | LF   | EA   | EA   | EA   | EA                        | DAY                                       | LF  | EA  | EA   |                                |                                       |                                   |                                       |                                   |
| IT<br>RD | 3                                     | 3                                    | 6                                    | 1813   | 1  | 1  | 1  | 1                         | 30  | 1162  | 6   | 5  |                                |                                       |                                   |                                       |                                   |
|          | 3                                     | 3                                    | 6                                    | 1813   | 1  | 1  | 1  | 1                         | 30  | 1162  | 6   | 5  |                                |                                       |                                   |                                       |                                   |

Y OF PAVEMENT MARKING ITEMS

|   |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|---|--|---------------------------------------|--|---------------------------------------|---|---|--|--|---|-------------------------------------|------------------------------------|-------------------------------------|-------------------------------|-------------------------------|----------------------------------|-------------------------------------|-----------------------------------|-----------------------------------|
| N |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   | 678                               |
|   |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     | 6006                              | 6008                              |
|   | REFL<br>PAV MRK<br>TY II (W)<br>4" (BRK)     | REFL PAV<br>MRK TY II<br>(W) 4" (SLD) | REFL PAV MRK<br>TY II (Y) 4"<br>(SLD)      | REFL PAV<br>MRK TY II<br>(W) 8" (SLD) | REFL PAV<br>MRK TY II<br>(Y) 12"<br>(SLD) | REFL PAV<br>MRK TY II<br>(W)2 4"<br>(SLD) | REFL PAV<br>MRK TY II<br>(W) 4"<br>(ARROW) | REFL PAV<br>MRK TY II (W)<br>4" (DBL<br>ARROW) | REFL PAV<br>MRK TY II (W)<br>4" (LNDP<br>ARROW) | REFL PAV<br>MRK TY II<br>(W) (WORD) | ELIM EXT<br>PAV MRK &<br>MRKS (4") | ELIM EXT<br>PAV MRK &<br>MRKS (24") | REFL PAV<br>MRKR TY<br>II-A-A | REFL PAV<br>MRKR TY<br>II-C-R | PAV SURF<br>PREP FOR<br>MRK (4") | PAV SURF<br>PREP<br>FOR MRK<br>(8") | PAV SURF<br>PREP FOR<br>MRK (12") | PAV SURF<br>PREP FOR<br>MRK (24") |
|   | EA   | LF                                    | LF   | LF                                    | LF  | LF  | LF   | EA   | EA  | EA                                  | EA                                 | EA                                  | EA                            | LF                            | EA                               | LF                                  | LF                                | LF                                |
|   |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|   |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|   |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
| N | 644  |                                       | 644  |                                       | 644                                       |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|   | 6001   |                                       | 6068                                       |                                       | 6076                                      |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|   | IN SM RD SN<br>SUP&AM TY10<br>BWG (1) SA (P) |                                       | RELOCATE<br>SM RD SN<br>SUP&AM TY<br>10BWG |                                       | REMOVE SM<br>RD SN<br>SUP&AM              |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|   | EA   |                                       | EA   |                                       | EA  |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|   |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
|   |  |                                       |  |                                       |   |   |  |  |   |                                     |                                    |                                     |                               |                               |                                  |                                     |                                   |                                   |
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290 S. CASTELL AVE., STE. 100  
NEW BRAUNFELS, TX 78130  
TBPE FIRM F-10961  
TBPLS FIRM 1053600

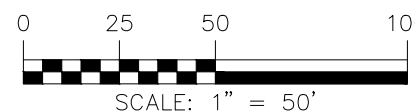
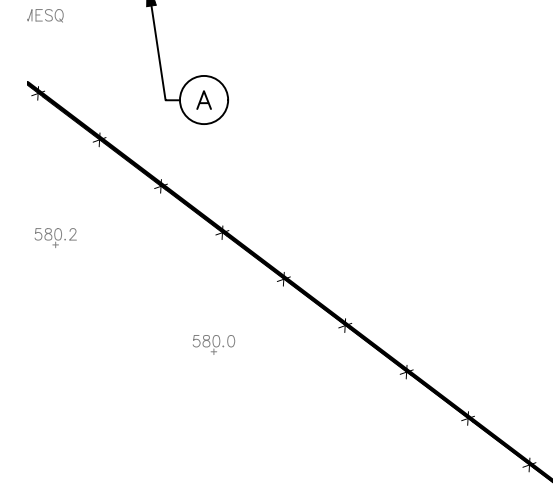
HMT  
ENGINEERING & SURVEYING

QUANTITY  
SUMMARY

GREENSPOINT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

|                          |                      |  |  |  |  |  |  |  |  |  |
|--------------------------|----------------------|--|--|--|--|--|--|--|--|--|
| NO.                      | REVISION DATE        |  |  |  |  |  |  |  |  |  |
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| DATE: September 22       |                      |  |  |  |  |  |  |  |  |  |
| DRAWN BY: ZAH            |                      |  |  |  |  |  |  |  |  |  |
| DESIGNED BY: JMP         |                      |  |  |  |  |  |  |  |  |  |
| REVIEWED BY: JMP         |                      |  |  |  |  |  |  |  |  |  |
| HMT PROJECT NO.: 032.067 |                      |  |  |  |  |  |  |  |  |  |





1. X-SEE BC SHEETS FOR PROPER SPACING AND MORE INFORMATION ON ADVANCE WARNING SIGNS.

3. TYPICAL ADVANCE SIGNAL PROJECT SIGNING FOR MAJOR STREET APPROACH FOR SIGNAL WORK AT INTERSECTION.



**HMT**  
ENGINEERING & SURVEYING

**TRAFFIC CONTROL PLAN**  
**ADVANCE WARNING**  
GREENSPPOINT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

[illegible]

DATE: September 22

DRAWN BY: ZAH

DESIGNED BY: JMP

REVIEWED BY: JMP

HMT PROJECT NO.:  
**032.067**

**SHEET**

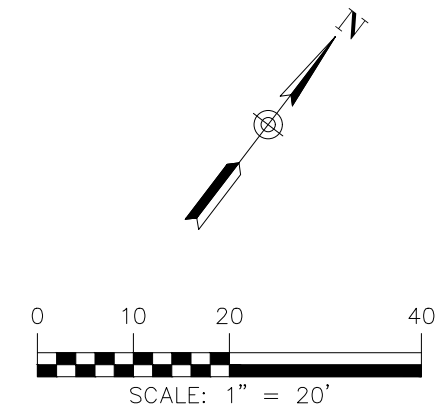
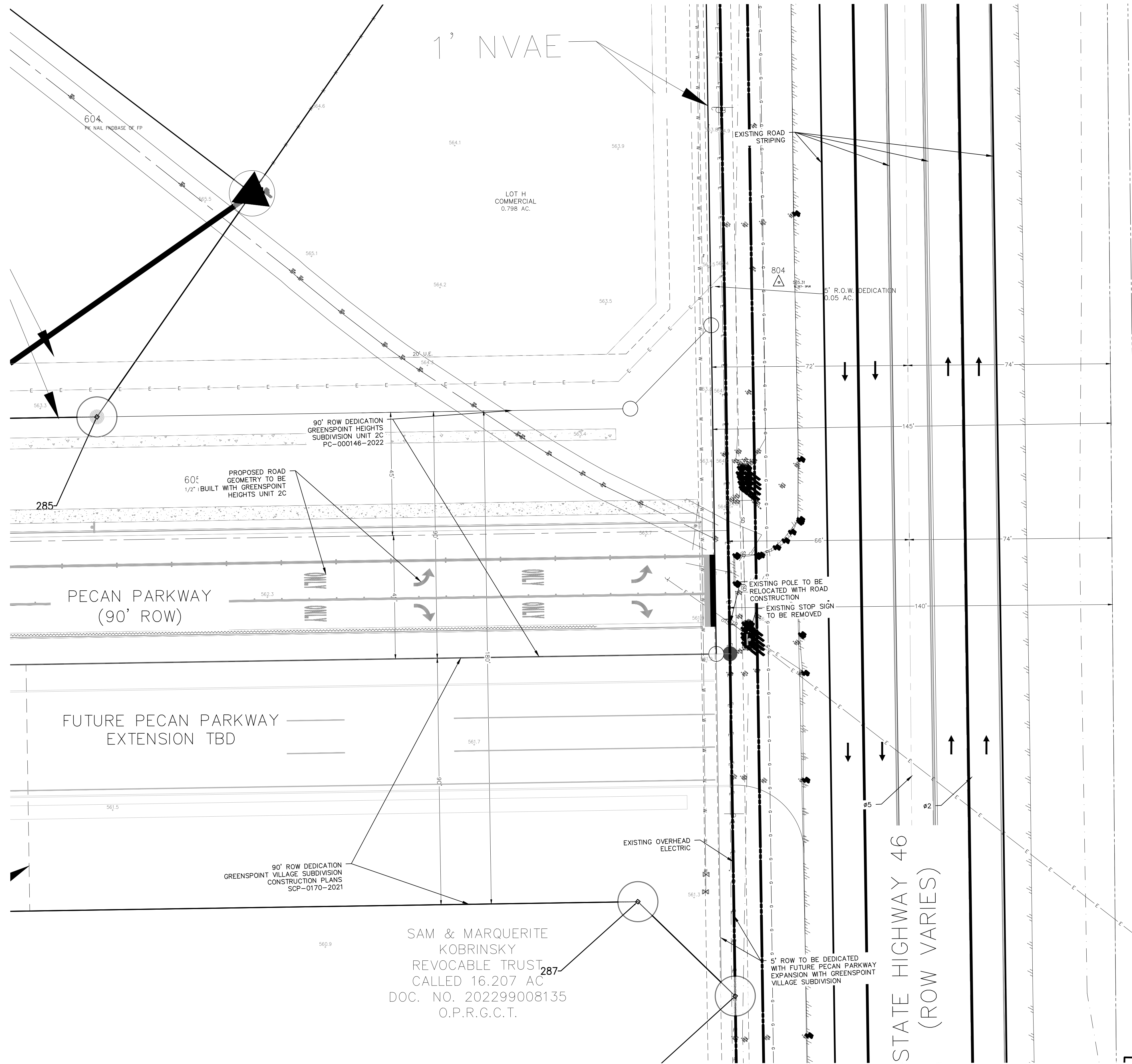
## C1.01

THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR WILL AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MUST BE INCURRED BY THEIR FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES, STRUCTURES OR FACILITIES. CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES 24- HOURS PRIOR TO COMMENCING CONSTRUCTION.





Drawing Name: N:\\_Projects\032 - Fred Heimer\032.067 - Greenspoint Heights Unit 3C (Pecan Parkway)\CDA\032.067\_0001\_LAYOUT.dwg User: jpbhuck Sep 20, 2024 - 9:53am



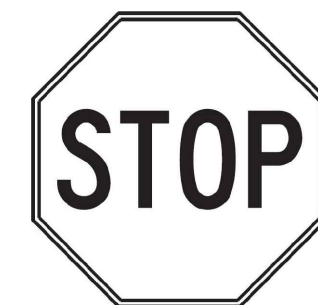
- LEGEND**
- U.E. UTILITY EASEMENT
  - D.E. DRAINAGE EASEMENT
  - EXISTING GROUND BOX
  - EXISTING CONDUIT
  - RIGHT OF WAY
  - GROUND MOUNTED SIGN
  - TRAVEL DIRECTION
  - TIMBER POLE

**NOTES:**

1. LOCATION OF UNDERGROUND AND OVERHEAD UTILITIES ARE APPROXIMATE. R.O.W., ROADWAY, AND UTILITY INFORMATION SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY AND SHALL BE VERIFIED IN THE FIELD.
2. ALL EXISTING SIGNAL APPURTENANCES SHALL BE REMOVED UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR SHALL CALL UTILITY LOCATION SERVICE AT LEAST 48 HOURS PRIOR TO COMMENCING WORK. TEXAS 'ONE-CALL' SYSTEM: 1-800-245-4545.
4. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY DAMAGES CAUSED BY THE FAILURE TO LOCATE AND PRESERVE THE UNDERGROUND FACILITIES.

AWK ENTERPRISES, LLC  
CALLED 23.9540 AC  
DOC #2017001876  
O.P.R.G.C.T.

EXISTING SIGNS



STOP SIGN

THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR WILL AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE INCURRED BY THEIR FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES, STRUCTURES OR FACILITIES. CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES 24- HOURS PRIOR TO COMMENCING CONSTRUCTION.

**EXISTING LAYOUT**

GREENSPPOINT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

290 S. CASTELL AVE., STE. 100  
NEW BRAUNFELS, TX 78130  
TBPE FIRM F-10961  
TBPLS FIRM 1053600

**HMT**  
ENGINEERING & SURVEYING

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| DATE:        | September 22 |
| DRAWN BY:    | ZAH          |
| DESIGNED BY: | JMP          |
| REVIEWED BY: | JMP          |

HMT PROJECT NO.:  
032.067

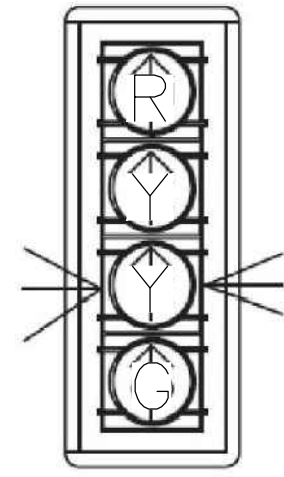
**SHEET**  
**C2.02**



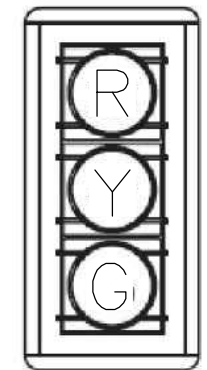
Drawing Name: N:\\_Projects\032 - Fred Heimer\032.067 - Greenspoint Heights Unit 3C - Greenspoint Heights Unit 3C (Pecan Parkway)\CADD\032.067\_0001\_LAYOUT.dwg User: jphuck Sep 20, 2024 - 9:54am

4-SECTION SIGNAL HEAD  
12" HORIZONTAL

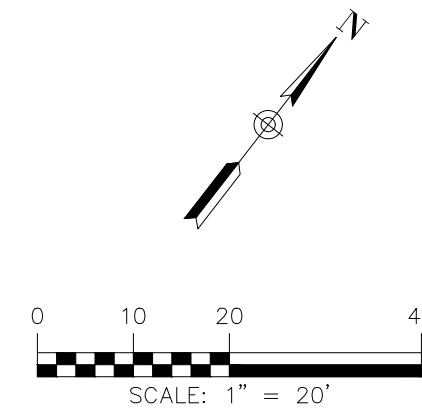
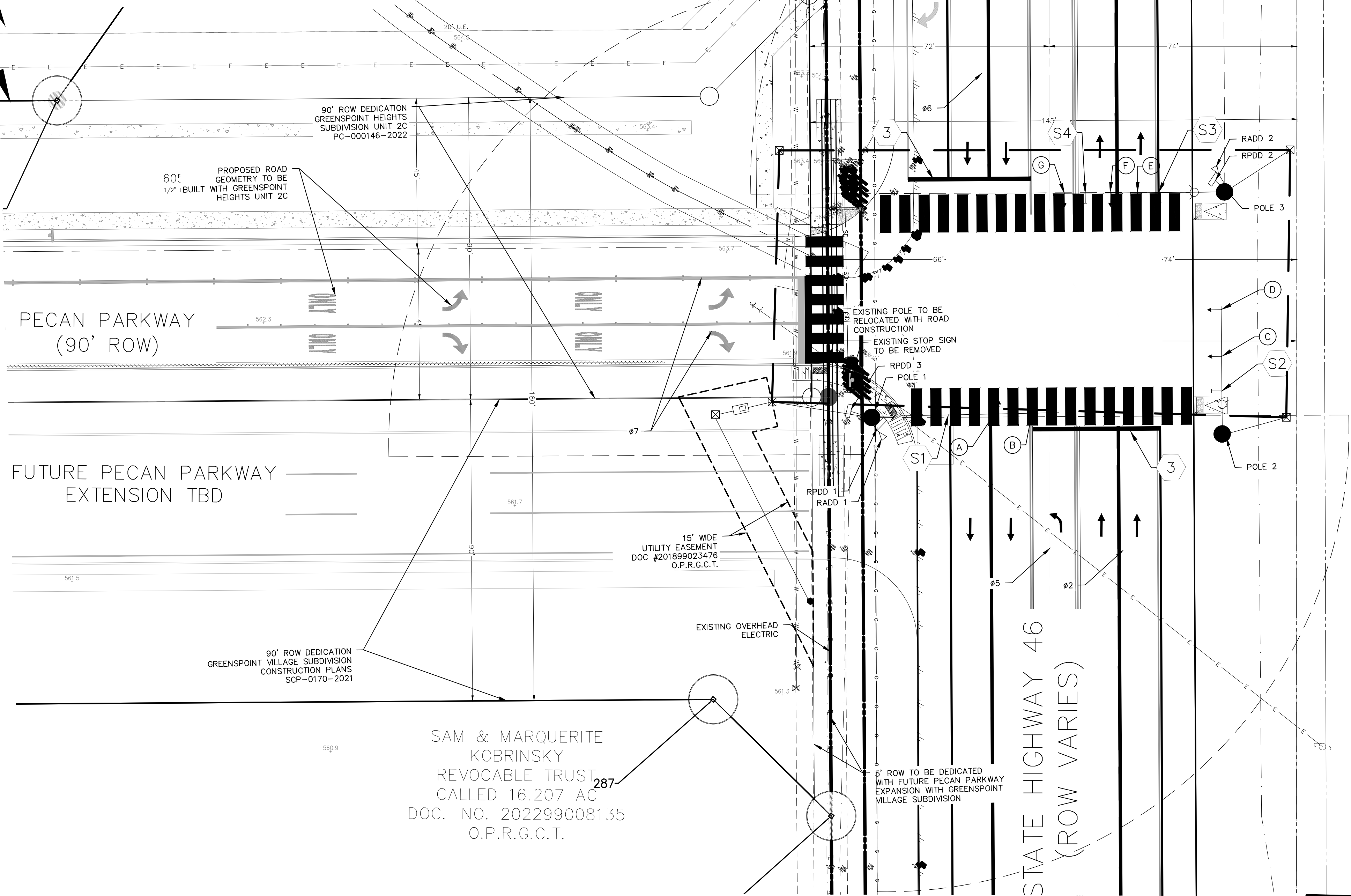
3-SECTION SIGNAL HEAD  
12" HORIZONTAL



G



A B C  
D E F



- LEGEND**
- U.E. UTILITY EASEMENT
  - D.E. DRAINAGE EASEMENT
  - EXISTING GROUND BOX
  - EXISTING CONDUIT
  - PROPOSED CONDUIT
  - POLE MOUNTED CONTROLLER
  - GROUND BOX (TYPE A)
  - ELECTRICAL SERVICE
  - LUMINAIRE
  - SPAN MOUNTED SIGN
  - TRAFFIC SIGNAL HEAD
  - TRAVEL DIRECTION
  - TIMBER POLE
  - RADAR PRESENCE DETECTION DEVICE (RPDD)
  - RADAR ADVANCE DETECTION DEVICE (RADD)
  - RIGHT OF WAY
  - CABINET

NOTES:

1. LOCATION OF UNDERGROUND AND OVERHEAD UTILITIES ARE APPROXIMATE. R.O.W., ROADWAY, AND UTILITY INFORMATION SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY AND SHALL BE VERIFIED IN THE FIELD.
2. ALL EXISTING SIGNAL APPURTENANCES SHALL BE REMOVED UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR SHALL CALL UTILITY LOCATION SERVICE AT LEAST 48 HOURS PRIOR TO COMMENCING WORK. TEXAS 'ONE-CALL' SYSTEM: 1-800-245-4545.
4. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY DAMAGES CAUSED BY THE FAILURE TO LOCATE AND PRESERVE THE UNDERGROUND FACILITIES.
5. SEE SIGN AND STRIPING LAYOUT FOR DETAILS OF SIGNAGE AND STRIPING.

AWK ENTERPRISES, LLC  
CALLED 23.9540 AC  
DOC #2017001876  
O.P.R.G.C.T.

PROPOSED LAYOUT

GREENSPONT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

| NO. | REVISION DESCRIPTION | REVISION DATE |
|-----|----------------------|---------------|
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DATE: September 22  
DRAWN BY: ZAH  
DESIGNED BY: JMP  
REVIEWED BY: JMP

HMT PROJECT NO.:  
032.067

**SHEET**  
**C2.03**



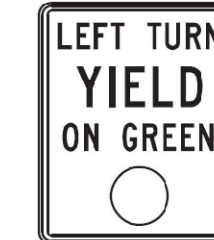
S4

SH 46

S2  
D3-1G  
36" x 12"

PECAN PARKWAY

S1, S3  
D3-1G  
78" x 12"



S4  
R10-12  
30" x 36"

THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR WILL AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE INCURRED BY THEIR FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES, STRUCTURES OR FACILITIES. CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES 24- HOURS PRIOR TO COMMENCING CONSTRUCTION.



Drawing Name: N:\\_Projects\032 - Fred Heimer\032.067 - Greenspoint Heights Unit 3C - (Pecan Parkway)\CDA\1001\032.067\_1001\_LAYOUT.dwg User: jphuck Sep 20, 2024 - 9:54am

CONDUIT AND CONDUCTOR SCHEDULE

| RUN NO. | CONDUIT |     |      |     | NUMBER OF CABLES |             |            |                          |                         |                                |
|---------|---------|-----|------|-----|------------------|-------------|------------|--------------------------|-------------------------|--------------------------------|
|         | TRENCH  |     | BORE |     | LENGTH           | #6 AWG BARE | #6 AWG INS | 7/C #14 AWG (VEH SIGNAL) | 6/C COMM. CABLE (RADAR) | 4/C #12 TRAY CABLE (LUMINAIRE) |
|         | 2"      | 3"  | 2"   | 3"  |                  |             |            |                          |                         |                                |
| RUN 1   |         | 2   |      |     | 65               | 1           | 2          |                          |                         | 2                              |
| RUN 2   |         | 1   |      |     | 10               | 1           | 2          |                          |                         |                                |
| RUN 3   |         | 2   |      |     | 15               |             |            | 4                        | 5                       |                                |
| RUN 4   |         | 2   |      |     | 30               |             |            | 1                        | 3                       |                                |
| RUN 5   |         |     |      | 2   | 150              |             |            | 3                        | 2                       |                                |
| RUN 6   |         | 2   |      |     | 20               |             |            | 1                        |                         | 1                              |
| RUN 7   |         | 2   |      |     | 85               |             |            | 2                        | 2                       |                                |
| RUN 8   |         | 2   |      |     | 30               |             |            | 2                        | 2                       | 1                              |
| RUN 9   |         |     |      | 2   | 155              |             |            |                          |                         |                                |
| RUN 10  |         | 2   |      |     | 75               |             |            |                          |                         |                                |
|         |         |     |      |     |                  |             |            |                          |                         |                                |
| TOTAL   | 0       | 650 | 0    | 610 |                  | 75          | 150        | 790                      | 695                     | 180                            |

CABLE TERMINATION CHART

| CNDR. COLOR | CABLE 1<br>POLE 1 TO CNTRL<br>7 CNDR. | CABLE 2<br>POLE 2 TO CNTRL<br>7 CNDR. | CABLE 3<br>POLE 3 TO CNTRL<br>7 CNDR. | CABLE 4<br>POLE 3 TO CNTRL<br>7 CNDR. |
|-------------|---------------------------------------|---------------------------------------|---------------------------------------|---------------------------------------|
| BLACK       | SH A,B - Y<br>PHASE $\phi$ 6          | SH C,D - Y<br>PHASE $\phi$ 7          | SH E,F - Y<br>PHASE $\phi$ 2          | SH G - Y ARR<br>PHASE $\phi$ 5        |
| WHITE       | SIGNAL COMMON                         | SIGNAL COMMON                         | SIGNAL COMMON                         | SIGNAL COMMON                         |
| RED         | SH A,B - R<br>PHASE $\phi$ 6          | SH C,D - R<br>PHASE $\phi$ 7          | SH E,F - R<br>PHASE $\phi$ 2          | SH G - R ARR<br>PHASE $\phi$ 5        |
| GREEN       | SH A,B - G<br>PHASE $\phi$ 6          | SH C,D - G<br>PHASE $\phi$ 7          | SH E,F - G<br>PHASE $\phi$ 2          | SH G - G ARR<br>PHASE $\phi$ 5        |
| ORANGE      | SPARE                                 | SPARE                                 | SPARE                                 | SH C - Y FLASH<br>ARR<br>OVL D        |
| BLUE        | SPARE                                 | SPARE                                 | SPARE                                 | SPARE                                 |
| WHITE/BLACK | SPARE                                 | SPARE                                 | SPARE                                 | SPARE                                 |

CABLE INSIDE ARMS

| INSIDE ARMS  | 14 AWG<br>7/C |
|--------------|---------------|
| POLE 1 (48') |               |
| HEAD A       | 22            |
| HEAD B       | 34            |
|              |               |
| POLE 2 (36') |               |
| HEAD C       | 24            |
| HEAD D       | 36            |
|              |               |
| POLE 3 (48') |               |
| HEAD E       | 26            |
| HEAD F       | 34            |
| HEAD G       | 48            |
|              |               |
| TOTAL        | 224           |

CABLE INSIDE CONDUITS ON POLES

| INSIDE POLE | 14 AWG<br>7/C | RADAR<br>6/C | TRAY<br>CABLE<br>4/C | #8 AWG<br>(INS) |
|-------------|---------------|--------------|----------------------|-----------------|
|             |               |              |                      |                 |
| POLE 1      | 40            | 60           |                      | 60              |
| POLE 2      | 40            |              | 30                   | 60              |
| POLE 3      | 40            | 40           | 30                   | 60              |
| TOTAL       | 120           | 100          | 60                   | 180             |

CABLE INSIDE CABINET

| INSIDE CABINET | 7/C<br>14 AWG | RADAR<br>6/C | #6 AWG<br>INS | #6 AWG<br>BARE |
|----------------|---------------|--------------|---------------|----------------|
| TOTAL          | 40            | 35           | 10            | 5              |

ELECTRICAL SERVICE DETAILS

| ELECTRICAL SERVICE TYPE<br>(REFER TO ED (5)-14) | SERVICE<br>CONDUIT SIZE | SERVICE CONDUCTORS<br>NO./SIZE | SAEFTY SWITCH<br>AMPS | MAIN CKT. BRK.<br>POLE/AMPS | TWO-POLE<br>CONTRACTOR AMPS | PANELEB./LOADCENTER<br>AMP RATING | BRANCH CIRCUIT ID           | BRANCH CKT. BKR.<br>POLE/AMPS | KVA LOAD |
|---|-------------------------|--------------------------------|-----------------------|-----------------------------|-----------------------------|-----------------------------------|-----------------------------|-------------------------------|----------|
| TY D 120/240 070 (NS) AL<br>(E) SP (O)          | 1 1/4"                  | 3/#6                           | N/A                   | 2P/60                       | 30                          | 100                               | TRAFFIC SIGNAL<br>LUMINAIRE | 1P/30,2P/20                   | <7.1     |

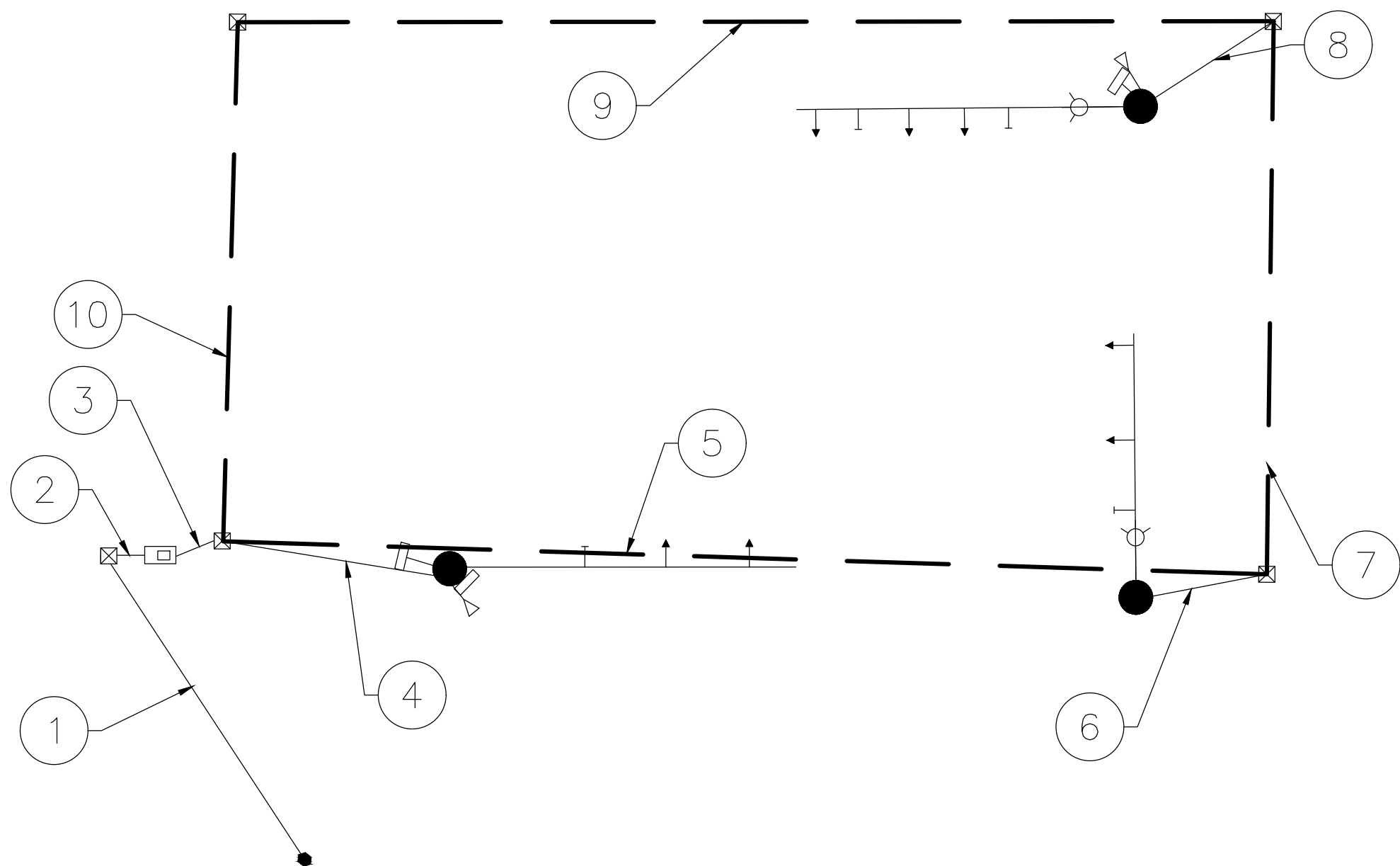
TRAFFIC POLE SCHEDULE

| POLE                | POLE 1                     | POLE 1 (48' ARM)                | POLE 2    | POLE 2 (36' ARM)                | POLE 3           | POLE 3 (48' ARM)                               |
|---------------------|----------------------------|---------------------------------|-----------|---------------------------------|------------------|--|
| MOUNTING HEIGHT     | 30'                        | 19'                             | 30'       | 19'                             | 30'              | 19'  |
| EQUIPEMENT ATTACHED | RPDD 1<br>RPDD 3<br>RADD 1 | 2 3-SECTION<br>VEH SIGNAL HEADS | LUMINAIRE | 2 3-SECTION<br>VEH SIGNAL HEADS | RPDD 2<br>RADD 2 | 2 3-SECTION;<br>14-SECTION<br>VEH SIGNAL HEADS |

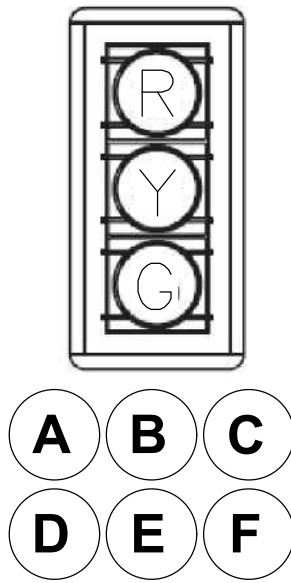
RADAR DETECTOR SCHEDULE

| DETECTOR ID | APPROACH   | TYPE     | PHASE               | MOUNTNG<br>LOCATION |
|-------------|------------|----------|---------------------|---------------------|
| RPDD 1      | NORTHBOUND | PRESENCE | $\phi$ 2 + $\phi$ 5 | POLE 1              |
| RPDD 2      | SOUTHBOUND | PRESENCE | $\phi$ 6            | POLE 3              |
| RPDD 3      | EASTBOUND  | PRESENCE | $\phi$ 7            | POLE 1              |
| RADD 1      | NORTHBOUND | ADVANCE  | $\phi$ 2            | POLE 1              |
| RADD 2      | SOUTHBOUND | ADVANCE  | $\phi$ 6            | POLE 3              |

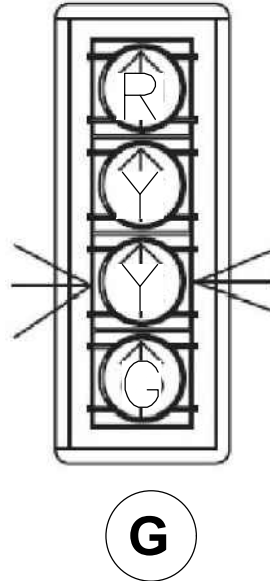
CONDUIT/SPAN RUN DIAGRAM



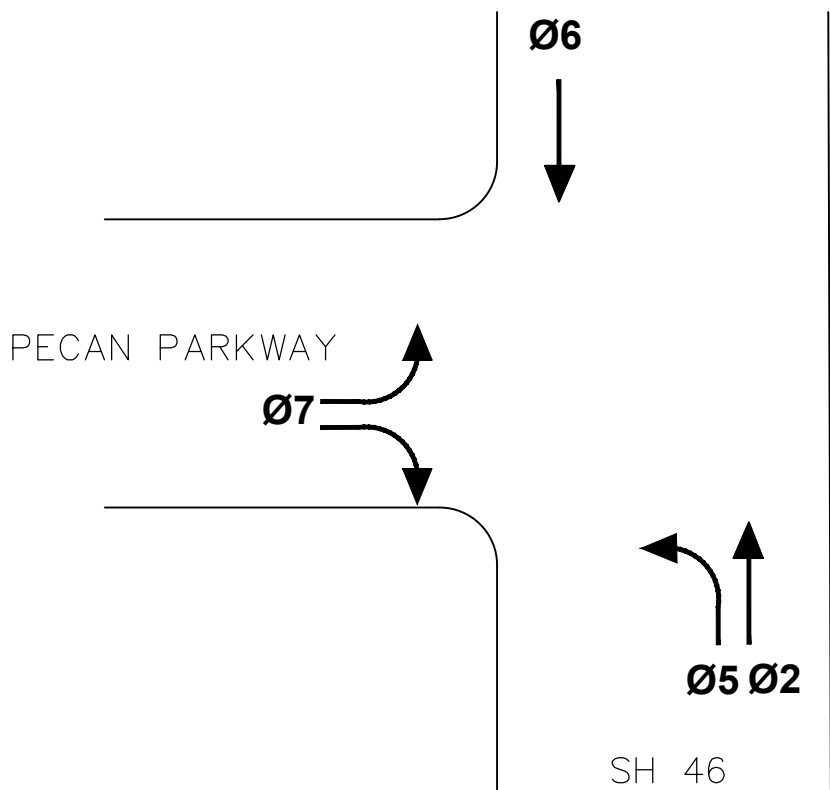
3-SECTION SIGNAL HEAD  
12" HORIZONTAL



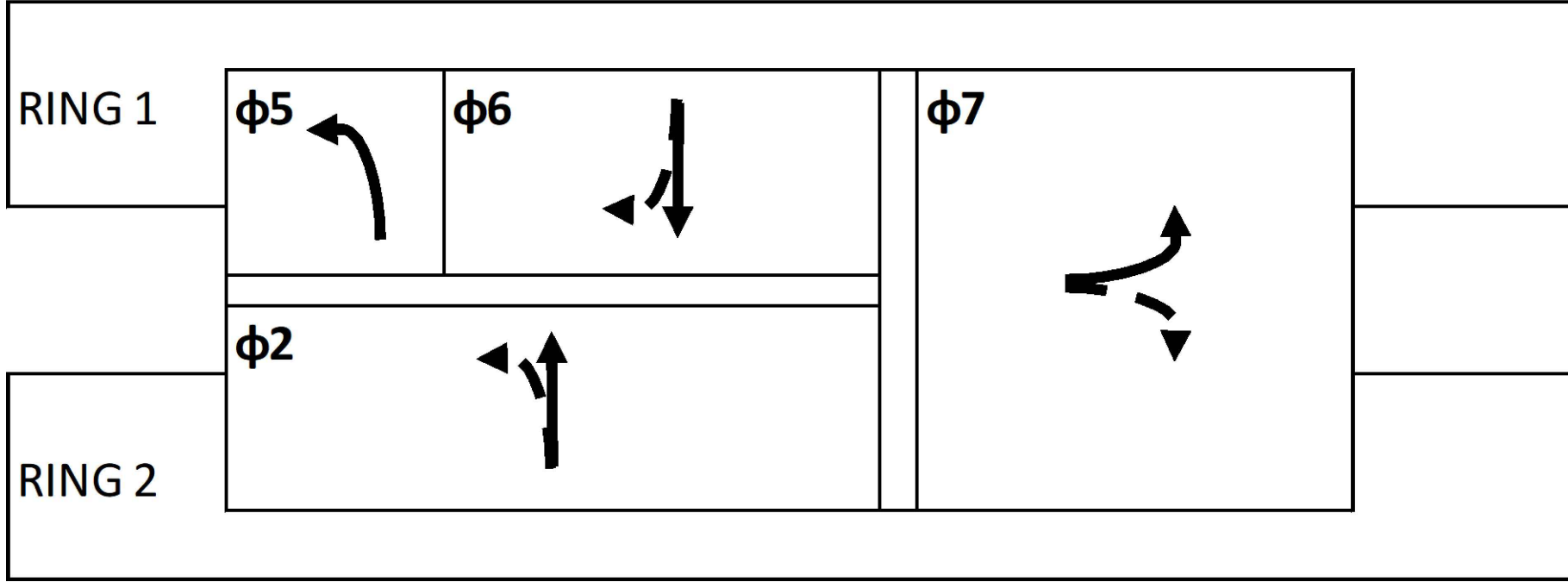
4-SECTION SIGNAL HEAD  
12" HORIZONTAL



INTERSECTION DIAGRAM



PHASE DIAGRAM



290 S. CASTELL AVE., STE. 100  
NEW BRAUNFELS, TX 78130  
TBPE FIRM F-10961  
TBPLS FIRM 1053600



SIGNAL DETAILS

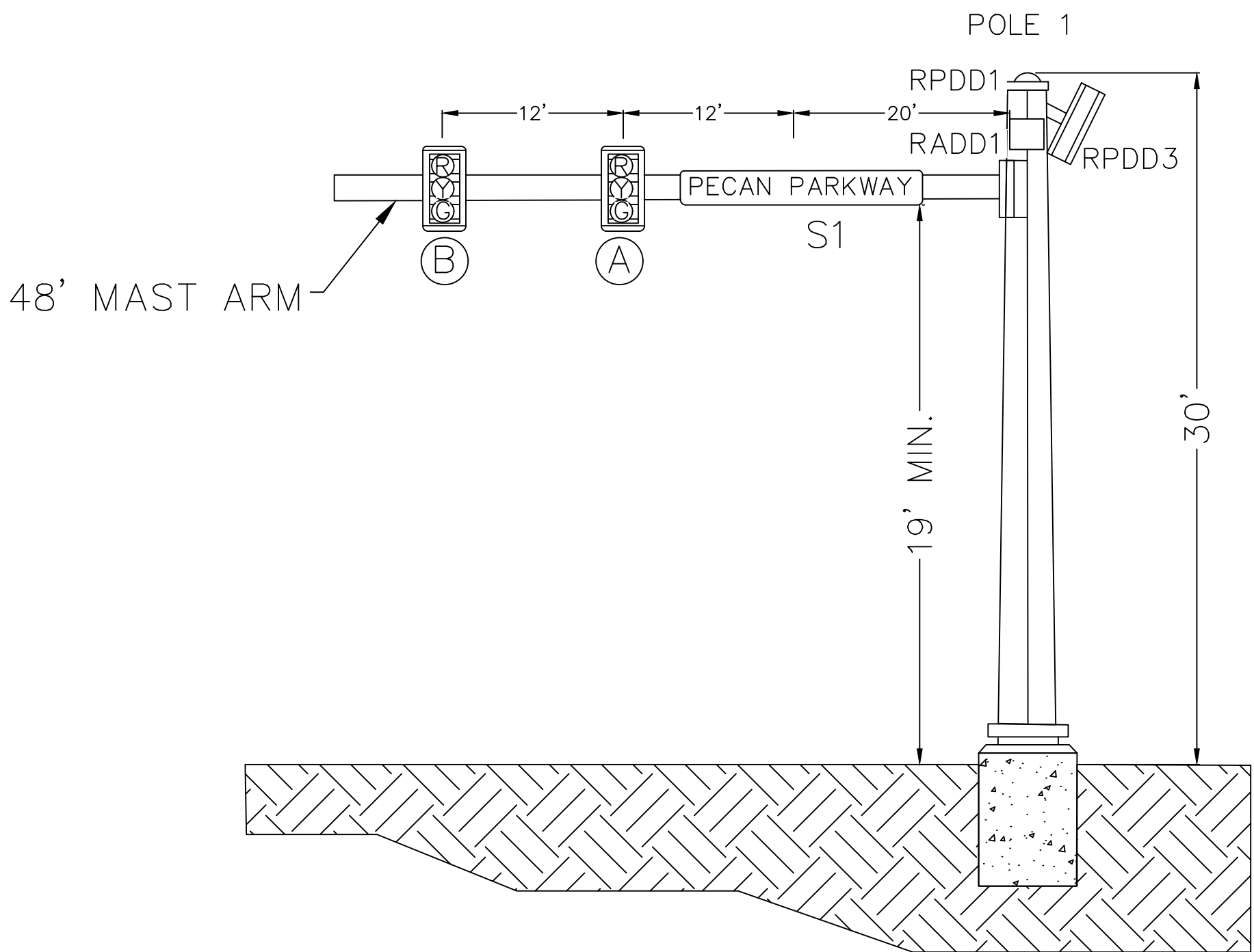
GREENSPONT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

| NO. | REVISION | DESCRIPTION | REVISION DATE |
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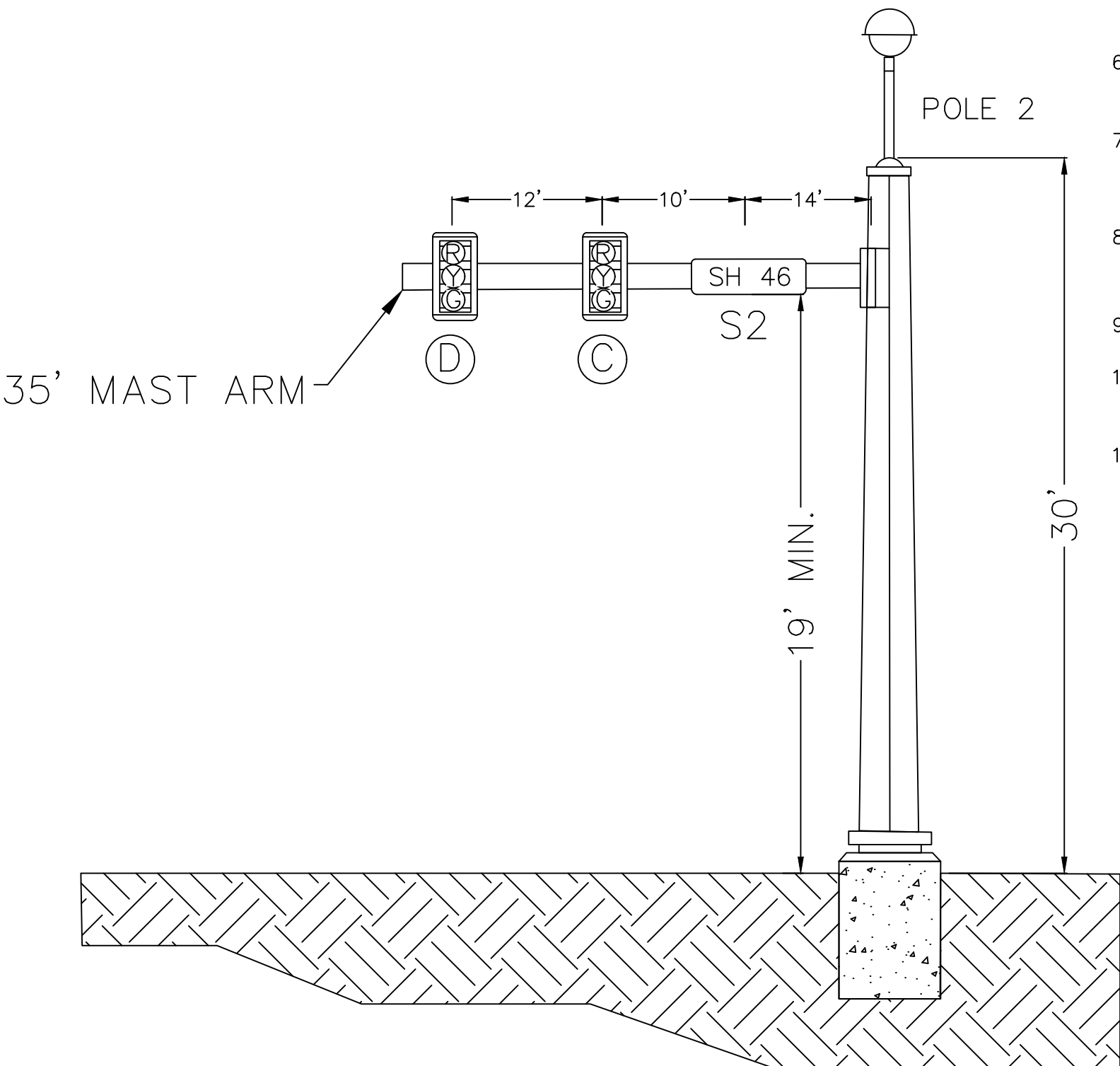
DATE: September 22  
DRAWN BY: ZAH  
DESIGNED BY: JMP  
REVIEWED BY: JMP  
HMT PROJECT NO.: 032.067

SHEET  
C2.04

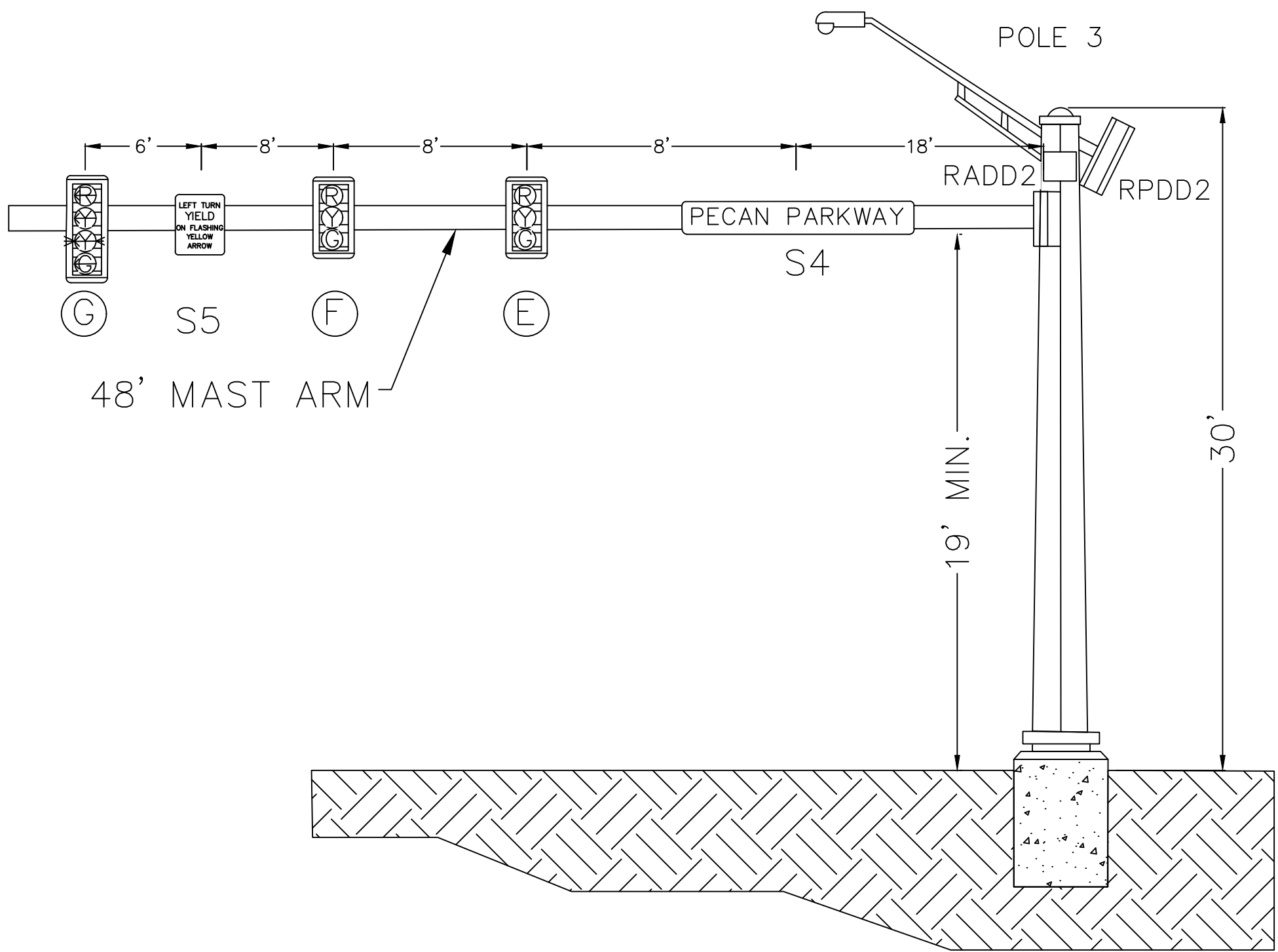
Drawing Name: N:\\_Projects\032 - Fred Heimer\032.067 - Greenspoint Heights Unit 3C\GDA\000\032.067\_0001\_LAYOUT.dwg User: jashuck Sep 20, 2024 - 9:54am



LOOKING SOUTH ON SH 46  
NOT TO SCALE



LOOKING EAST ON PECAN PARKWAY  
NOT TO SCALE



LOOKING NORTH ON SH 46  
NOT TO SCALE

- NOTES:
- HEADS WILL BE INSTALLED PER TXMUTCD 2011
  - FOUNDATIONS WILL BE ADJUSTED IN THE FIELD IN ORDER TO MEET CLEARANCE.
  - LOCATION OF MAST ARMS IS APPROXIMATE. ANY CHANGES WILL BE APPROVED BY THE ENGINEER.
  - MAST ARM ATTACHMENT HEIGHT WILL BE CALCULATED BY THE CONTRACTOR IN THE FIELD AND APPROVED BY THE ENGINEER.
  - LOCATION OF UNDERGROUND AND OVERHEAD UTILITIES ARE APPROXIMATE. R.O.W., ROADWAY, AND UTILITY INFORMATION SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY AND SHALL BE VERIFIED IN THE FIELD.
  - ALL EXISTING SIGNAL APPURTENANCES SHALL BE REMOVED UNLESS OTHERWISE SPECIFIED.
  - THE CONTRACTOR SHALL CALL UTILITY LOCATOR SERVICE AT LEAST 48 HOURS PRIOR TO COMMENCING WORK. TEXAS ' ONE - CALL ' SYSTEM: 1 -800 -24 5-4545.
  - THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY DAMAGES CAUSED BY THE FAILURE TO LOCATE AND PRESERVE THE UNDERGROUND FACILITIES.
  - SEE SIGN AND STRIPING LAYOUT FOR DETAILS OF SIGNAGE AND STRIPING.
  - POLE 1 WILL BE INSTALLED AFTER THE EXTENSION OF HAROLD GREEN WEST OF FM 973.
  - POLE 5 WILL BE INSTALLED WITH THE WIDENING OF FM 973 AND ADDITION OF SIDEWALKS ALONG FM 973.

ELEVATION SHEET

GREENSPOINT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

| NO. | REVISION DESCRIPTION | REVISION DATE |
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| DATE:            | September 22 |
| DRAWN BY:        | ZAH          |
| DESIGNED BY:     | JMP          |
| REVIEWED BY:     | JMP          |
| HMT PROJECT NO.: | 032.067      |

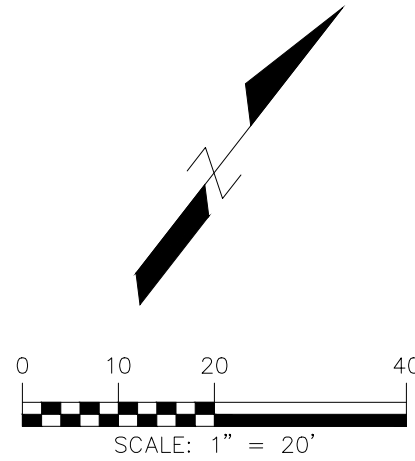
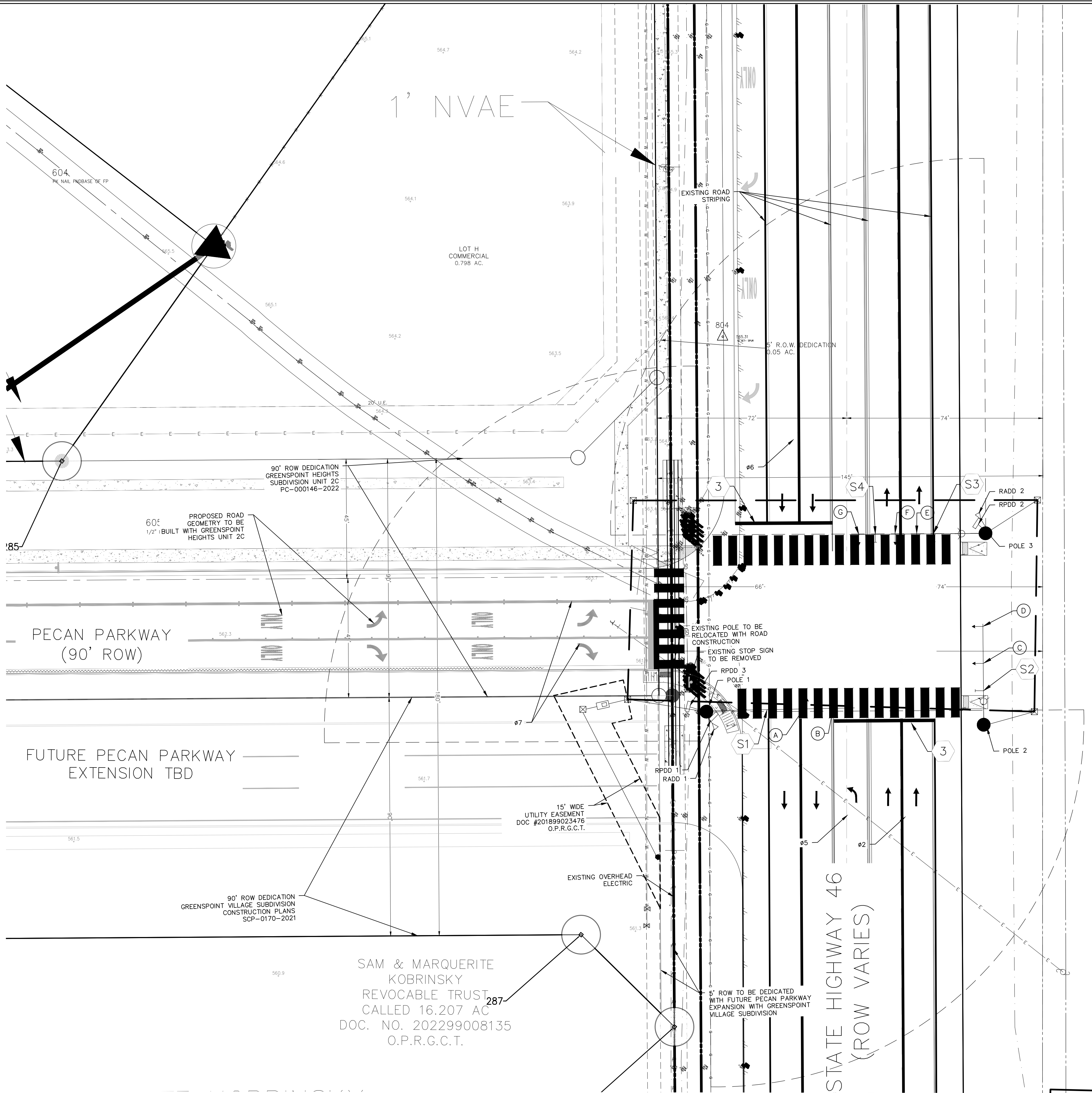
SHEET  
C2.05

290 S. CASTELL AVE., STE. 100  
NEW BRAUNFELS, TX 78130  
TBPE FIRM F-10961  
TBPLS FIRM 1053600






Drawing Name: N:\\_Projects\032 - Fred Heimer\032.067 - Greenspoint Heights Unit 3C (Pecan Parkway)\CDA\1007\032.067\_1007\_LAYOUTS.dwg User: jphuck Sep 20, 2024 - 9:54am



- NOTES:
1. CONTRACTOR TO INSTALL LENGTHS OF PROPOSED PAVEMENT MARKING AS SHOWN ON PLANS.
  2. REMOVE EXISTING MARKINGS WITHIN PROJECT LIMITS. PROPOSED PAVEMENT MARKINGS TO BE PLACED AFTER PREPARING ROADWAY SURFACE.

| SUMMARY OF PAVEMENT MARKINGS  |                       |                        |                             |
|-------------------------------|-----------------------|------------------------|-----------------------------|
| ITEM 666<br>PAVEMENT MARKINGS |                       |                        | ITEM 672<br>RPM             |
| 1                             | 2                     | 3                      | 4                           |
| 4" YELLOW<br>SLD<br>LF        | 4" WHITE<br>SLD<br>LF | 24" WHITE<br>SLD<br>LF | CL B REFL<br>TYII-A-A<br>EA |
|                               |                       | 75                     |                             |

AWK ENTERPRISES, LLC  
CALLED 23.9540 AC  
DOC #2017001876  
O.P.R.G.C.T.



290 S. CASTELL AVE., STE. 100  
NEW BRAUNFELS, TX 78130  
TBPE FIRM F-10961  
TBPLS FIRM 1053600

STRIPING & SIGNAGE  
LAYOUT  
GREENSPPOINT HEIGHTS UNIT 3C,  
TXDOT SIGNALS  
NEW BRAUNFELS, TEXAS

| NO. | REVISION DESCRIPTION | REVISION DATE |
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| REVIEWED BY:     | JMP          |
| HMT PROJECT NO.: | 032.067      |

SHEET  
C2.06