CONSTRUCTION PLANS FOR

SUBMITTED BY: MOY TARIN RAMIREZ ENGINEERS, LLC. 12770 CIMARRON PATH, SUITE 100 SAN ANTONIO, TEXAS 78249 TEL: (210) 698-5051

FAX: (210) 698-5085

OWNER/DEVELOPER

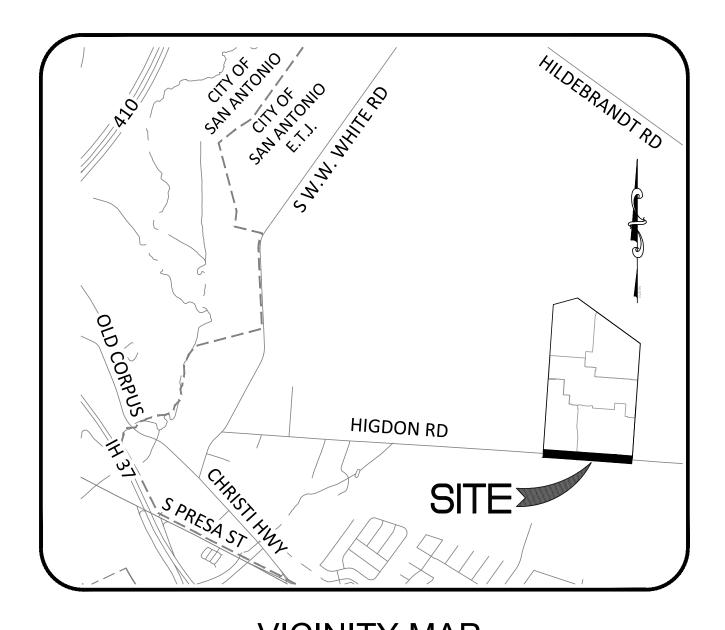
SAN ANTONIO LD, LLC 1919 OAKWELL FARMS PKWY SUITE 210 SAN ANTONIO, TX 78218 (210) 231-0703

BEXAR COUNTY R.O.W. NOTE:

BEXAR COUNTY R.O.W. PERMIT REQUIRED BEFORE WORKING IN EXISTING BEXAR COUNTY R.O.W.

HIGDON ROAD

TURN LANE IMPROVEMENTS



VICINITY MAP

SUBMITTAL DATE:

Sheet List Table

Sheet rist rable	
Sheet Number	Sheet Title
C7.0	COVER SHEET
C7.1	TYPICAL SECTIONS
C7.2	DEMOLITION PLAN
C7.3	OVERALL STORM WATER POLLUTION PREVENTION PLAN
C7.4	STORM WATER POLLUTION PREVENTION PLAN DETAILS
C7.5	OVERALL TRAFFIC CONTROL PLAN & ADVANCE WARNING SIGNS
C7.6	TRAFFIC CONTROL PLAN - PHASE A
C7.7	TRAFFIC CONTROL PLAN - PHASE B
C7.8	GRADING & PAVEMENT WIDENING
C7.9	PAVEMENT MARKINGS & SIGNAGE
C7.10	TxDOT DETAILS
C7.11	TxDOT DETAILS
C7.12	TxDOT DETAILS
C7.13	TxDOT DETAILS
C7.14	TxDOT DETAILS
C7.15	TxDOT DETAILS
C7.16	TxDOT DETAILS
C7.17	STANDARD DETAILS





BEXAR COUNTY

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE TO THE CITY OF SAN ANTONIO SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.

2. ALL CONSTRUCTION IS SUBJECT TO INSPECTION AND APPROVAL BY

UTILITIES PRIOR TO BEGINNING CONSTRUCTION:

TEXAS STATE WIDE ONE CALL LOCATOR

CITY PUBLIC SERVICE TIME WARNER CABLE

THE TELEPHONE COMPANY DURING CONSTRUCTION.

STRUCTURES OR FENCES (NO SEPARATE PAY ITEM).

DEVELOPER (NO SEPARATE PAY ITEM).

7. MATERIAL SPECIFICATIONS:

SAN ANTONIO WATER SYSTEM

3. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING EXISTING UTILITIES

RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF EXISTING

4. DUE TO FEDERAL REGULATIONS TITLE 49, PART 192.181, CPS MUST MAINTAIN

ACCESS TO GAS VALVES AT ALL TIMES. THE CONTRACTOR MUST PROTECT AND WORK AROUND ANY GAS VALVES THAT ARE IN THE PROJECT AREA. 5. THE CONTRACTOR HAS THE RESPONSIBILITY TO PROTECT AND SUPPORT

6. THE CONTRACTOR HAS THE RESPONSIBILITY OF RESTORING TO ITS ORIGINAL OR BETTER CONDITION, ANY DAMAGE DONE TO THE EXISTING PAVEMENT,

CONCRETE/CONCRETE RIPRAP: CLASS A 3000 PSI IN 28 DAYS UNLESS

(2" COVER UNLESS OTHERWISE NOTED ON PLANS)

REINFORCING STEEL: CONFORM TO A.S.T.M. A-615, GRADE 60

PIPE RAILING: CONFORM TO A.S.T.M. A-53, GRADE B, OR A-501

10. IMPROVED EARTHEN CHANNELS WILL BE VEGETATED BY SEEDING OR SODDING. EIGHTY-FIVE PERCENT OF THE CHANNEL SUBGRADE AREA MUST HAVE ESTABLISHED VEGETATION BEFORE THE CHANNEL IS ACCEPTED FOR MAINTENANCE. REFER TO

STRUCTURAL STEEL: CONFORM TO A.S.T.M. A-36

8. CONTRACTOR TO COORDINATE CONCRETE CURB DEPRESSIONS WITH THE

9. TRANSITION TO/FROM WASHOUT CROWNS IN TWENTY-FIVE FEET (25').

35-504 OF THE CITY OF SAN ANTONIO UDC. NO EXTRA PAY ITEM.

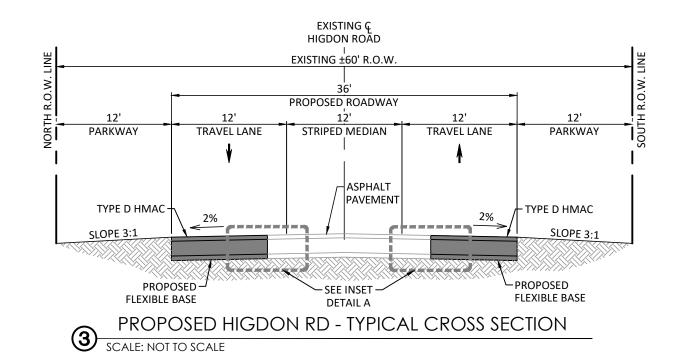
DURING CONSTRUCTION. THE LOCATION AND DEPTH OF EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE ONLY. THE CONTRACTOR IS

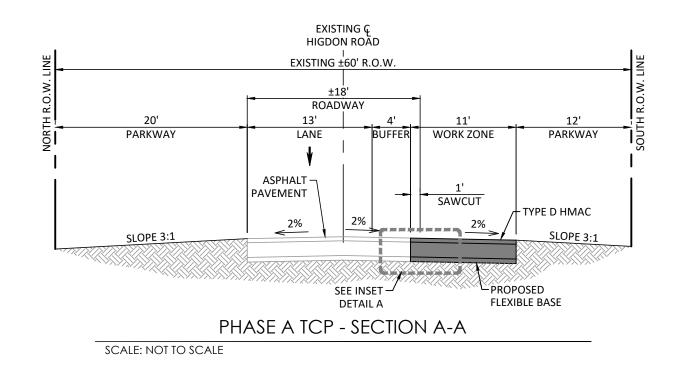
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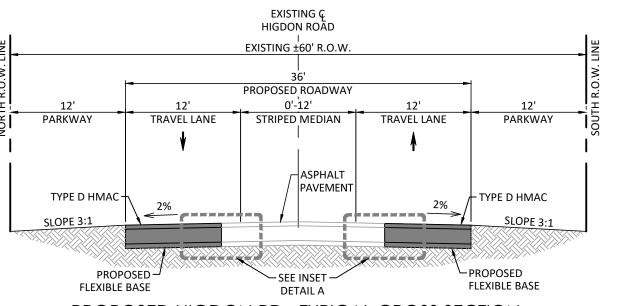
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THE CITY OF SAN ANTONIO.

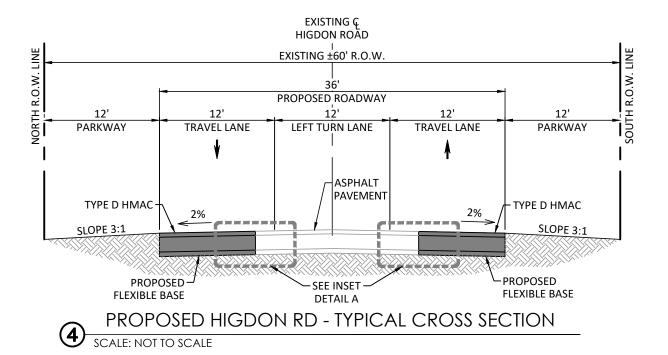
EXISTING HIGDON RD - TYPICAL CROSS SECTION SCALE: NOT TO SCALE

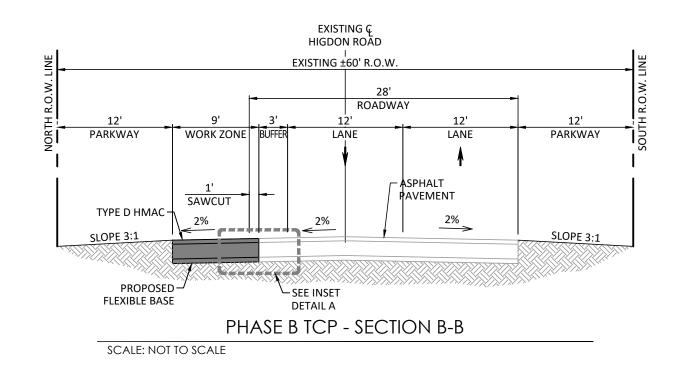


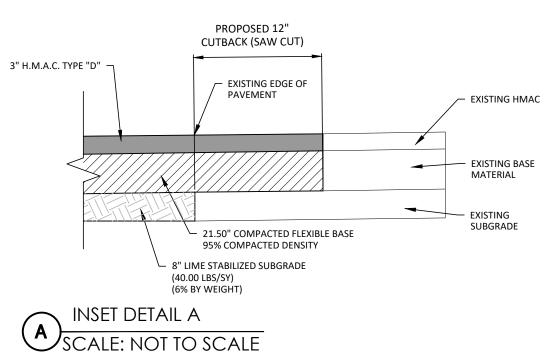




PROPOSED HIGDON RD - TYPICAL CROSS SECTION SCALE: NOT TO SCALE







GENERAL NOTES:

- 1. DO NOT BLOCK DRAINAGE WHEN HANDLING & STOCKPILING EXCAVATED MATERIAL WITHIN STATE R.O.W.
- 2. PROVIDE ADDITIONAL SIGNS AND BARRICADES AS NECESSARY TO ADDRESS FIELD CONSTRUCTIBILITY & VISIBILITY.
- 3. REMOVE OR COVER ALL EXISTING SIGNS WHICH ARE IN CONFLICT WITH THE TRAFFIC CONTROL.
- EXISTING STOP SIGNS UNTIL TEMPORARY SIGNS ARE IN PLACE. 5. COORDINATE THE TRAFFIC CONTROL PLAN AND THE VARIOUS SEQUENCES OF CONSTRUCTION WITH ADJACENT CONSTRUCTION PROJECTS

4. ADJUST STOP SIGNS AS NEEDED ON INTERSECTING STREET DURING THE VARIOUS CONSTRUCTION PHASES, DO NOT REMOVE ANY

- 6. ALL BARRICADES, SIGNS, WARNING LIGHTS, ETC, SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, BARRICADE STANDARD SHEETS.
- 7. PROVIDE AND MAINTAIN ACCESS TO ADJACENT PROPERTIES AT ALL TIMES.
- 8. NO EQUIPMENT SHALL BE PERMITTED TO REMAIN IN A POSITION AFTER WORKING HOURS THAT WILL ENDANGER THE TRAVELING PUBLIC.
- 9. STATE R.O.W. WILL NOT BE USED AS AN AREAS FOR CONTRACTOR PARKING OR FOR STAGING THE RECEIPT OF MATERIALS OR EQUIPMENT.
- 10. THE CONTRACTOR IS RESPONSIBLE FOR PLACING AND MAINTAINING EXISTING SIGNS ON TXDOT APPROVED TEMPORARY MOUNTS UNTIL PERMANENT SIGNS ARE PLACED.
- 11. THE CONTRACTOR MAY PROPOSE OR RECOMMEND MODIFICATIONS TO THE SEQUENCE OF WORK. THE CONTRACTOR WILL PREPARE AND AND SUBMIT ANY MODIFICATIONS TO THE TRAFFIC CONTROL PLAN TO THE ENGINEER AND RECEIVE APPROVAL PRIOR TO BEGINNING
- 12. ROADWAY WIDENING FOR DECELERATION LANE, STREET OR INTERSECTIONS WILL COMPLY WITH TXDOT SPECIFICATION ITEM NO. 112, 132, 316, 340 AND 341.
- 13. FINISHED GRADING AT THE EDGE OF EXISTING OR PROPOSED ROADWAY PAVEMENT WILL INCLUDE A LAYER OF TOP SOIL TO BE PLACED AT A MINIMUM 4" DEPTH.

SHEET

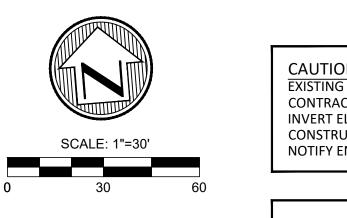
SUBMITTAL SET

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PAUL LANDA, JR.

100182

GENERAL NOTES: 1. THE INFORMATION SHOWN ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OVERHEAD UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OR HER OWN DETERMINATION AS TO THE TYPE AND LOCATION OF EXISTING UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO EVEN IF THEY ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR WILL COORDINATE ANY NECESSARY RELOCATIONS. 2. THE CONTRACTOR WILL REGRADE AND SOD AREAS OF PROPOSED ASPHALT REMOVAL. 3. ALL COURSES OF ASPHALTIC CONCRETE PAVEMENT (REGARDLESS OF TYPE OR LAYER) WILL BE PLACED WITH AN ASPHALT PAVER. ALL EQUIPMENT USED IN THE PLACEMENT OF THIS MATERIAL WILL MEET THE REQUIREMENTS OF ITEM 320, "EQUIPMENT FOR ASPHALT CONCRETE PAVEMENT". THE MAXIMUM LIFT THICKNESS FOR TYPE A ASPHALTIC CONCRETE PAVEMENT IS 3.5 INCHES. 4. ALL SURFACE AGGREGATES WILL MEET THE REQUIREMENTS OF TXDOT FRICTION CLASSIFICATION "B". 5. ALL SURFACE ASPHALT CONCRETE PAVEMENT WILL BE UNDERSEALED WITH A ONE COURSE SURFACE TREATMENT AND WILL MEET PG BINDER GRADE 70-22 AND PLACED IN ACCORDANCE WITH ITEM 340 OF TXDOT STANDARD SPECIFICATIONS. 6. ALL ASPHALTIC CONCRETE PAVEMENT USED IN BASE COURSES WILL BE TYPE "A" OR "B" AND WILL MEET PG BINDER GRADE 64-22. 7. ALL TEMPORARY EROSION SEDIMENT AND ENVIRONMENTAL CONTROLS SHALL BE DONE IN ACCORDANCE WITH ITEM 506 OF THE LATEST TXDOT STANDARD SPECIFICATIONS. 8. THE CONTRACTOR SHALL COORDINATE ALL WORK IN TXDOT RIGHT-OF-WAY WITH TXDOT'S REPRESENTATIVES PRIOR TO AND DURING CONSTRUCTION. 9. CONTRACTOR SHALL COORDINATE MAILBOX REMOVAL AND RELOCATION WITH POSTAL



CAUTION!!! **EXISTING UNDERGROUND UTILITIES.** CONTRACTOR TO VERIFY LOCATION AND INVERT ELEVATION PRIOR TO CONSTRUCTION, IF ANY DISCREPANCY NOTIFY ENGINEER

WORKING IN EXISTING BEXAR COUNTY R.O.W.

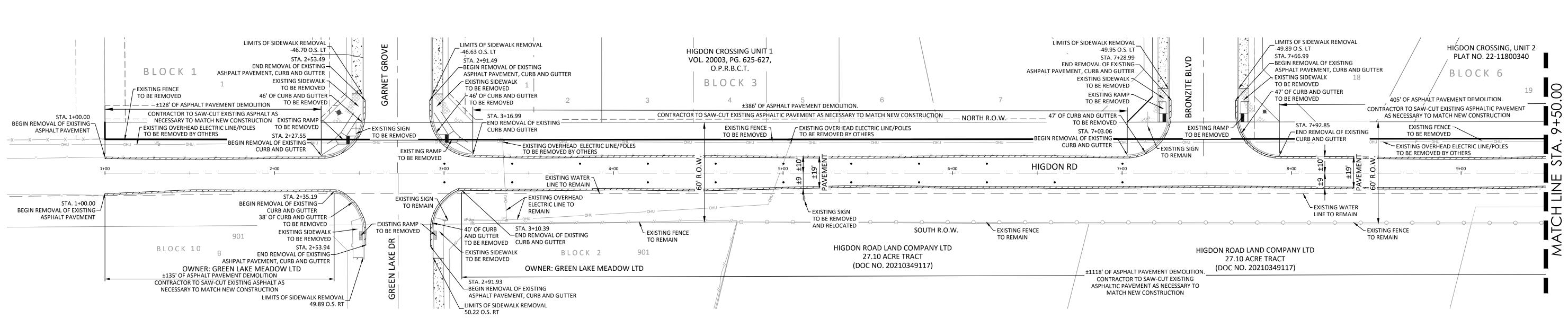
SAW, CUT, REMOVE AND REPLACE EXISTING PAVEMENT SAW, CUT, REMOVE AND REPLACE EXISTING PAVEMENT

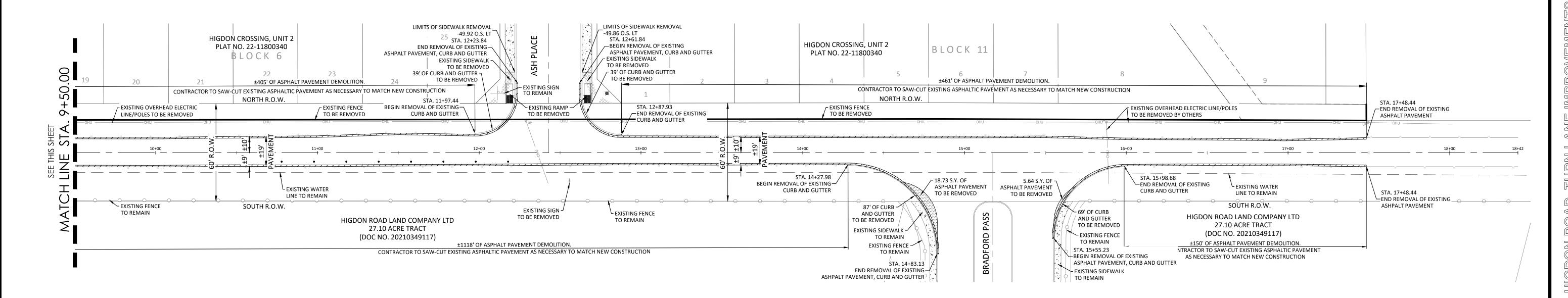
LEGEND

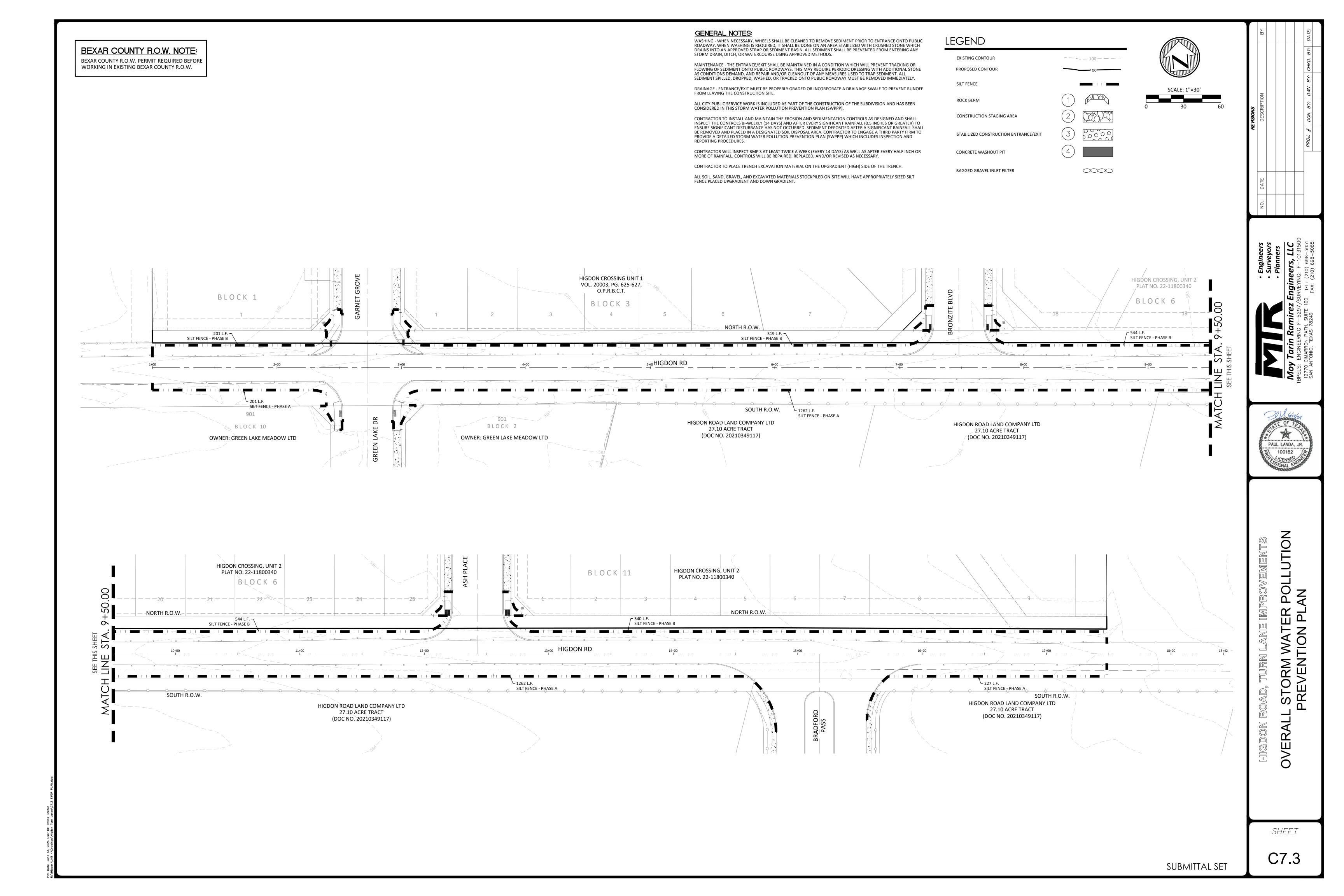
BEXAR COUNTY R.O.W. NOTE: BEXAR COUNTY R.O.W. PERMIT REQUIRED BEFORE







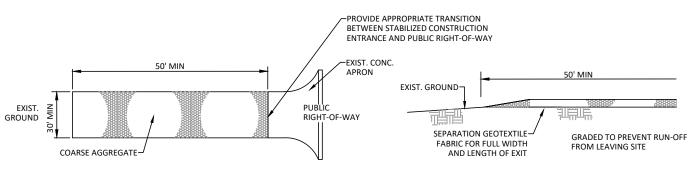




SILT FENCE NOTES

COMPACTED MATERIAL.

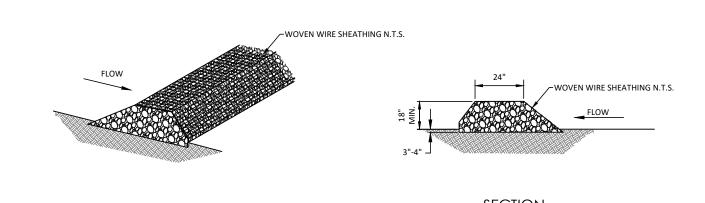
- 1. SILT FENCE MATERIAL SHOULD BE POLYPROPYLENE, POLYETHYLENE OR POLYAMIDE WOVEN OR NONWOVEN FABRIC. THE FABRIC WIDTH SHOULD BE 36 INCHES, WITH A MINIMUM UNIT WEIGHT OF 4.5 OZ/YD, MULLEN BURST STRENGTH EXCEEDING 190 LB/IN 2, ULTRAVIOLET STABILITY EXCEEDING 70%, AND MINIMUM APPARENT OPENING SIZE OF U.S. SIEVE NO. 30.
- 2. FENCE POSTS SHOULD BE MADE OF HOT ROLLED STEEL, AT LEAST 4 FEET LONG WITH TEE OR Y-BAR CROSS SECTION, SURFACE PAINTED OR GALVANIZED, MINIMUM NOMINAL WEIGHT 1.25 LB/FT 2 , AND BRINDELL HARDNESS EXCEEDING 140.
- 3. WOVEN WIRE BACKING TO SUPPORT THE FABRIC SHOULD BE GALVANIZED 2" X 4" WELDED WIRE, 12.5 GAUGE MINIMUM.
- 4. STEEL POSTS, WHICH SUPPORT THE SILT FENCE, SHOULD BE INSTALLED ON A SLIGHT ANGLE TOWARD THE ANTICIPATED RUNOFF SOURCE. POST MUST BE EMBEDDED A MINIMUM OF 1 FOOT DEEP AND SPACED NOT MORE THAN 5 FEET ON CENTER.
- 5. LAY OUT FENCING DOWN-SLOPE OF DISTURBED AREA, FOLLOWING THE CONTOUR AS CLOSELY AS POSSIBLE. THE FENCE SHOULD BE SITED SO THAT THE MAXIMUM DRAINAGE AREA IS ¼ ACRE/100 FEET OF FENCE.
- 6. THE TOE OF THE SILT FENCE SHOULD BE TRENCHED IN WITH A SPADE OR MECHANICAL TRENCHER, SO THAT THE DOWN-SLOPE FACE OF THE TRENCH IS FLAT AND PERPENDICULAR TO THE LINE OF FLOW. WHERE FENCE CANNOT BE TRENCHED IN (E.G., PAVEMENT OR ROCK OUTCROP), WEIGHT FABRIC FLAP WITH 3 INCHES OF PEA GRAVEL ON UPHILL SIDE TO PREVENT FLOW FROM SEEPING UNDER FENCE.
- 7. THE TRENCH MUST BE A MINIMUM OF 6 INCHES DEEP AND 6 INCHES WIDE TO ALLOW FOR THE SILT FENCE FABRIC TO BE LAID IN THE GROUND AND BACKFILLED WITH
- 8. SILT FENCE SHOULD BE SECURELY FASTENED TO EACH STEEL SUPPORT POST OR TO WOVEN WIRE, WHICH IS IN TURN ATTACHED TO THE STEEL FENCE POST. THERE SHOULD BE A 3-FOOT OVERLAP, SECURELY FASTENED WHERE ENDS OF FABRIC MEET.
- 9. SILT FENCE SHOULD BE REMOVED WHEN THE SITE IS COMPLETELY STABILIZED SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE.
- 10. REMOVE SEDIMENT WHEN BUILDUP REACHES 6 INCHES, OR INSTALL A SECOND LINE OF FENCING PARALLEL TO THE OLD FENCE.
- 11. REPLACE ANY TORN FABRIC OR INSTALL A SECOND LINE OF FENCING PARALLEL TO THE TORN SECTION.
- 12. REPLACE OR REPAIR ANY SECTIONS CRUSHED OR COLLAPSED IN THE COURSE OF CONSTRUCTION ACTIVITY. IF A SECTION OF FENCE IS OBSTRUCTING VEHICULAR ACCESS, CONSIDER RELOCATING IT TO A SPOT WHERE IT WILL PROVIDE EQUAL PROTECTION, BUT WILL NOT OBSTRUCT VEHICLES. A TRIANGULAR FILTER DIKE MAY BE PREFERABLE TO A



PROFILE PLAN VIEW

TEMPORARY CONSTRUCTION ENTRANCE/EXIT NOTES

- THE AGGREGATE SHOULD CONSIST OF 4 TO 8 INCH WASHED STONE OVER A STABLE FOUNDATION.
- THE AGGREGATE SHOULD BE PLACED WITH A MINIMUM THICKNESS OF 8 INCHES. THE GEOTEXTILE FABRIC SHOULD BE DESIGNED SPECIFICALLY FOR USE AS A SOIL FILTRATION MEDIA WITH AN APPROXIMATE WEIGHT OF 6 OZ/YD 2, A MULLEN
- BURST RATING OF 140 LB/IN 2, AND AN EQUIVALENT OPENING SIZE GREATER THAN A NUMBER 50 SIEVE.
- AVOID CURVES ON PUBLIC ROADS AND STEEP SLOPES. REMOVE VEGETATION AND OTHER OBJECTIONABLE MATERIAL FROM THE FOUNDATION AREA. GRADE CROWN FOUNDATION FOR POSITIVE DRAINAGE.
- THE MINIMUM WIDTH OF THE ENTRANCE/EXIT SHOULD BE 12 FEET OR THE FULL WIDTH OF EXIT ROADWAY, WHICHEVER IS GREATER. THE CONSTRUCTION ENTRANCE SHOULD BE AT LEAST 50 FEET LONG.
- 7. PLACE GEOTEXTILE FABRIC AND GRADE FOUNDATION TO IMPROVE STABILITY, ESPECIALLY WHERE WET CONDITIONS ARE ANTICIPATED. PLACE STONE TO DIMENSIONS AND GRADE SHOWN. LEAVE SURFACE SMOOTH AND SLOPE FOR DRAINAGE.
- THE ENTRANCE SHOULD BE MAINTAINED IN A CONDITION, WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP
- 10. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ON TO PUBLIC RIGHTS-OF-WAY SHOULD BE REMOVED IMMEDIATELY BY CONTRACTOR. 11. WHEN NECESSARY, WHEELS SHOULD BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
- 12. WHEN WASHING IS REQUIRED, IT SHOULD BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR
- 13. ALL SEDIMENT SHOULD BE PREVENTED FROM ENTERING ANY STORM DRAIN, DITCH OR WATER COURSE.



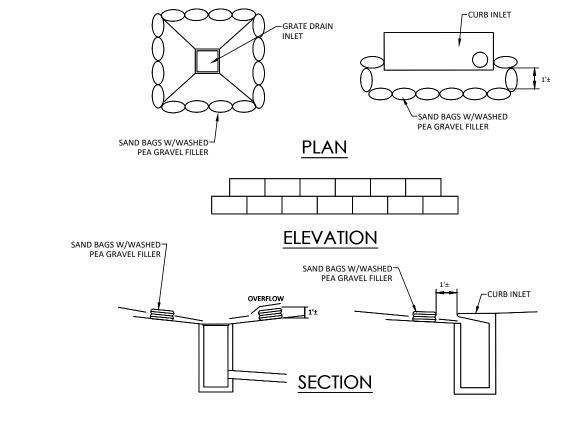
ROCK BERM NOTES

ROCK BERM

- . THE BERM STRUCTURE SHOULD BE SECURED WITH A WOVEN WIRE SHEATHING HAVING MAXIMUM OPENING OF 1 INCH AND A MINIMUM WIRE DIAMETER OF 20 GAUGE GALVANIZED AND SHOULD BE SECURED WITH SHOAT RINGS.
- 2. CLEAN, OPEN GRADED 3-TO 5-INCH DIAMETER ROCK SHOULD BE USED, EXCEPT IN AREAS WHERE HIGH VELOCITIES OR LARGE VOLUMES OF FLOW ARE EXPECTED. WHERE 5-TO 8-INCH DIAMETER ROCKS MAY BE USED.
- 3. LAY OUT THE WOVEN WIRE SHEATHING PERPENDICULAR TO THE FLOW LINE.

ISOMETRIC PLAN

- 4. BERM SHOULD HAVE A TOP WIDTH OF 2 FEET MINIMUM WITH SIDE SLOPES BEING 2:1 (H:V) OR FLATTER.
- 5. PLACE THE ROCK ALONG THE SHEATHING TO A HEIGHT NOT LESS THAN 18".
- 6. WRAP THE WIRE SHEATHING AROUND THE ROCK AND SECURE WITH TIE WIRE SO THAT THE ENDS OF THE SHEATHING OVERLAP AT LEAST 2 INCHES, AND THE BERM RETAINS ITS
- 7. BERM SHOULD BE BUILT ALONG THE CONTOUR AT ZERO PERCENT GRADE OR AS NEAR AS POSSIBLE.
- 8. THE ENDS OF THE BERM SHOULD BE TIED INTO EXISTING UPSLOPE GRADE AND THE BERM SHOULD BE BURIED IN A TRENCH APPROXIMATELY 3 TO 4 INCHES DEEP TO PREVENT
- 9. INSPECTION SHOULD BE MADE WEEKLY AND AFTER EACH RAINFALL BY THE RESPONSIBLE PARTY. FOR INSTALLATIONS IN STREAMBEDS. ADDITIONAL DAILY INSPECTIONS SHOULD BE MADE. THE BERM SHOULD BE RESHAPED AS NEEDED DURING INSPECTION.
- 10. REMOVE SEDIMENT AND OTHER DEBRIS WHEN BUILDUP REACHES 6 INCHES AND DISPOSE OF THE ACCUMULATED SILT OF IN AN APPROVED MANNER AND REPAIR ANY LOOSE WIRE
- 11. THE BERM SHOULD BE REPLACED WHEN THE STRUCTURE CEASES TO FUNCTION AS INTENDED DUE TO SILT ACCUMULATION AMONG THE ROCKS, WASHOUT, CONSTRUCTION
- 12. THE ROCK BERM SHOULD BE LEFT IN PLACE UNTIL ALL UPSTREAM AREAS ARE STABILIZED AND ACCUMULATED SILT REMOVED.

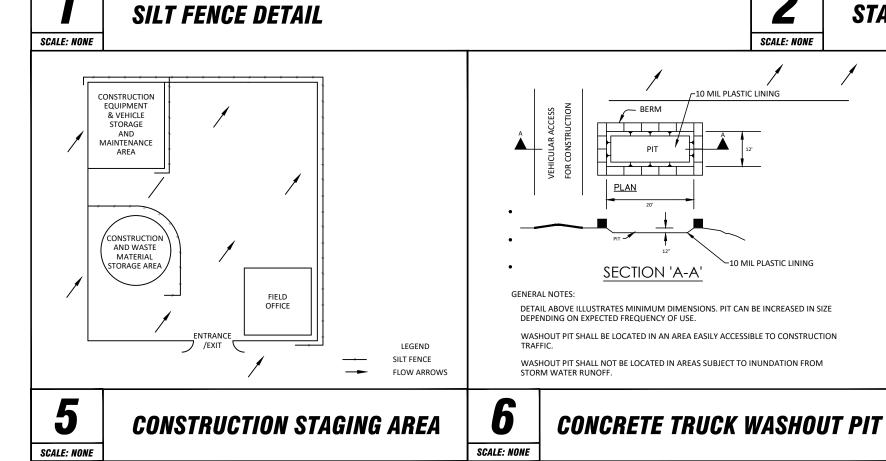


- THE GRAVEL BAG MATERIAL SHOULD BE POLYPROPYLENE, POLYETHYLENE, POLYAMIDE OR COTTON BURLAP WOVEN FABRIC, MINIMUM UNIT WEIGHT 4 OZ/YD 2 , MULLEN BURST STRENGTH EXCEEDING 300 PSI AND ULTRAVIOLET STABILITY EXCEEDING 70
- 2. THE BAG LENGTH SHOULD BE 24 INCHES, WIDTH SHOULD BE 18 INCHES AND THICKNESS SHOULD BE 6 INCHES.
- 3. THE GRAVEL BAGS SHOULD BE FILLED WITH $\frac{3}{4}$ " GRAVEL .
- 4. WHEN A GRAVEL BAG IS FILLED WITH GRAVEL, THE OPEN END OF THE GRAVEL BAG SHOULD BE STAPLED OR TIED WITH NYLON OR
- THE GRAVEL BAGS SHOULD BE PLACED AS SHOWN ON THE DETAIL. THE GRAVEL BAGS SHALL BE STACKED TO FORM A CONTINUOUS BARRIER AROUND THE INLETS. THE BAGS SHOULD BE TIGHTLY ABUTTED AGAINST EACH OTHER TO PREVENT RUNOFF FROM FLOWING BETWEEN THE BAGS.
- INSPECTION SHOULD BE MADE WEEKLY AND AFTER EACH RAINFALL. REPAIR OR REPLACEMENT SHOULD BE MADE PROMPTLY AS NEEDED BY THE CONTRACTOR.
- CHECK PLACEMENT OF DEVICE TO PREVENT GAPS BETWEEN DEVICE AND CURB. 8. REMOVE SEDIMENT WHEN BUILDUP REACHES A DEPTH OF 3 INCHES. REMOVED SEDIMENT SHOULD BE DEPOSITED IN A SUITABLE
- AREA AND IN SUCH A MANNER THAT IT WILL NOT ERODE.
- 9. STRUCTURES SHOULD BE REMOVED AND THE AREA STABILIZED ONLY AFTER THE REMAINING DRAINAGE AREA HAS BEEN PROPERLY STABILIZED.

SCALE: NONE

BAGGED GRAVEL INLET FILTER

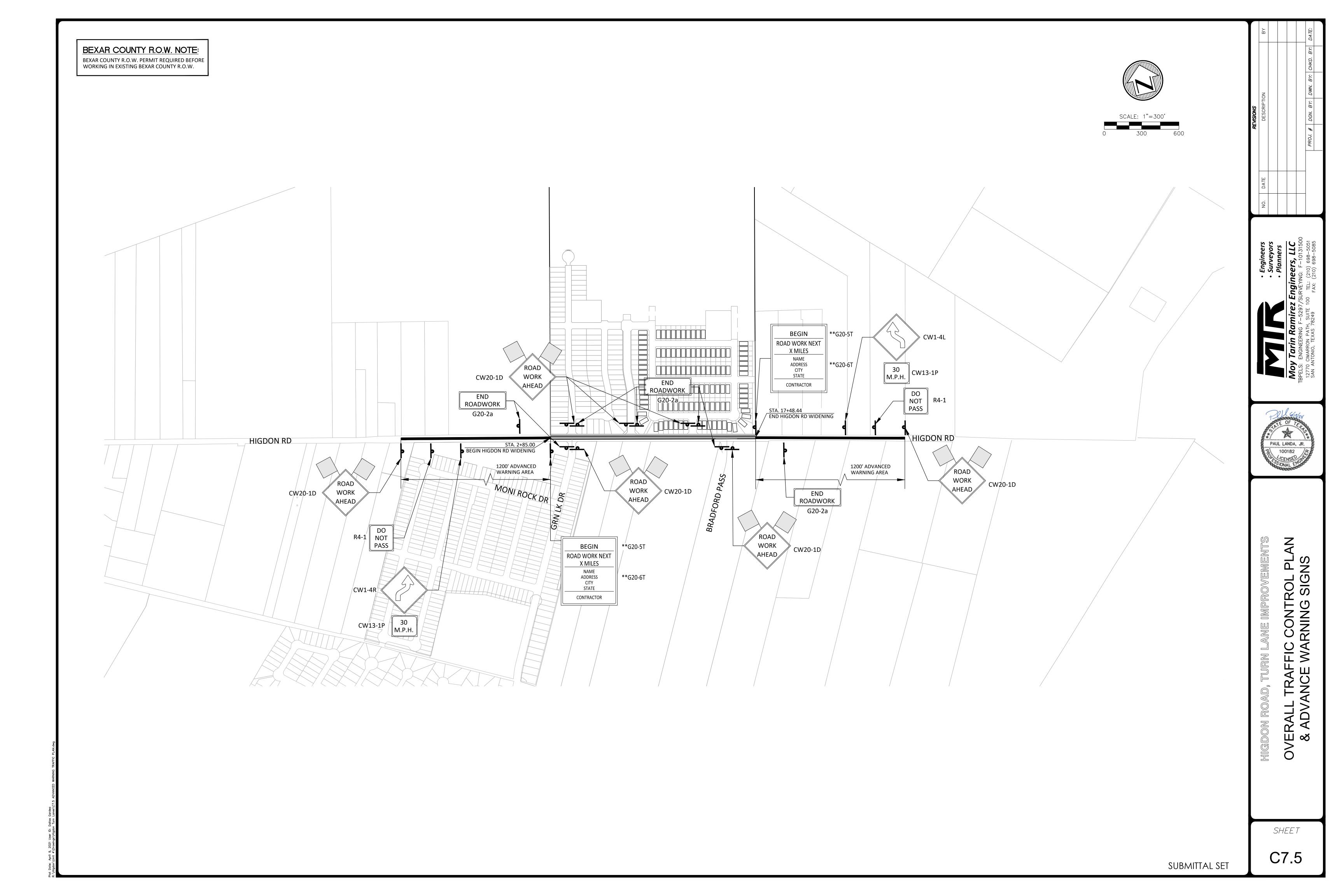
STABILIZED CONSTRUCTION ENTRANCE / EXIT

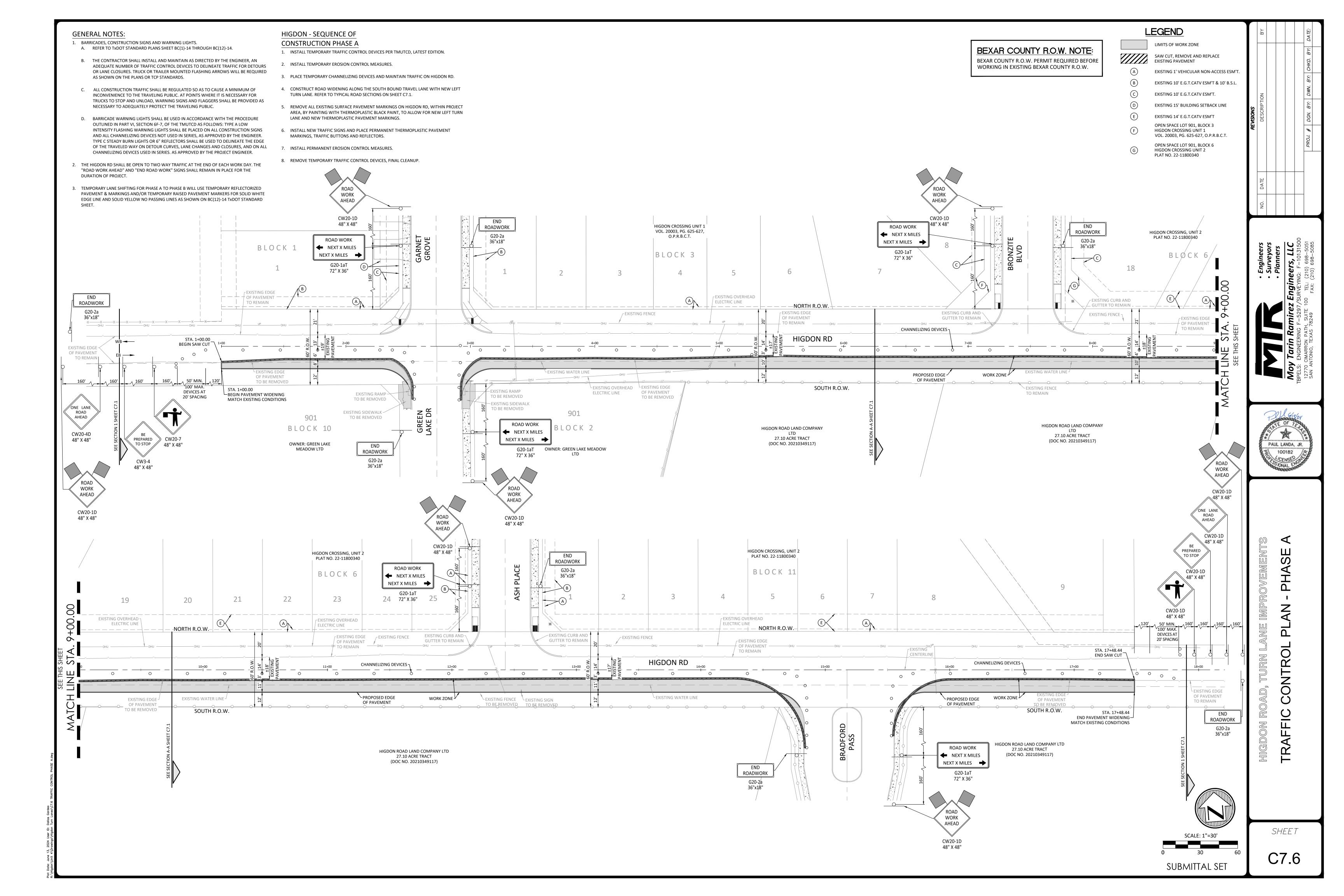


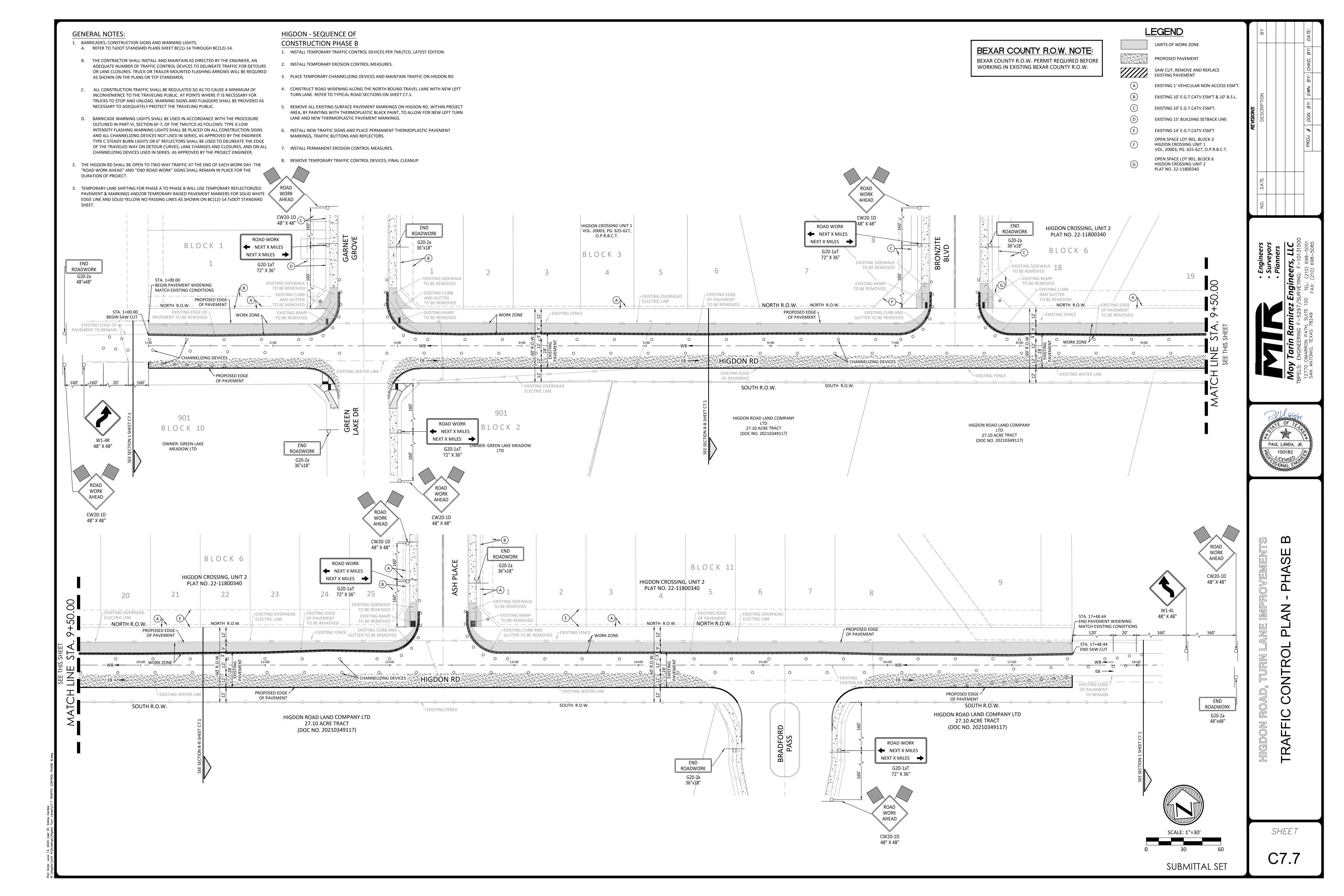


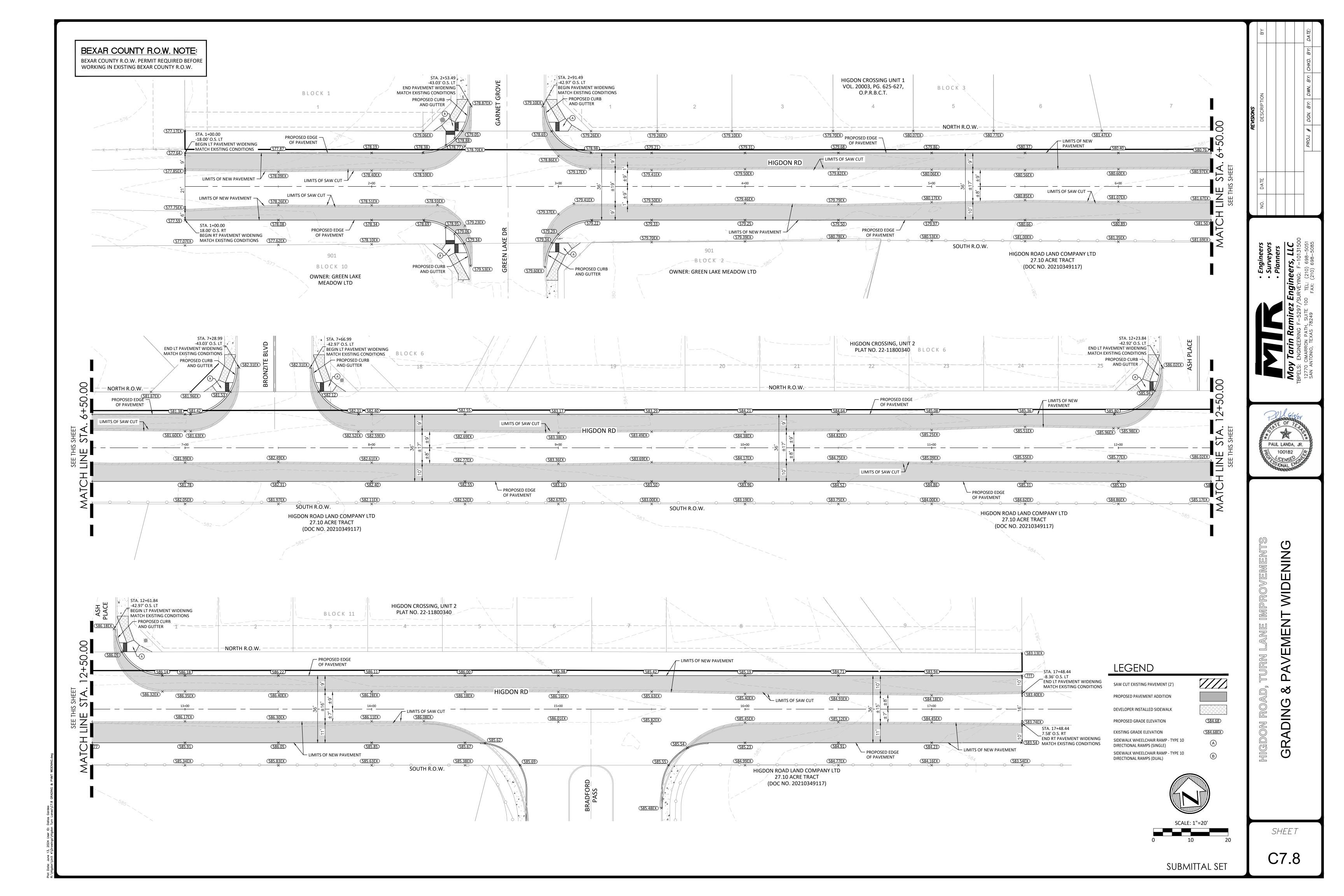
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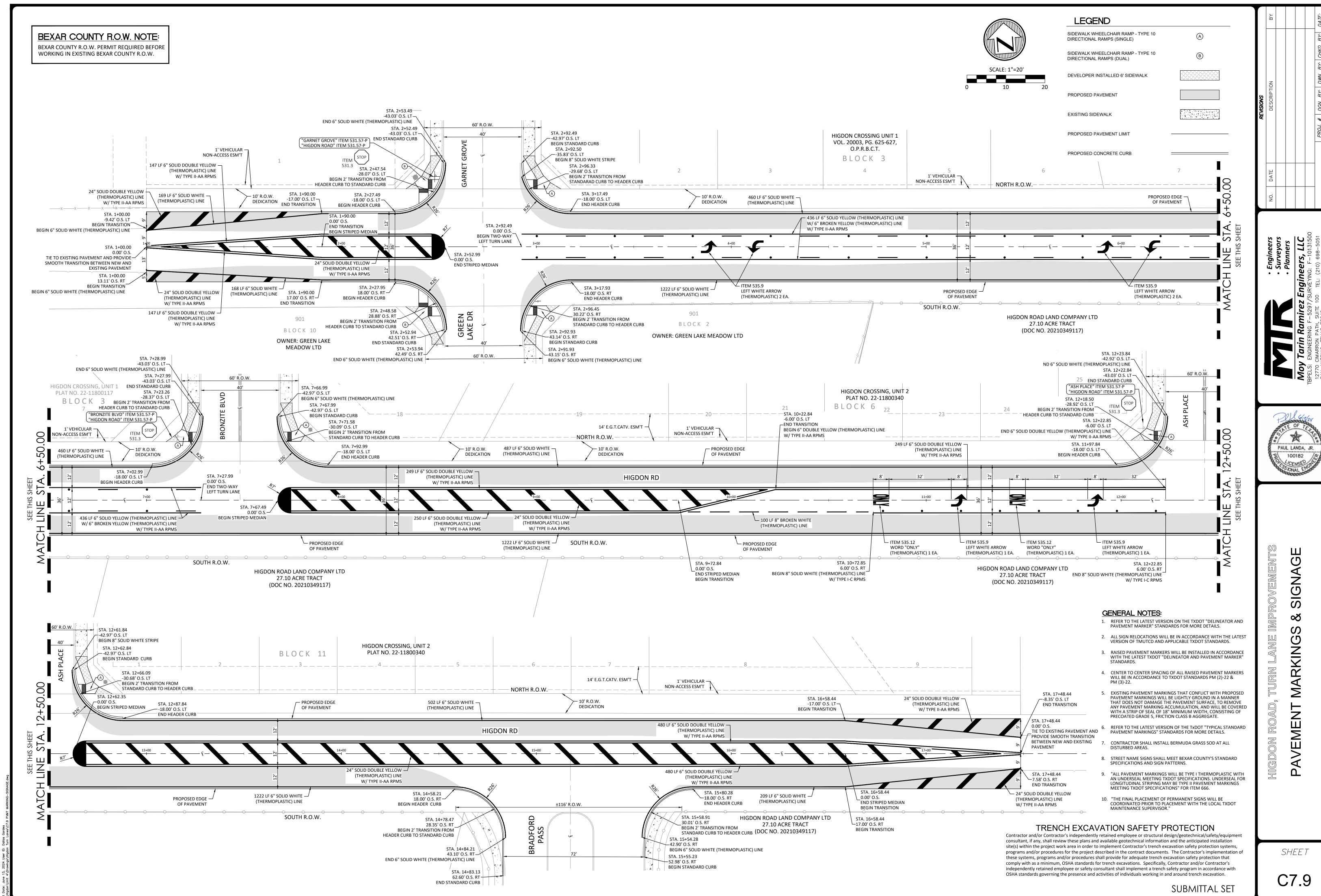
WATER

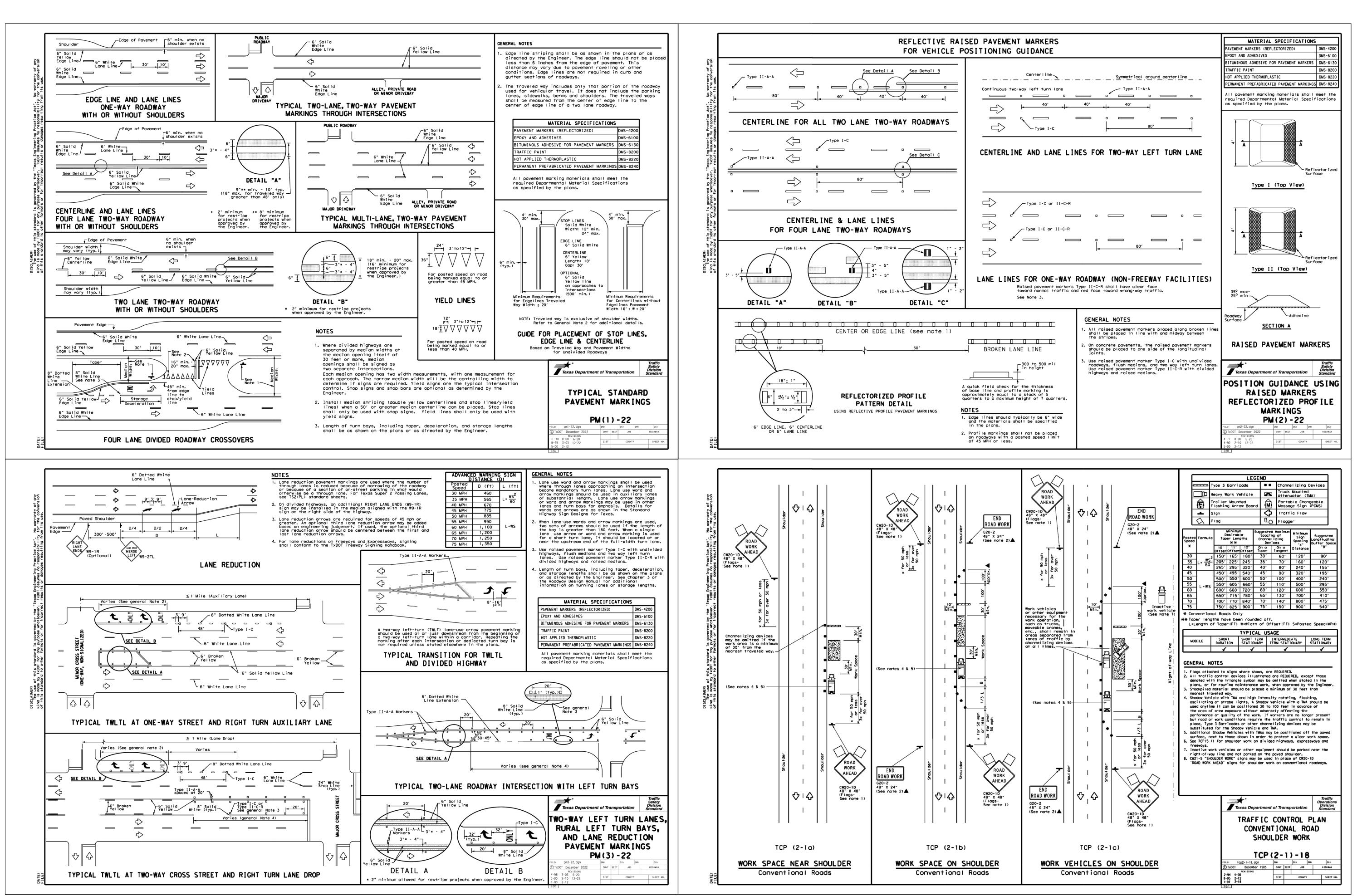












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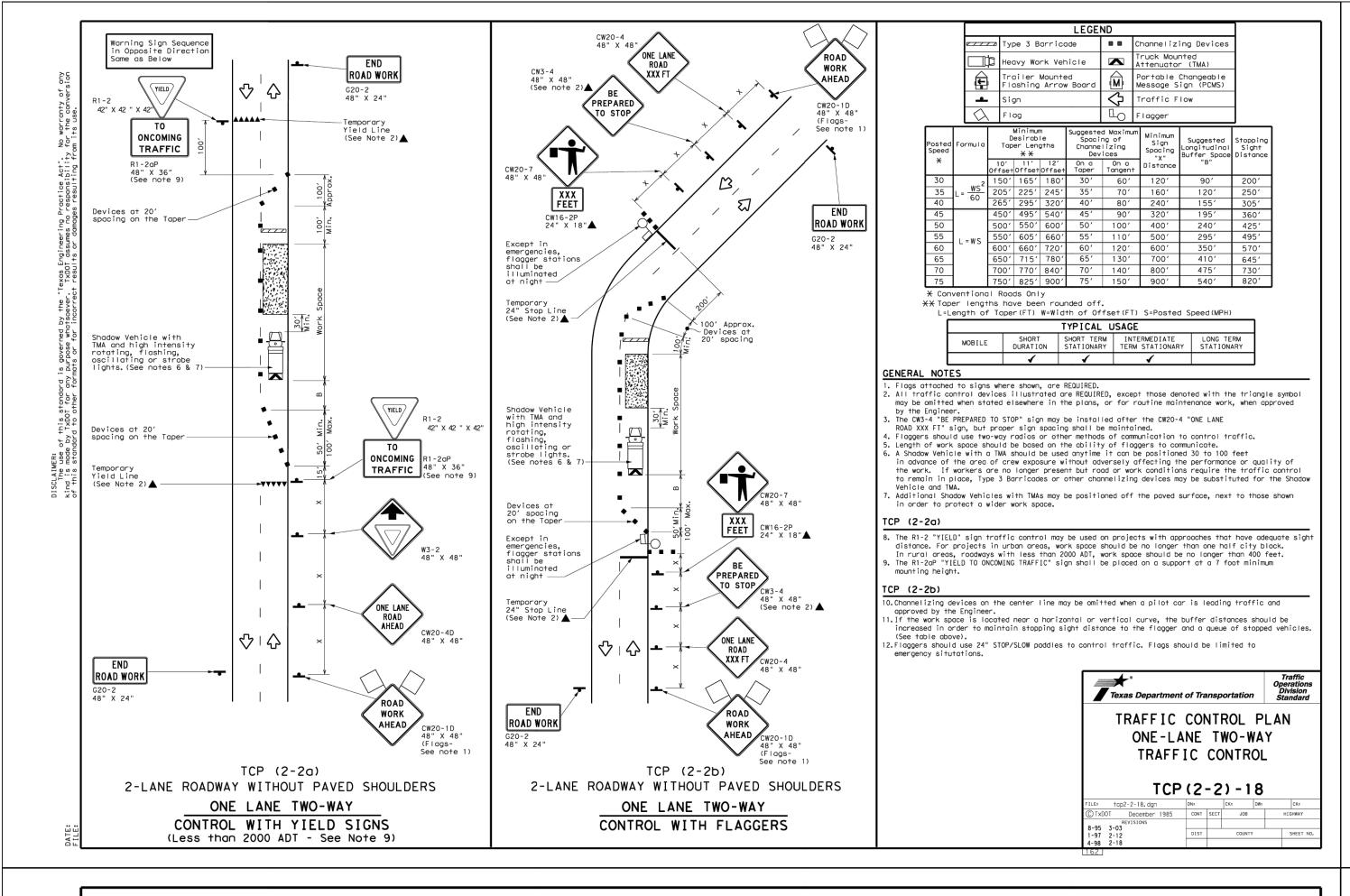
770 CIMARRON PATH, SUITE 100 TEL: (210) 698

N ANTONIO, TEXAS 78249 FAX: (210) 698



AD, TURN LANE IMPROVEME TXDOT DETAILS





BARRICADE AND CONSTRUCTION (BC) STANDARD SHEETS GENERAL NOTES: The Barricade and Construction Standard Sheets (BC sheets) are intended to show typical examples for placement of temporary traffic control devices, construction payement markings, and typical work zone signs. The information contained in these sheets meet or exceed the requirements

The development and design of the Traffic Control Plan (TCP) is the responsibility of the Engineer.

The Contractor may propose changes to the TCP that are signed and sealed by a licensed professional engineer for approval. The Engineer may develop, sign and seal Contractor proposed changes.

shown in the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).

The Contractor is responsible for installing and maintaining the traffic control devices as shown in the plans. The Contractor may not move or change the approximate location of any device without the approval of the Engineer. Geometric design of lane shifts and detours should, when possible, meet the

applicable design criteria contained in manuals such as the American Association of State Highway and Transportation Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets," the TxDOT "Roadway Design Manual" or engineering judgment.

When projects abut, the Engineer(s) may omit the END ROAD WORK, TRAFFIC FINES DOUBLE, and other advance warning signs if the signing would be redundant and the work areas appear continuous to the motorists. If the adjacent project is completed first, the Contractor shall erect the necessary warning signs as shown on these sheets, the TCP sheets or as directed by the Engineer. The BEGIN ROAD WORK NEXT X MILES sign shall be revised to show appropriate work zone distance.

The Engineer may require duplicate warning signs on the median side of divided highways where median width will permit and traffic volumes justify the signing.

8. All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.

The temporary traffic control devices shown in the illustrations of the BC sheets are examples. As necessary, the Engineer will determine the most appropriate traffic control devices to be used.

10. Where highway construction or maintenance work is being undertaken, other than mobile operations as defined by the Texas Manual on Uniform Traffic Control Devices, CSJ limit signs are required. CSJ limit signs are shown on BC(2). The OBEY WARNING SIGNS STATE LAW sign, STAY ALERT TALK OR TEXT LATER and the WORK ZONE TRAFFIC FINES DOUBLE sign with plaque shall be erected in advance of the CSJ limits. The BEGIN ROAD WORK NEXT X MILES. CONTRACTOR and END ROAD WORK signs shall be erected at or near the CSJ limits. For mobile operations, CSJ limit signs are not required.

11. Traffic control devices should be in place only while work is actually in progress or a definite need exists.

12. The Engineer has the final decision on the location of all traffic control

13. Inactive equipment and work vehicles, including workers' private vehicles must be parked away from travel lanes. They should be as close to the right-of-way line as possible, or located behind a barrier or guardrail, or as approved by the Engineer.

WORKER SAFETY NOTES:

1. Workers on foot who are exposed to traffic or to construction equipment within the right-of-way shall wear high-visibility safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel, " or equivalent revisions, and labeled as ANSI 107-2004 standard performance for Class 2 or 3 risk exposure. Class 3 garments should be considered for high traffic volume work areas or night time work.

2. Except in emergency situations, flagger stations shall be illuminated when flagging is used at night.

COMPLIANT WORKZONE TRAFFIC CONTROL DEVICES

1. Only pre-qualified products shall be used. The "Compliant Work Zone Traffic Control Devices List" (CWZTCD) describes pre-qualified products

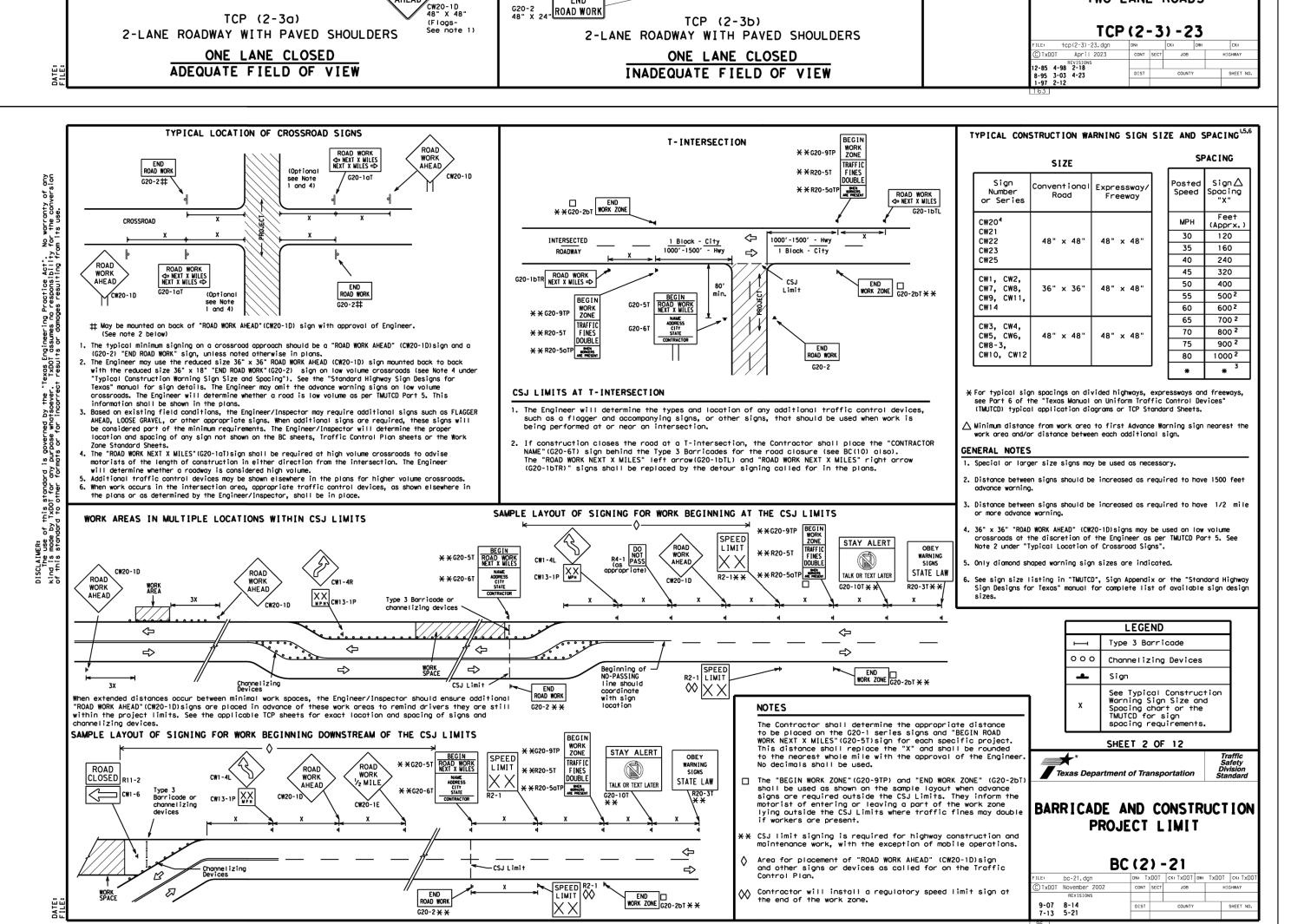
2. Work zone traffic control devices shall be compliant with the Manual for Assessing safety Hardware (MASH).

THE DOCUMENTS BELOW CAN BE FOUND ON-LINE AT http://www.txdot.gov

COMPLIANT WORK ZONE TRAFFIC CONTROL DEVICES LIST (CWZTCD) DEPARTMENTAL MATERIAL SPECIFICATIONS (DMS) MATERIAL PRODUCER LIST (MPL) ROADWAY DESIGN MANUAL - SEE "MANUALS (ONLINE MANUALS)"

STANDARD HIGHWAY SIGN DESIGNS FOR TEXAS (SHSD) TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD) TRAFFIC ENGINEERING STANDARD SHEETS

> SHEET 1 OF 12 BARRICADE AND CONSTRUCTION GENERAL NOTES AND REQUIREMENTS



ROAD WORK G20-2

Yellow in Buffer

areas betweem recurrent work spaces

PASS R4-1 24" X 30

WORK

AHEAD

PASS If applicable

ROAD WORK G20-

CARE R4-2 24" X 30"

I иот I

ROAD WORK

AHEAD

PASS R4

CW20-1D AHEAD 48" X 48" (Flags-

R4-2 24" X 30" CARE

G20-2 48" X 24" ROAD WORK

f applicable

ROAD WORK

PASS

Shadow Vehicle with TMA and high intensity rotating, flashing, oscillating or strobe lights. (See notes 7 & 8)

CW13-1P 24" X 24" MPH

R4-2 24" X 30" CARE

If applicable

CW1-6aT

CW20-1D AHEAD 48" X 48" (Flags-See note 1)

Z Type 3 Barricade ■ ■ Channelizing Device:

L=Length of Taper(FT) W=Width of Offset(FT) S=Posted Speed(MPH

TYPICAL USAGE

Flags attached to signs where shown, are REQUIRED.

All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans,

markings may remain in place. Channelizing devices shall be used to separate traffic. Flagger control should NOT be used unless roadway conditions or heavy traffic

volume require additional emphasis to safely control traffic. Flagger should be positioned at end of traffic queue.

The R4-1 "DO NOT PASS," R4-2 " PASS WITH CARE" and construction

or for routine maintenance work, when approved by the Engineer. When work space will be in place less than three days existing povement

regulatory speed zone signs may be installed within CW20-ID "ROAD WORK AHEAD" signs. Proper spacing of signs shall be maintained.

A Snadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely

affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain

Conflicting pavement markings shall be removed for long-term projects.

For shorter durations where traffic is directed over a yellow centerline, channelizing devices which separate two-way traffic should be spaced on

tapers at 20' or 15' if posted speeds are 35 mph or slower, and for tangent sections, at 1/2(S) where S is the speed in mph. This tighter device spacing is intended for the area of the conflicting markings, not the entire work zone.

Texas Department of Transportation

TRAFFIC CONTROL PLAN

TRAFFIC SHIFTS ON

TWO-LANE ROADS

in place, Type 3 Barricades or other channelizing devices may be substituted.

Additional Shadow Vehicles with TMAs may be positioned off the paved surface

Conflicting pavement marking shall be removed for long term projects.

next to those shown in order to protect a wider work space.

TCP (2-3a)

Heavy Work Vehicle

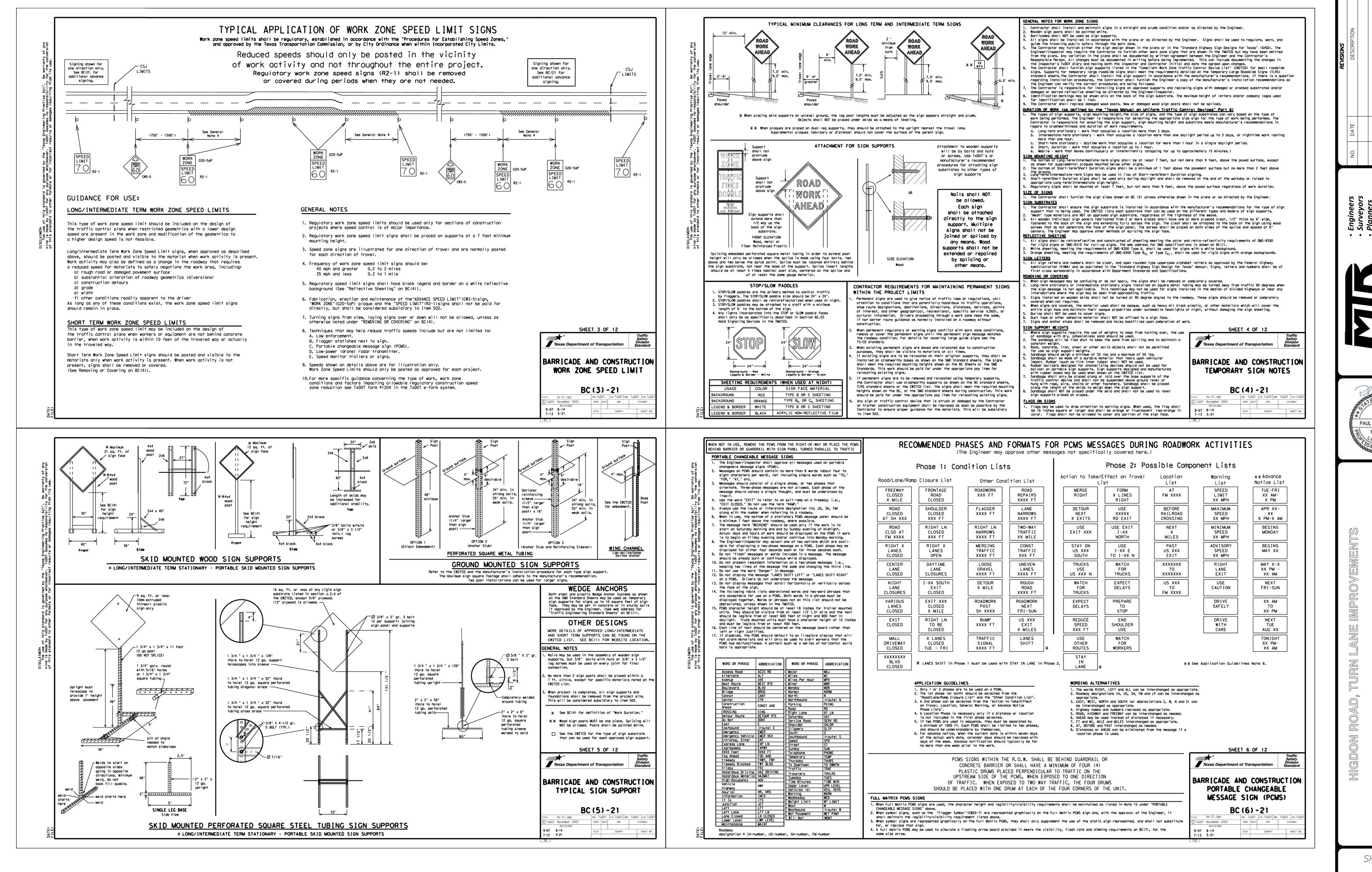
* Conventional Roads Only

** Taper lengths have been rounded off.

Attenuator (TMA)

Raised Pavement
Markers Ty II-AA

Traffic Flow

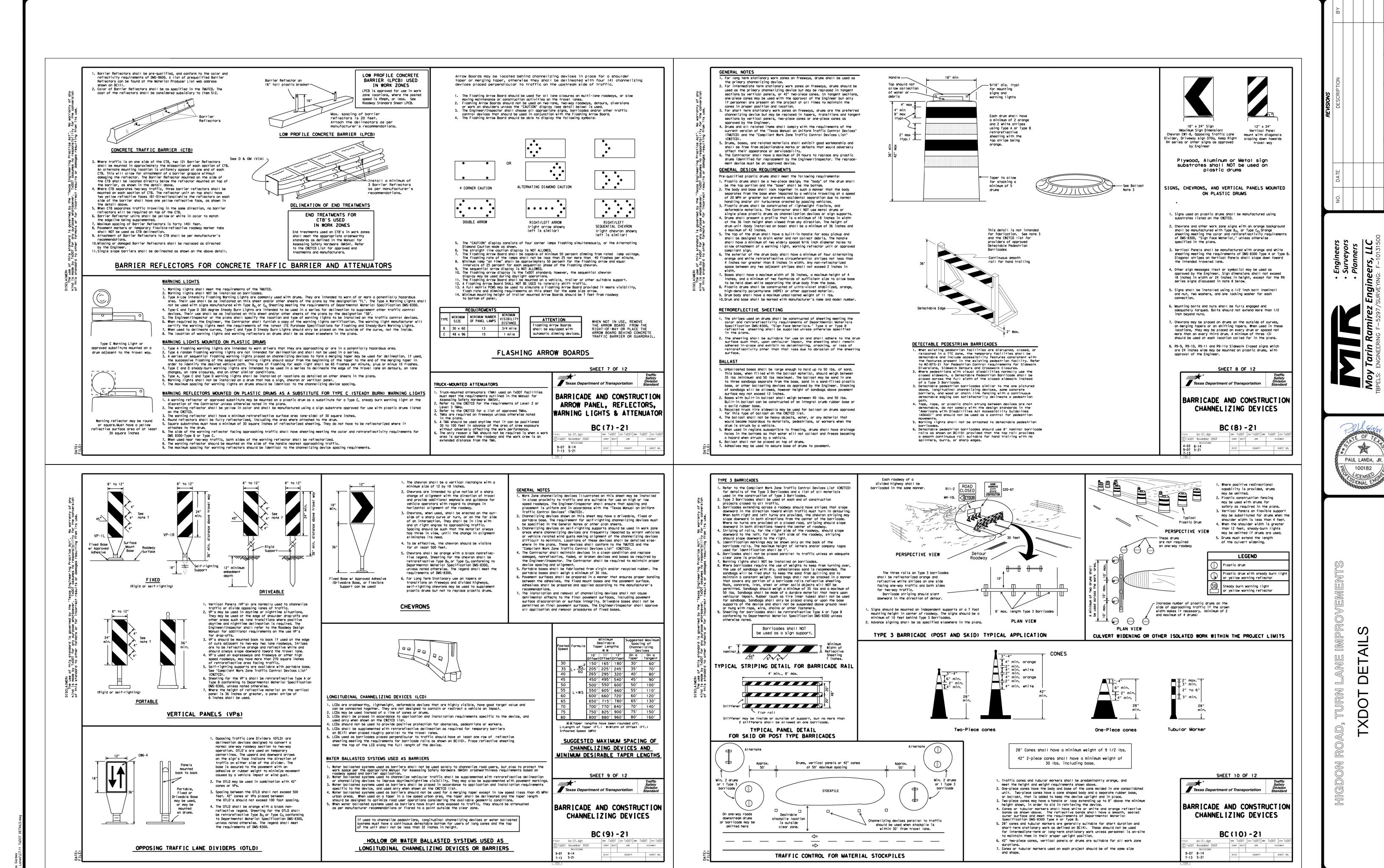


C7.12

SHEET

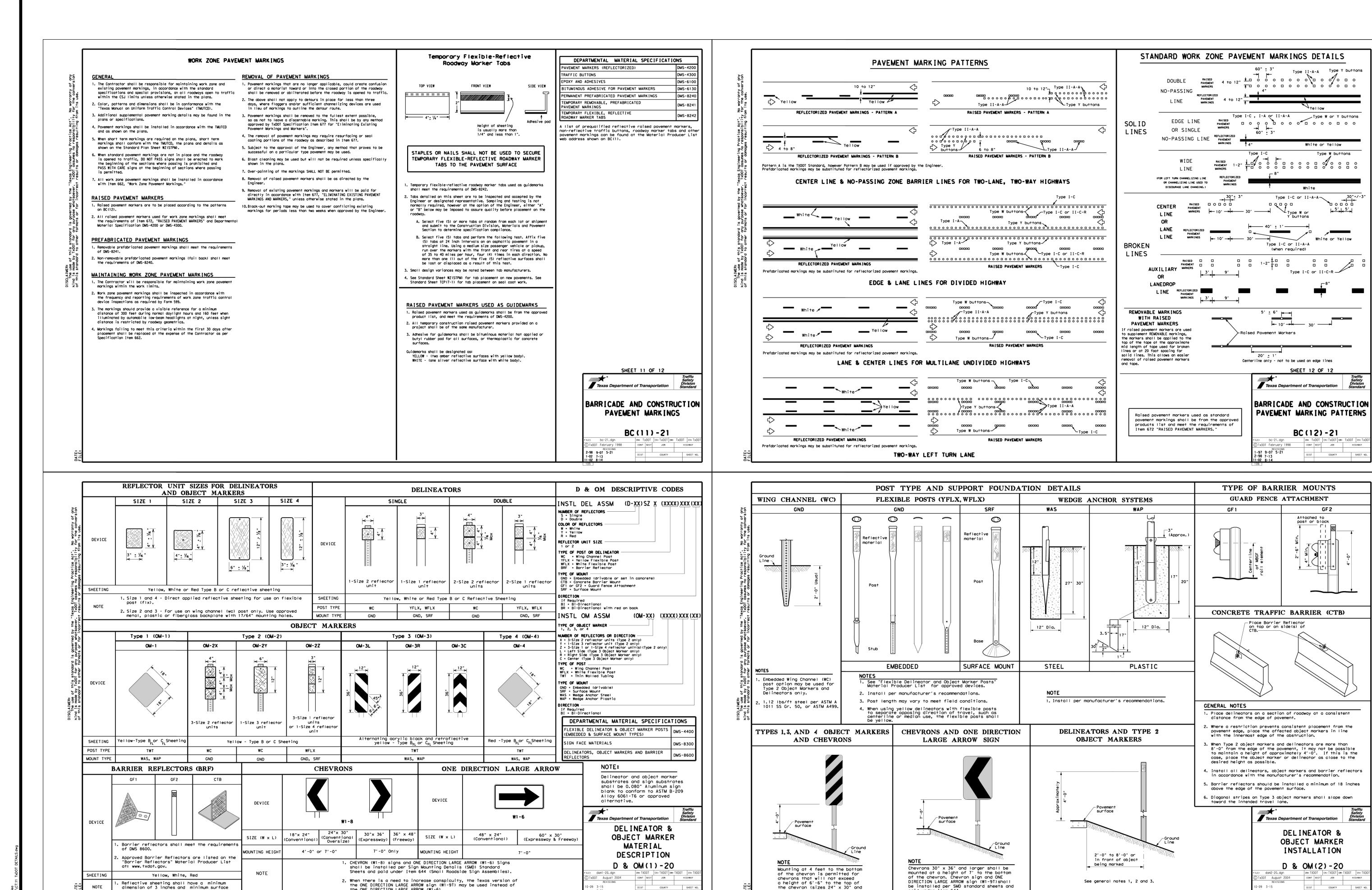
SUBMITTAL SET

PAUL LANDA, JR. 100182



SHEET

SUBMITTAL SET



the ONE DIRECTION LARGE ARROW (W1-6).

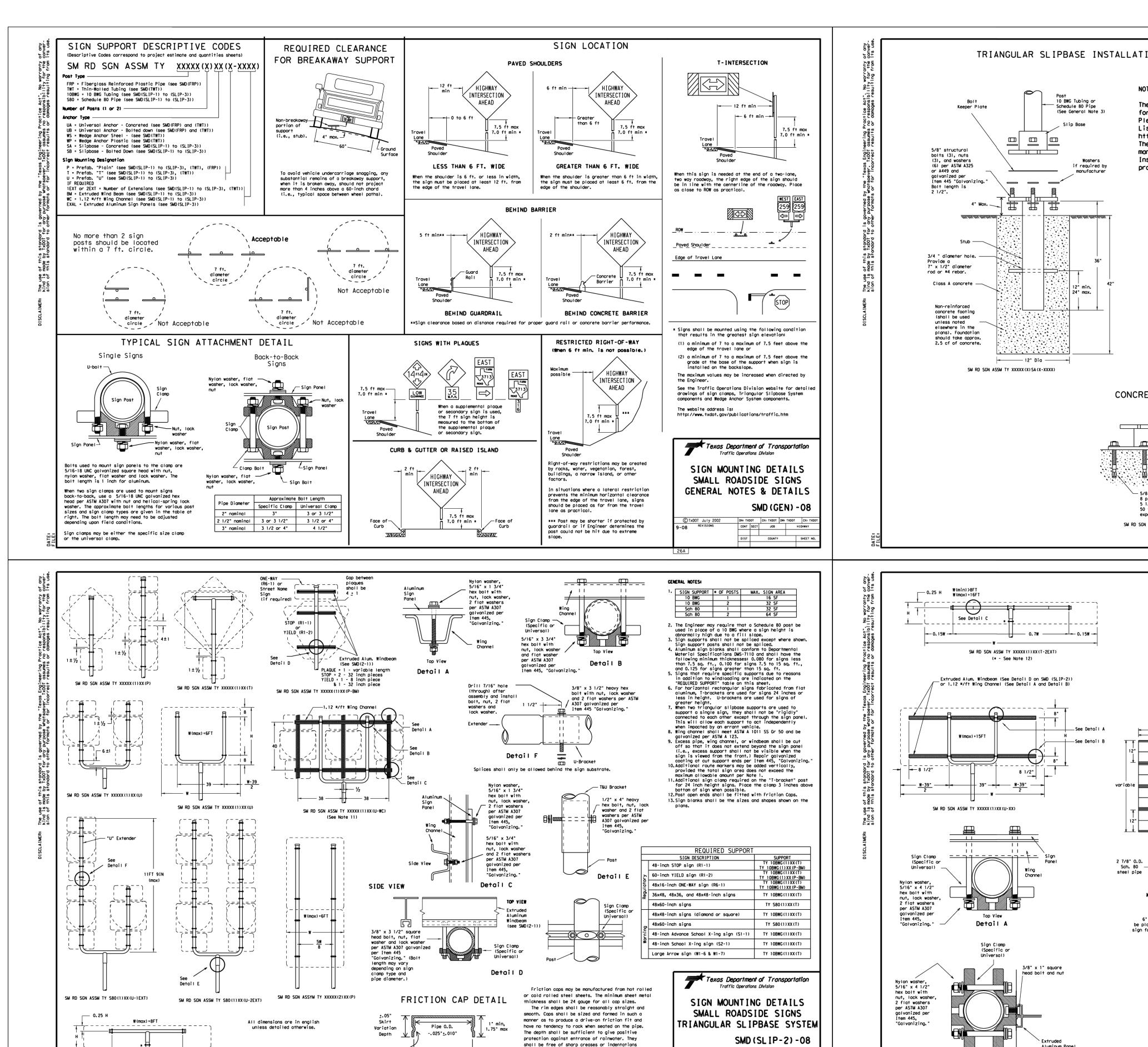
the chevron (sizes 24" x 30" and

be installed per SMD standard sheets and

. . .



O X



and show no evidence of metal fracture.

B633 Class FE/ZN 8.

Caps shall have an electrodeposited coating of

zinc in accordance with the requirements of ASTM

CONT SECT JOB HIGHNAY

DIST COUNTY SHEET NO.

SM RD SGN ASSM TY XXXXX(1)XX(T)

(* - See Note 12)

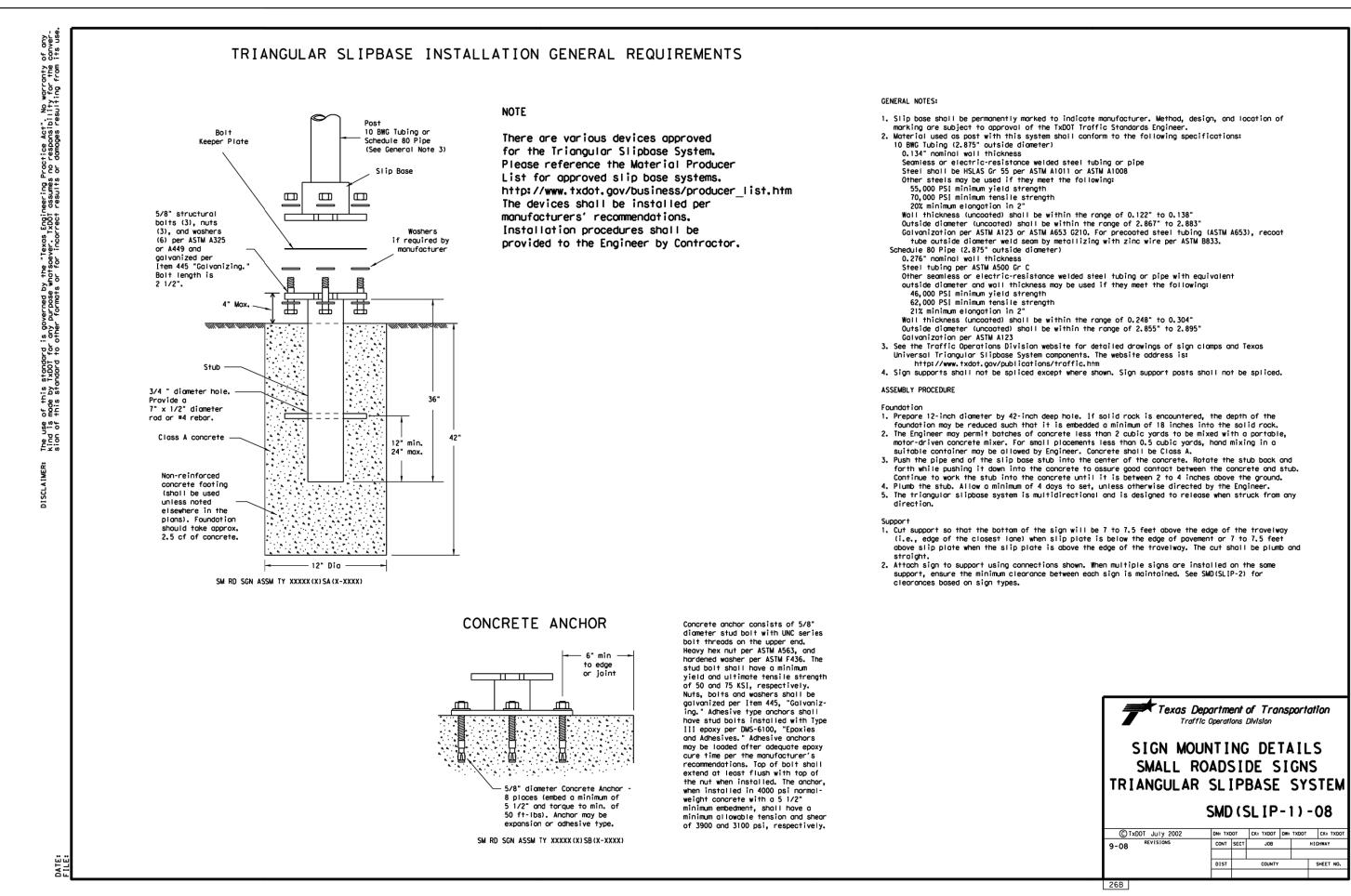
engage pipe 0.D.

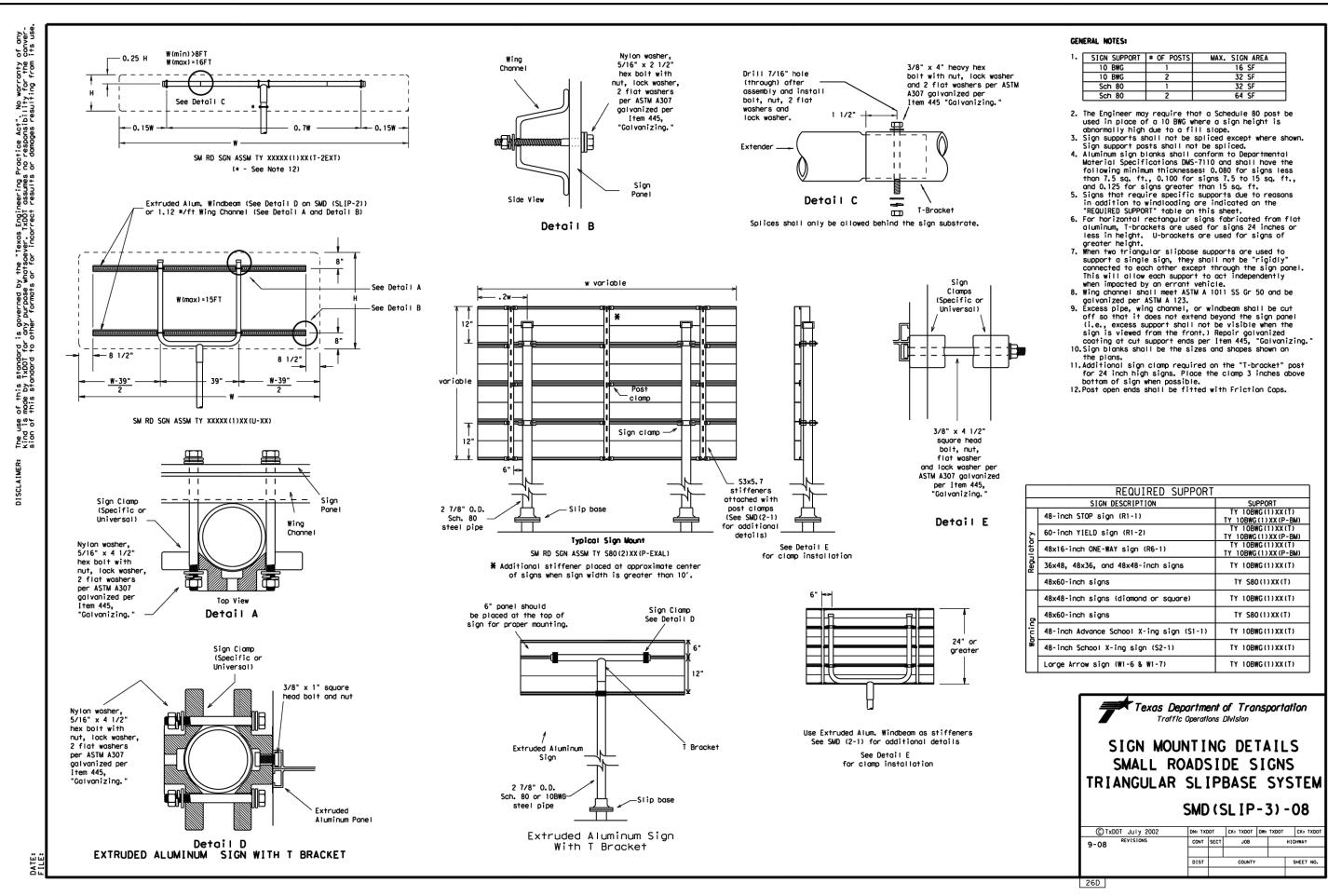
Pipe O.D.

+. 025" ±. 010"

← 0.2W →

0.6W -- 0.2W -





ROAD, TURN LANE
TXDOT DETA

. . .

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PAUL LANDA, JR.
100182

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TXD

Texas Department of Transportation SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS

Prequalification procedures are obtained by writings Texas Department of Transportation

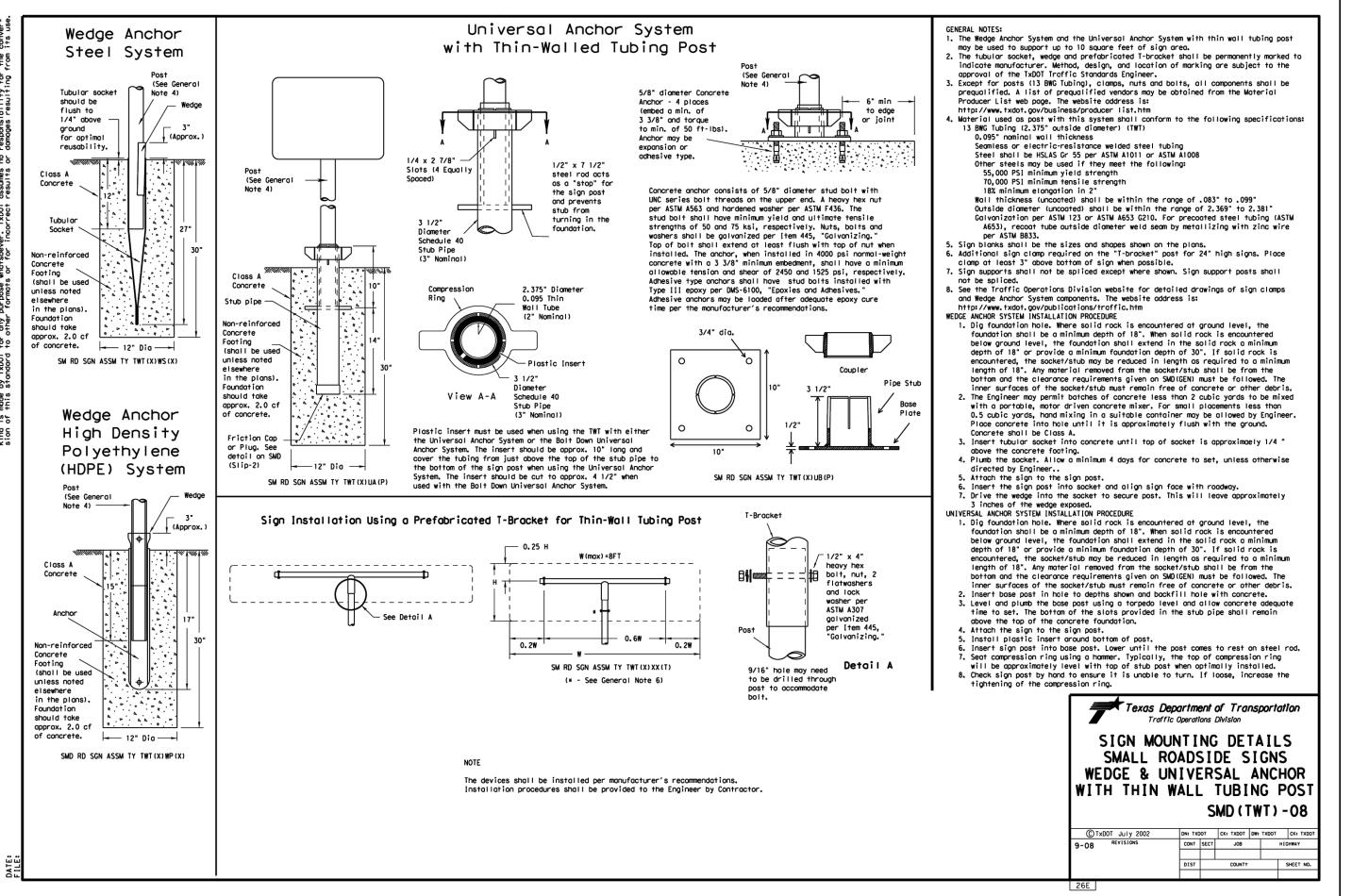
Bottom of base post slots shall be above the concrete footing.

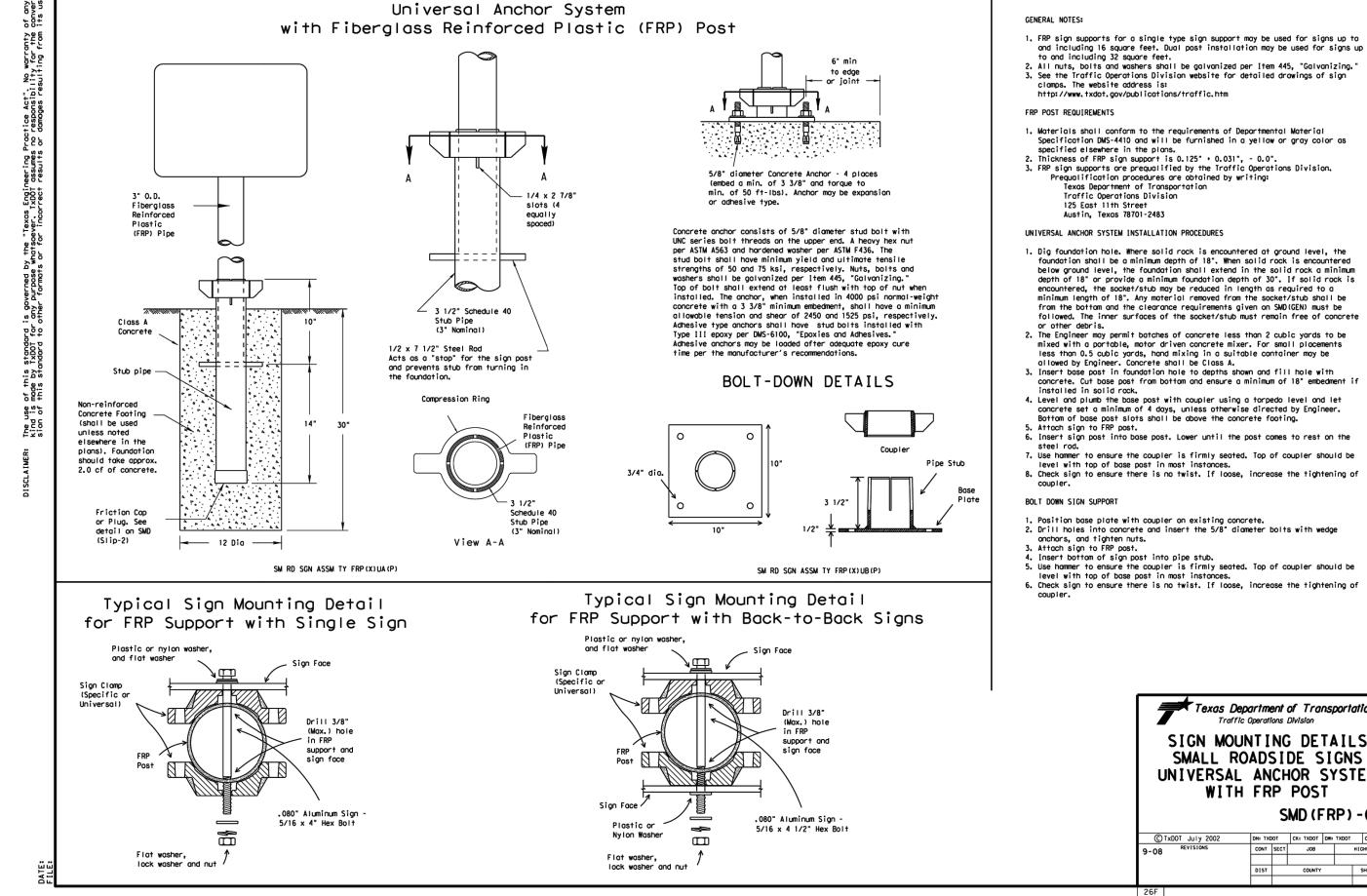
Traffic Operations Division

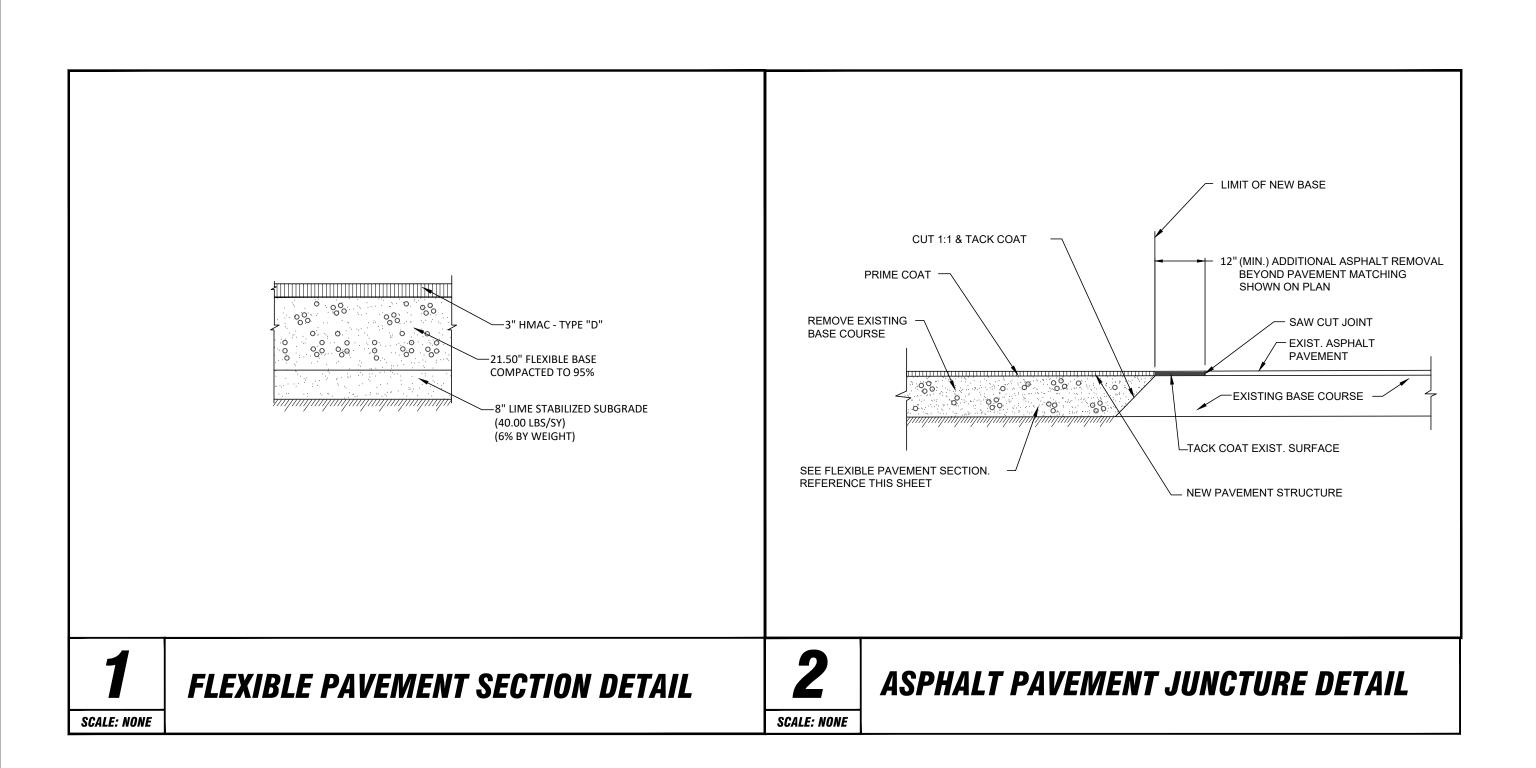
125 East 11th Street Austin, Texas 78701-2483

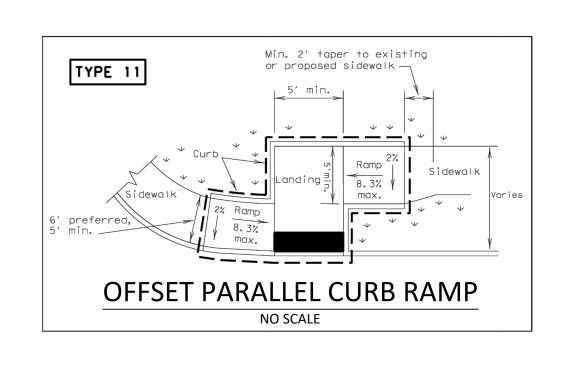
UNIVERSAL ANCHOR SYSTEM WITH FRP POST

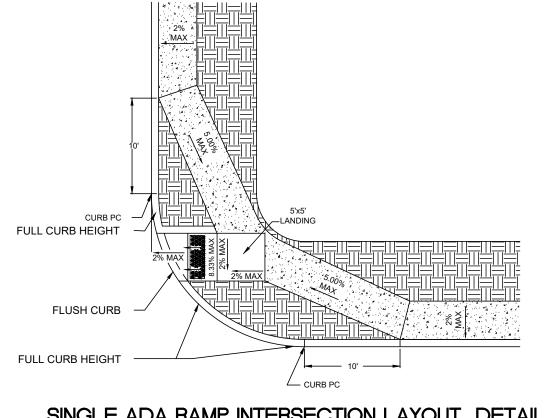
SMD (FRP) -08











SINGLE ADA RAMP INTERSECTION LAYOUT DETAIL (TYPE 10 DIRECTIONAL RAMPS)

SHEET

DETAIL

STANDARD

Engin