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AND MAINTAINED BY ARWSC

INDEX

SHEET TITLE	SHEET NO.
COVER SHEET	0.0
GENERAL NOTES	1.0
SHEET INDEX & CONTROL	2.0
WATER LINE STA. 1+00 - 19+50	3.0
WATER LINE STA. 19+50 - 49+50	3.1
WATER LINE STA. 49+50 - END	3.2
WATER DETAILS - 1 OF 2	4.0
WATER DETAILS - 2 OF 2	4.1
PAVEMENT & DRIVEWAY REPAIR INDEX	5.0
PAVEMENT & DRIVEWAY REPAIR STA. 1+00 - 19+50	5.1
PAVEMENT & DRIVEWAY REPAIR STA. 19+50 - 49+50	5.2
PAVEMENT & DRIVEWAY REPAIR STA. 49+50 - END	5.3
SW3P SHEET INDEX	6.0
SW3P STA. 1+00 - 19+50	6.1
SW3P STA. 19+50 - 49+50	6.2
SW3P STA. 49+50 - END	6.3
SW3P DETAILS	6.4
TRAFFIC CONTROL PLAN NARRATIVE	7.0
TRAFFIC CONTROL NOTES	7.1
TRAFFIC CONTROL PLAN STANDARDS	7.2
TRAFFIC CONTROL PLAN STANDARDS	7.3
TRAFFIC CONTROL PLAN STANDARDS	7.4
TRAFFIC CONTROL PLAN STANDARDS	7.5
TRAFFIC CONTROL PLAN STANDARDS	7.6
TRAFFIC CONTROL PLAN STANDARDS	7.7
TRAFFIC CONTROL PLAN STANDARDS	7.8
TRAFFIC CONTROL PLAN STANDARDS	7.9
TRAFFIC CONTROL PLAN STANDARDS	7.10

FW **ENGINEERS + SURVEYING** 3921 Paesanos Pkwy., Suite 200, San Antonio, TX 78232 Phone #: (210) 979-8444 • Fax #: (210) 979-8441 TBPE Firm #: 9513 • TBPLS Firm #: 10122300

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SHEET 0.0

SAWS STANDARD GENERAL CONSTRUCTION NOTES ASSOCIATED WITH 2021 SAWS STANDARD SPECS UPDATED DECEMBER 14, 2021

GENERAL CONSTRUCTION

- 1. ALL MATERIALS AND CONSTRUCTION PROCEDURES WITHIN THE SCOPE OF THIS CONTRACT SHALL BE APPROVED BY THE SAN ANTONIO WATER SYSTEM (SAWS) AND COMPLY WITH THE PLANS, SPECIFICATIONS, GENERAL CONDITIONS AND WITH THE FOLLOWING AS APPLICABLE:
- A. CURRENT TEXAS COMMISSION ON ENVIRONMENTAL QUALITY (TCEQ) "DESIGN CRITERIA FOR DOMESTIC WASTEWATER SYSTEM", TEXAS ADMINISTRATIVE CODE (TAC) TITLE 30 PART 1 CHAPTER 217 AND "PUBLIC DRINKING WATER", TAC TITLE 30 PART 1 CHAPTER 290.
- B. CURRENT TXDOT "STANDARD SPECIFICATIONS FOR CONSTRUCTION OF HIGHWAYS, STREETS AND DRAINAGE."
- C. CURRENT "SAN ANTONIO WATER SYSTEM STANDARD SPECIFICATIONS FOR WATER AND SANITARY SEWER CONSTRUCTION."
- D. CURRENT CITY OF SAN ANTONIO "STANDARD SPECIFICATIONS FOR CONSTRUCTION.
- E. CURRENT CITY OF SAN ANTONIO "UTILITY EXCAVATION CRITERIA MANUAL" (UECM).
- 2. THE CONTRACTOR SHALL OBTAIN SAWS STANDARD DETAILS FROM SAWS WEBSITE, <u>HTTPS://APPS.SAWS.ORG/BUSINESS_CENTER/SPECS/CONSTSPECS/</u> UNLESS OTHERWISE NOTED WITHIN DESIGN PLANS.
- 3. THE CONTRACTOR IS TO NOTIFY AND MAKE ARRANGEMENTS WITH THE ARWSC CONSTRUCTION INSPECTION DIVISION AT 210-622-3901 (DURING REGULAR ARWSC WORKING HOURS) AND PROVIDE NOTIFICATION PROCEDURES THE CONTRACTOR WILL USE TO NOTIFY AFFECTED HOME RESIDENTS AND/OR PROPERTY OWNERS TWO (2) WEEKS PRIOR TO EXCAVATION.
- 4. LOCATIONS AND DEPTHS OF EXISTING UTILITIES AND SERVICE LATERALS SHOWN ON THE PLANS ARE UNDERSTOOD TO BE APPROXIMATE. ACTUAL LOCATIONS AND DEPTHS MUST BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE UTILITY SERVICE LINES AS REQUIRED FOR CONSTRUCTION AND TO PROTECT THEM DURING CONSTRUCTION AT NO COST TO ARWSC.
- 5. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF UNDERGROUND UTILITIES AND DRAINAGE STRUCTURES PRIOR TO CONSTRUCTION WHETHER SHOWN ON PLANS OR NOT. CONTRACTOR SHALL COORDINATE PHYSICAL LOCATES FOR ARWSC INFRASTRUCTURE THROUGH THE ARWSC INSPECTOR. PLEASE ALLOW UP TO 7 BUSINESS DAYS FOR LOCATES REQUESTING PIPE LOCATION MARKERS ON ARWSC INFRASTRUCTURE. THE FOLLOWING CONTACT INFORMATION ARE SUPPLIED FOR VERIFICATION PURPOSES:
 - TEXAS STATEWIDE ONE CALL LOCATOR 1-800-545-6005 OR 811
- 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING EXISTING FENCES, CURBS, STREETS, DRIVEWAYS, SIDEWALKS, LANDSCAPING, AND STRUCTURES TO ITS ORIGINAL OR BETTER CONDITION AS A RESULT OF DAMAGES DONE BY THE PROJECT'S CONSTRUCTION.
- 7. CONTRACTOR SHALL NOT MAKE USE OF DUMPSTERS OR WASTE BINS THAT ARE INTENDED TO SERVE RESIDENTS AND/OR BUSINESSES.
- 8. ALL WORK IN BEXAR COUNTY RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH RESPECTIVE CONSTRUCTION SPECIFICATIONS AND PERMIT.
- 9. THE CONTRACTOR SHALL COMPLY WITH CITY OF SAN ANTONIO OR OTHER GOVERNING MUNICIPALITY'S TREE ORDINANCES WHEN EXCAVATING NEAR TREES.
- 10. ALL WORK WITHIN THE 100-YEAR FLOODPLAIN SHALL BE DONE IN ACCORDANCE WITH FLOODPLAIN DEVELOPMENT PERMIT.
- 11. ANY WORK COMPLETED WITHOUT PRIOR WRITTEN AUTHORIZATION WHICH IS NOT INCLUDED IN THESE PLANS AND SPECIFICATIONS WILL NOT BE COMPENSATED BY ARWSC
- 12. HOLIDAY WORK: CONTRACTORS WILL NOT BE ALLOWED TO PERFORM ARWSC WORK ON ARWSC RECOGNIZED HOLIDAYS.
- WEEKEND WORK: NONE.
- 13. PRE-CON SITE VIDEO: BEFORE THE START OF ANY CONSTRUCTION. THE SITE MUST BE VIDEO RECORDED BY THE CONTRACTOR WITH ONE COPY SUBMITTED TO ARWSC INSPECTIONS. A PRE-SITE VIDEO WILL PROVIDE ACCURATE DOCUMENTATION OF THE EXISTING CONDITIONS (NSPI).
- 14. POWER POLE BRACING: CONTRACTORS SHOULD BE ADVISED THAT THERE ARE EXISTING OVERHEAD UTILITY POLES ALONG THE PROJECT CORRIDOR. CONTRACTORS SHOULD FURTHER BE ADVISED THAT IF THE DISTANCE FROM THE OUTSIDE FACE OF A UTILITY TRENCH TO THE FACE OF A UTILITY POLE IS LESS THAN 5 FEET, SAID UTILITY POLE IS SUBJECT TO BRACING, BASED ON A DETERMINATION MADE BY UTILITY POLE OWNER. COSTS INCURRED BY CONTRACTOR FOR BRACING OF THESE UTILITY POLES IS SUBSIDIARY TO THAT RESPECTIVE UTILITY COMPANY'S WORK. IT IS ADVISABLE FOR THE CONTRACTOR TO REVIEW THE CONSTRUCTION DOCUMENTS AND VISIT THE CONSTRUCTION SITE TO DETERMINE POTENTIAL IMPACTS.
- 15. CONSTRUCTION SEQUENCING: IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO SCHEDULE SEQUENCING FOR REMOVAL AND INSTALLATION OF EXISTING AND PROPOSED ARWSC UTILITIES IN CONJUNCTION WITH GENERAL PROJECT CONSTRUCTION. SEQUENCE OF CONSTRUCTION ACTIVITIES SHALL BE CONSIDERED IN ORDER TO MINIMIZE THE EXTENT AND DURATION OF DISTURBANCES
- 16. CONTRACTOR SHALL COMPLY WITH APPLICABLE REGULATIONS INCLUDING, BUT NOT LIMITED TO, THOSE OVERSEEN BY THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA). OSHA INFORMATION AND RELATED MATERIALS MAY BE OBTAINED AT HTTPS://WWW.OSHA.GOV/ OR AT THE OSHA SAN ANTONIO OFFICE LOCATED AT FOUNTAINHEAD TOWER, SUITE 605 8200 W. INTERSTATE 10 SAN ANTONIO, TX 78230 WHICH IS ALSO REACHABLE BY PHONE AT (210) 472-5040.
- 17. TRENCH EXCAVATION SAFETY PROTECTION: CONTRACTOR AND/OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR STRUCTURAL DESIGN/GEOTECHNICAL/SAFETY/EQUIPMENT CONSULTANT, IF ANY, SHALL REVIEW THESE PLANS AND AVAILABLE GEOTECHNICAL INFORMATION AND THE ANTICIPATED INSTALLATION SITE(S) WITHIN THE PROJECT WORK AREAS IN ORDER TO IMPLEMENT CONTRACTOR'S TRENCH EXCAVATION SAFETY PROTECTION SYSTEMS, PROGRAMS AND/OR PROCEDURES. THE CONTRACTOR'S IMPLEMENTATION OF THE SYSTEMS, PROGRAMS AND/OR PROCEDURES SHALL PROVIDE FOR ADEQUATE TRENCH EXCAVATION SAFETY PROTECTION THAT COMPLIES WITH, AS A MINIMUM, OSHA STANDARDS FOR TRENCH EXCAVATIONS. SPECIFICALLY, CONTRACTOR AND/OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR SAFETY CONSULTANT SHALL IMPLEMENT A TRENCH SAFETY PROGRAM IN ACCORDANCE WITH OSHA STANDARDS GOVERNING THE PRESENCE AND ACTIVITIES OF INDIVIDUALS WORKING IN AND AROUND TRENCH EXCAVATION.

WATER

- 18. PRIOR TO TIE-INS, ANY SHUTDOWNS OF EXISTING MAINS OF ANY SIZE MUST BE COORDINATED WITH THE ARWSC INSPECTION AND/OR ARWSC PRODUCTION GROUPS AT LEAST TWO WEEKS OR MORE IN ADVANCE OF THE SHUTDOWN. THE CONTRACTOR MUST ALSO PROVIDE A SEQUENCE OF WORK AS RELATED TO THE TIE-INS; THIS IS AT NO ADDITIONAL COST TO ARWSC OR THE PROJECT AND IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO SEQUENCE THE WORK ACCORDINGI Y
- 19. ASBESTOS CEMENT (AC) PIPE, ALSO KNOWN AS TRANSITE PIPE WHICH IS KNOWN TO CONTAIN ASBESTOS-CONTAINING MATERIAL (ACM), MAYBE LOCATED WITHIN THE PROJECT LIMITS. SPECIAL WASTE MANAGEMENT PROCEDURES AND HEALTH AND SAFETY REQUIREMENTS WILL BE APPLICABLE WHEN REMOVAL AND/OR DISTURBANCE OF THIS PIPE OCCURS. PAYMENT FOR SUCH WORK IS TO BE MADE UNDER ITEM NO. 3000. "HANDLING ASBESTOS CEMENT PIPE".
- AC PIPE REMOVED ON CONSTRUCTION PROJECTS FOR TIE-IN(S) SHOULD BE IN LENGTH OF 26 LINEAR FEET (LF). LENGTHS OF 13 LF SHOULD BE REMOVED WHERE AC PIPE IS BEING REMOVED AND CROSSING PIPES, CONDUITS, OR BOXES.
- 20. VALVE REMOVAL: WHERE THE CONTRACTOR IS TO ABANDON A WATER MAIN, THE CONTROL VALVE LOCATED ON THE ABANDONING BRANCH WILL BE REMOVED AND REPLACED WITH A CAP/PLUG. (NSPI)
- 21. DIVISION VALVES: DIVISION VALVES SHOWN ON PLANS OR NOT SHOWN ON PLANS BUT FOUND IN THE FIELD SHALL ONLY BE OPERATED BY ARWSC DISTRIBUTION AND COLLECTION STAFF AND ONLY WITH PRIOR WRITTEN APPROVAL OF THE ARWSC DIRECTOR OF PRODUCTION AND OPERATIONS AND PROPER COORDINATION WITH ALL ARWSC DEPARTMENTS. CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO THE INSPECTOR A MINIMUM OF TWO WEEKS IN ADVANCE TO START THE COORDINATION PROCESS AND WILL BE INFORMED BY THE INSPECTOR WHEN THE DIVISION VALVE WILL BE OPERATED BY THE ARWSC DISTRIBUTION AND COLLECTION STAFF. THE DIVISION VALVE CAN ONLY BE OPERATED BY ARWSC DISTRIBUTION AND COLLECTION STAFF MEMBER NOT THE INSPECTOR OR THE CONTRACTOR. OPERATION OF A DIVISION VALVE WITHOUT THE EXPRESS PRIOR WRITTEN APPROVAL OF THE ARWSC DISTRIBUTION AND COLLECTION STAFF WILL CONSTITUTE A MATERIAL BREACH OF ANY WRITTEN ARWSC CONTRACT OR PERMIT IN ADDITION TO SUBJECTING THE CONTRACTOR TO LIABILITY FOR ANY AND ALL FINES, FEES, OR OTHER DAMAGES, DIRECT OR CONSEQUENTIAL, THAT MAY ARISE FROM OR BE CAUSED BY THE OPERATION OF THE VALVE WITHOUT PRIOR WRITTEN PERMISSION. PLEASE BE INFORMED THAT THE APPROVAL OF THE OPERATION OR OPENING OR CLOSING OF A DIVISION VALVE CAN TAKE SEVERAL WEEKS FOR APPROVAL.

TRENCH EXCAVATION SAFETY PROTECTION

CONTRACTOR AND/ OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR STRUCTURAL DESIGN/GEOTECHNICAL/SAFETY/EQUIPMENT CONSULTANT, IF ANY, SHALL REVIEW THESE PLANS AND ANY AVAILABLE GEOTECHNICAL INFORMATION AND THE ANTICIPATED INSTALLATION SITES WITHIN THE PROJECT WORK AREA IN ORDER TO IMPLEMENT CONTRACTOR'S TRENCH EXCAVATION SAFETY PROTECTION SYSTEMS, PROGRAMS AND/OR PROCEDURES FOR THE PROJECT DESCRIBED IN THE CONTRACT DOCUMENTS. THE CONTRACTOR'S IMPLEMENTATION OF THESE SYSTEMS, PROGRAMS AND/OR PROCEDURES SHALL PROVIDE FOR ADEQUATE TRENCH EXCAVATION SAFETY PROTECTION THAT COMPLY WITH AS A MINIMUM, OSHA STANDARDS FOR TRENCH EXCAVATIONS. SPECIFICALLY, CONTRACTOR AND/OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR SAFETY CONSULTANT SHALL IMPLEMENT A TRENCH SAFETY PROGRAM IN ACCORDANCE WITH OSHA STANDARDS GOVERNING THE PRESENCE AND ACTIVITIES OF INDIVIDUALS WORKING IN AND AROUND TRENCH EXCAVATION

WATER/SEWER CROSSING NOTE:

ALL WATER/SEWER CROSSINGS SHALL COMPLY WITH 30 TAC 217.53(d) AND 290.44(e) AS PER SAWS SPECIFICATION ITEM 812.

WATER PLAN NOTES 1. ALL PVC PIPE 16" TO BE C-909 CLASS 165.

2. ALL VALVES SHALL READ "OPEN LEFT". 3. CONTRACTOR TO COORDINATE POWER POLE BRACING (IF NECESSARY) WITH CPS ENERGY. 4. CONTRACTOR SHALL INSTALL THE NEW MAIN WITH A MINIMUM 6' COVER BELOW EXISTING GROUND THROUGHOUT THE LENGTH OF THE PROJECT.

FIRE FLOW NOTE

IN AN EFFORT TO MEET THE CITY OF SAN ANTONIO'S FIRE FLOW REQUIREMENTS FOR THE PROPOSED RESIDENTIAL DEVELOPMENT, THE PUBLIC WATER MAIN SYSTEM HAS BEEN DESIGNED FOR A MINIMUM FIRE FLOW DEMAND OF 1,500 GPM AT 25 PSI RESIDUAL PRESSURE. THE FIRE FLOW REQUIREMENTS FOR INDIVIDUAL STRUCTURES WILL BE REVIEWED DURING THE BUILDING PERMIT PROCESS IN ACCORDANCE WITH THE PROCEDURES SET FORTH BY THE CITY OF SAN ANTONIO DIRECTOR OF DEVELOPMENT SERVICES DEPARTMENT AND THE SAN ANTONIO FIRE DEPARTMENT FIRE MARSHAL. JOINT RESTRAINT NOTE:

CONTRACTOR SHALL INSTALL RETAINER GLANDS AT ALL FITTINGS AND PROVIDE JOINT RESTRAINING HARNESS OR FIELD LOCK GASKETS AT ALL JOINTS WITHIN THE LENGTH SHOWN. CONTRACTOR SHALL INSURE THAT ALL TEES, BENDS, VALVES, ETC. HAVE A MINIMUM OF 5 FT OF PIPE WITH NO JOINTS ON EACH SIDE OF THE FITTING. JOINT RESTRAINTS AND RETAINER GLANDS SHALL BE CALCULATED BY SAWS APPROVED PROGRAMS. THERE WILL BE NO SEPARATE PAY ITEM FOR RETAINER GLANDS AND OTHER JOINT RESTRAINING HARNESS AND GASKETS, BUT SHALL BE SUBSIDIARY TO THE UNIT COST PER LINEAL FOOT OF PIPE INSTALLED.

 \otimes = ENTIRE LENGTH OF PIPE TO BE RESTRAINED

COMPACTION NOTE

HE CONTRACTOR SHALL BE RESPONSIBLE FOR MEETING 98% COMPACTION ON ALL TRENCH BACKFILL AND PAYING FOR THE TESTS TO BE PERFORMED BY A THIRD PARTY. COMPACTION TESTS WILL BE DONE AT ONE LOCATION POINT RANDOMLY SELECTED OR AS INDICATED BY THE ARWSC INSPECTOR/TEST ADMINISTRATOR, PER EACH 12 INCH LOOSE LIFT PER 400 LINEAR FEET AT A MINIMUM. PERMITS AND/OR WILL NOT BE ACCEPTED AND FINALIZED BY ARWSC WITHOUT THIS REQUIREMENT BEING MET AND VERIFIED BY PROVIDING ALL NECESSARY DOCUMENTED TEST RESULTS.

CONTRACTOR TO COORDINATE CONSTRUCTION STORAGE AREA AND CONSTRUCTION ENTRANCE WHERE NEEDED (N.S.P.I.).

TRAFFIC CONTROL PLAN NOTES 1. CONTRACTOR IS RESPONSIBLE TO COORDINATE TRAFFIC CONTROL PLAN (N.S.P.I.).

2. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN VEHICULAR AND PEDESTRIAN ACCESS AT ALL TIMES TO LOCAL RESIDENCES AND BUSINESSES.

3. WHEN THE WORK REQUIRES THE EXCAVATION OF THE STREET AND THE REMOVAL OF THE EXISTING DRIVEWAY APPROACHES AND SIDEWALKS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TEMPORARY ALL-WEATHER ACCESS TO THE BUSINESSES AND RESIDENCES. THE TEMPORARY DRIVEWAY APPROACHES SHALL BE CONSTRUCTED WITH FLEXIBLE BASE OR GRAVEL MATERIAL AT NO SEPARATE COST TO THE CITY.

4. PRIOR TO INITIATING THE CONSTRUCTION OF NEW DRIVEWAY APPROACHES, THE CONTRACTOR SHALL GIVE ADVANCE WARNING IN PERSON, OR ON WRITING, OF AT LEAST 48 HOURS TO EACH RESIDENCE THAT WILL BE IMMEDIATELY AFFECTED, SO THAT ALTERNATE PLANS MAT BE MADE BY RESIDENTS.

5. FOR BUSINESSES WITH MORE THAN ONE DRIVEWAY, AT LEAST ONE DRIVEWAY SHALL REMAIN OPEN WHILE THE OTHER NEW DRIVEWAY APPROACHES ARE CONSTRUCTED. FOR BUSINESSES WITH ONLY ONE DRIVEWAY, THE NEW DRIVEWAY APPROACH SHALL BE CONSTRUCTED IN HALF WIDTHS, UNLESS A TEMPORARY ASPHALT DRIVEWAY IS FIRST INSTALLED AT NO SEPARATE COST TO THE CITY.

<u>CAUTION!!:</u> THE CONTRACTOR SHALL BE REQUIRED TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES INCLUDING BUT NOT LIMITED TO: WATER, SEWER, TELEPHONE AND FIBER OPTIC LINES, SITE LIGHTING ELECTRIC, SECONDARY ELECTRIC, PRIMARY ELECTRICAL DUCTBANKS, LANDSCAPE IRRIGATION FACILITIES, AND GAS LINES. ANY UTILITY CONFLICTS THAT ARISE SHOULD BE COMMUNICATED TO THE ENGINEER IMMEDIATELY AND PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL CONTACT 1-800-DIG-TESS A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION. ANY DAMAGE TO EXISTING UTILITIES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND THE REPAIR SHALL BE AT CONTRACTOR'S SOLE EXPENSE WHETHER THE UTILITY IS SHOWN ON THESE PLANS OR NOT.

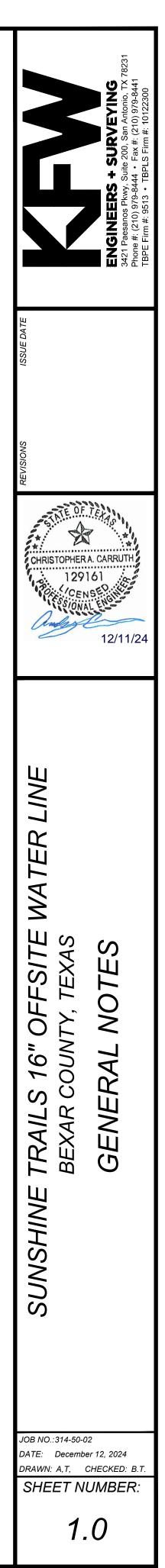
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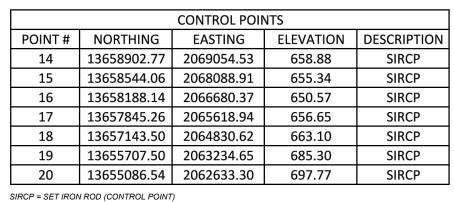
ALL MATERIALS AND CONSTRUCTION PROCEDURES WITHIN THE SCOPE OF THIS CONTRACT SHALL BE IN ACCORDANCE WITH THE SAN ANTONIO WATER SYSTEM (SAWS) STANDARD SPECIFICATIONS

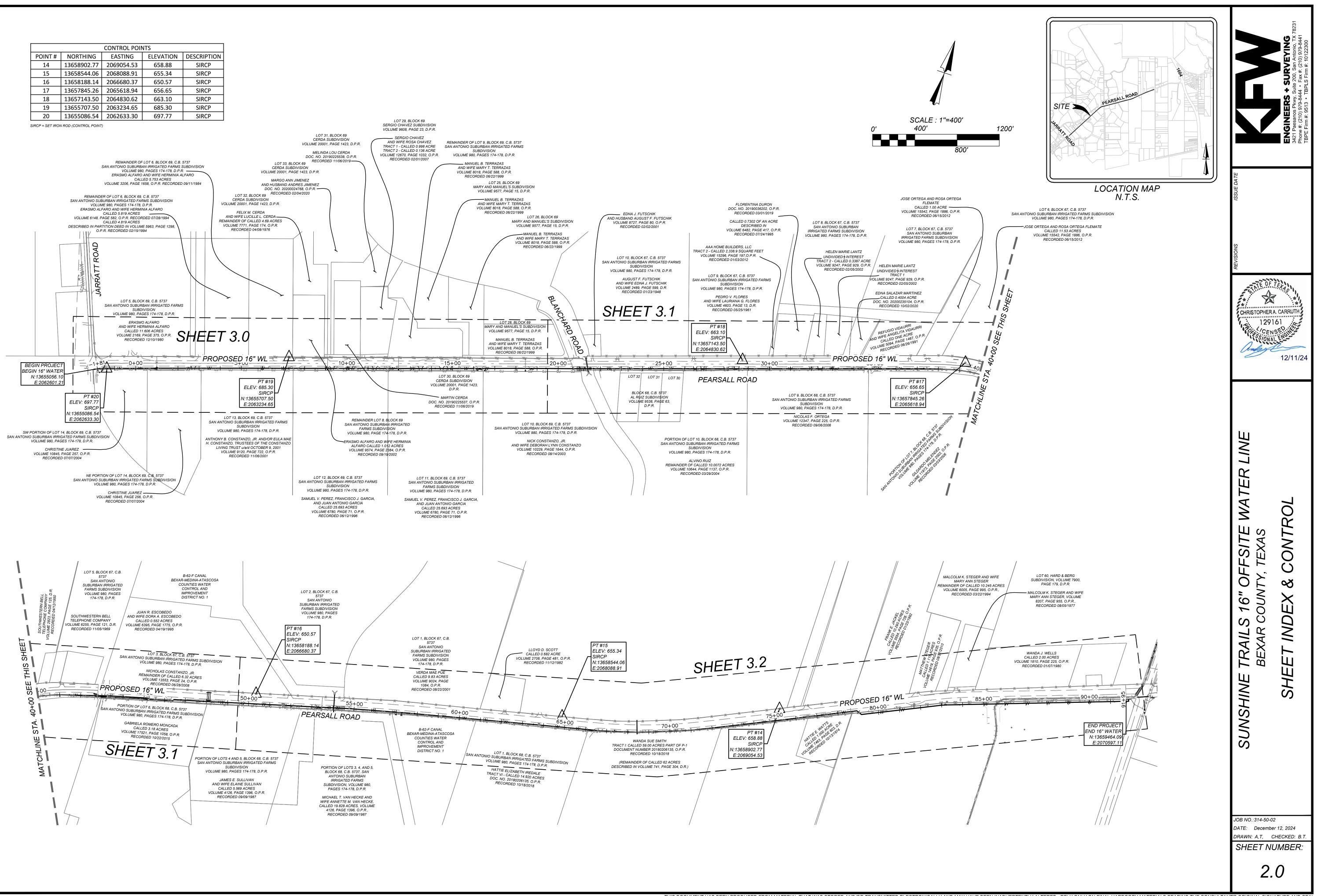
BEXAR COUNTY R.O.W. NOTE A BEXAR COUNTY PERMIT MUST BE OBTAINED BEFORE WORKING IN BEXAR COUNTY R.O.W.

- NECESSARY) WITH CPS ENERGY. POLE BRACING WILL BE CONSIDERED TO BE INCLUDED WITH THE COST OF THE PROJECT.
- BE BRACED BASED ON ACTUAL FIELD DIMENSIONS. IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY LOCATIONS OF POWER POLES IN RELATION TO THE PROPOSED SEWER LINE AND ADD BRACING WHERE REQUIRED (NSPI).
- 3. CONTRACTOR SHALL ENSURE EVERY PROPERTY HAVE ACCESS AT THE END OF THE DAY. UNDER NO CIRCUMSTANCE SHALL ACCESS TO ANY PROPERTY NOT BE ALLOWED OVERNIGHT. ANY DAYTIME DRIVEWAY CLOSURES SHALL BE COORDINATED WITH THE PROPERTY OWNERS PRIOR TO CONSTRUCTION
- 4. TEMPORARY MAILBOXES SHALL BE PROVIDED FOR ALL PROPERTIES THAT HAVE A MAILBOX SHOWN AS TO BE REMOVED AND REPLACED. CONTRACTOR TO MAINTAIN ACCESS TO GARBAGE SERVICES AND THE TEMPORARY MAILBOX AT ALL TIMES UNTIL THE REPLACEMENT MAILBOX IS INSTALLED AND ACCESSIBLE.

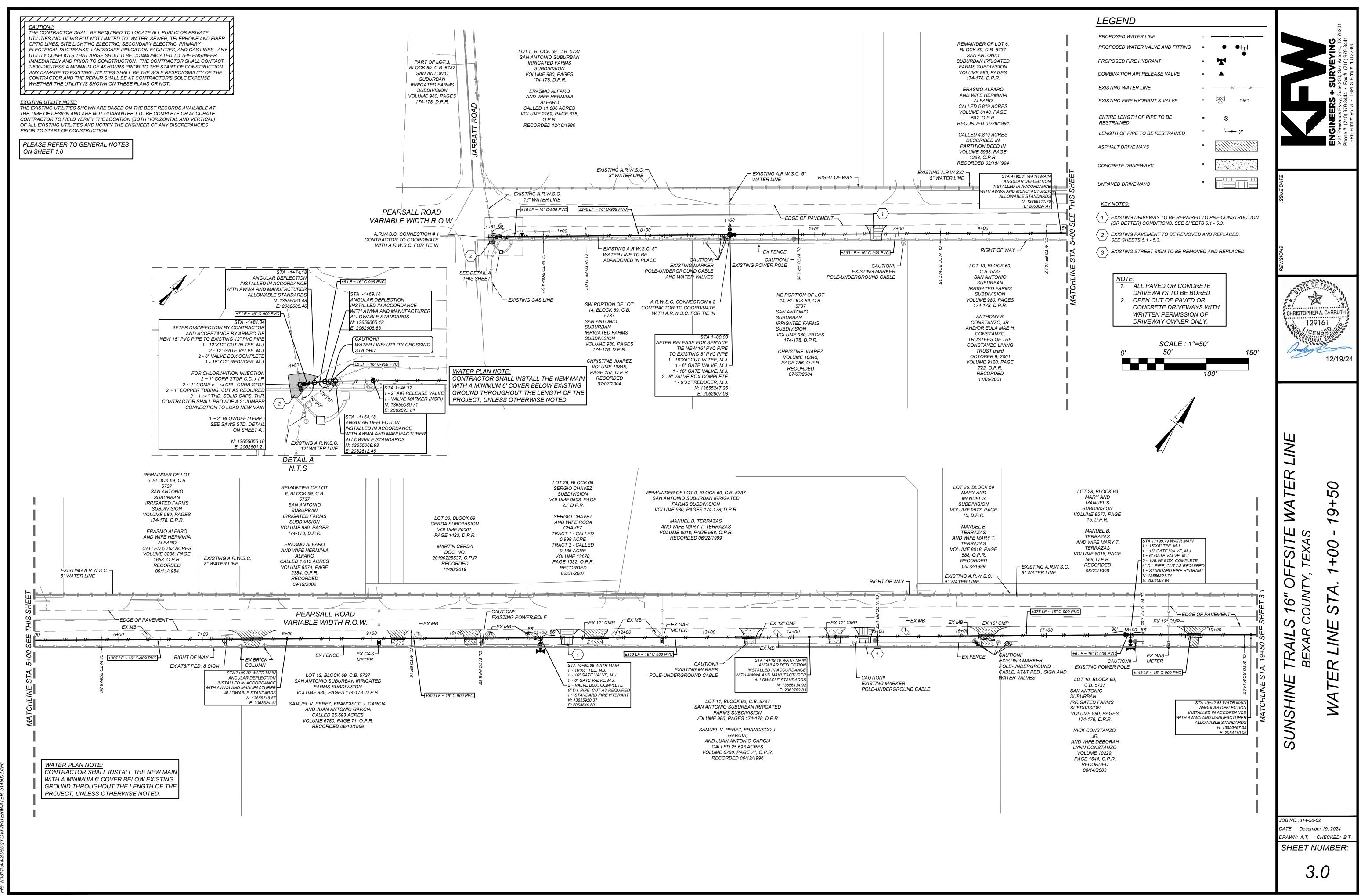
WHERE EXCAVATION IS WITHIN 5' OF A UTILITY POLE. THE POLE SHALL BE BRACED. CONTRACTOR TO COORDINATE POWER POLE BRACING (IF 2. DIMENSIONS SHOWN ON PLANS BETWEEN SEWER LINE AND POWER POLE SHOULD BE VERIFIED BY CONTRACTOR. MORE POLES MAY NEED TO



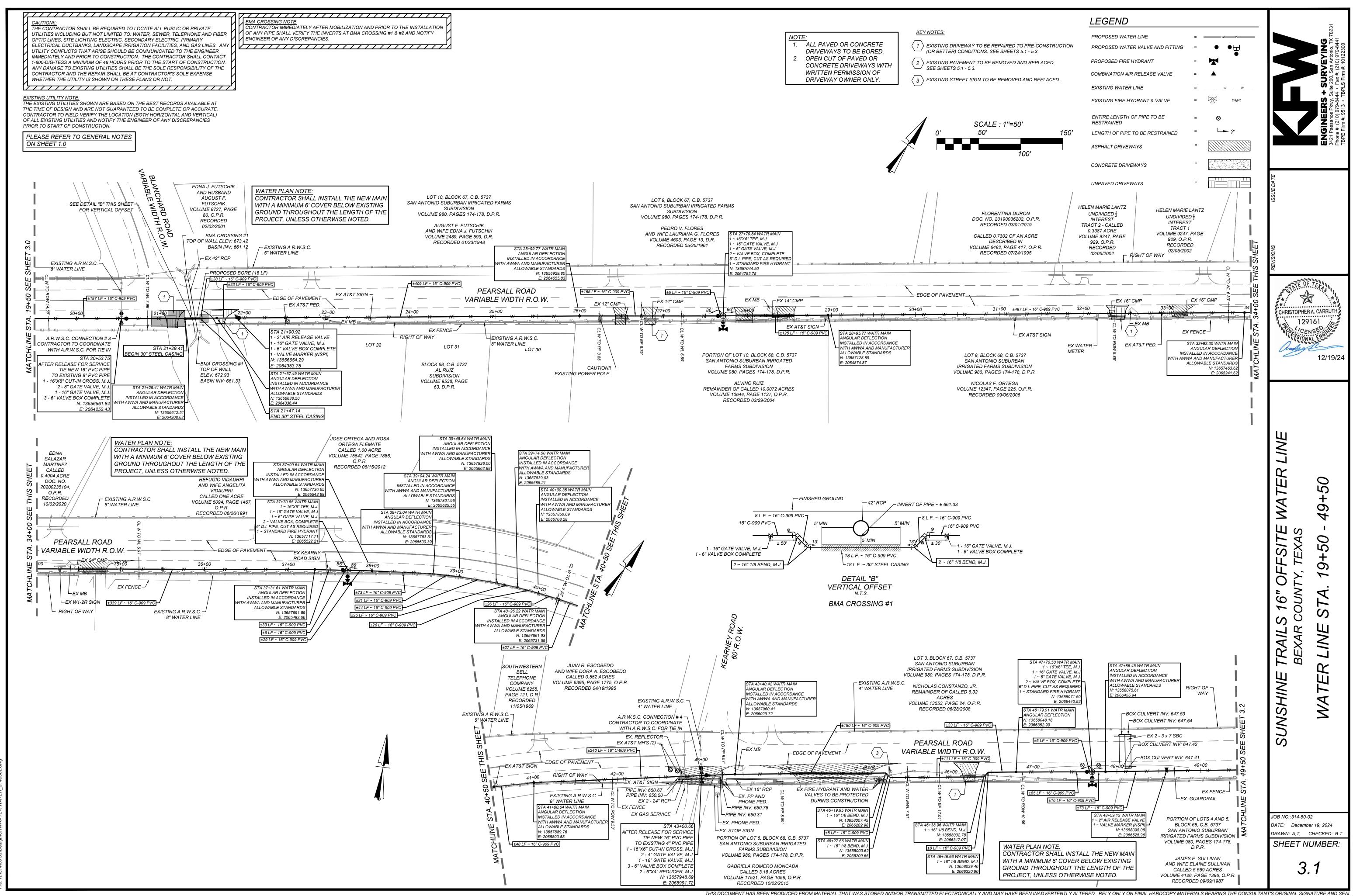


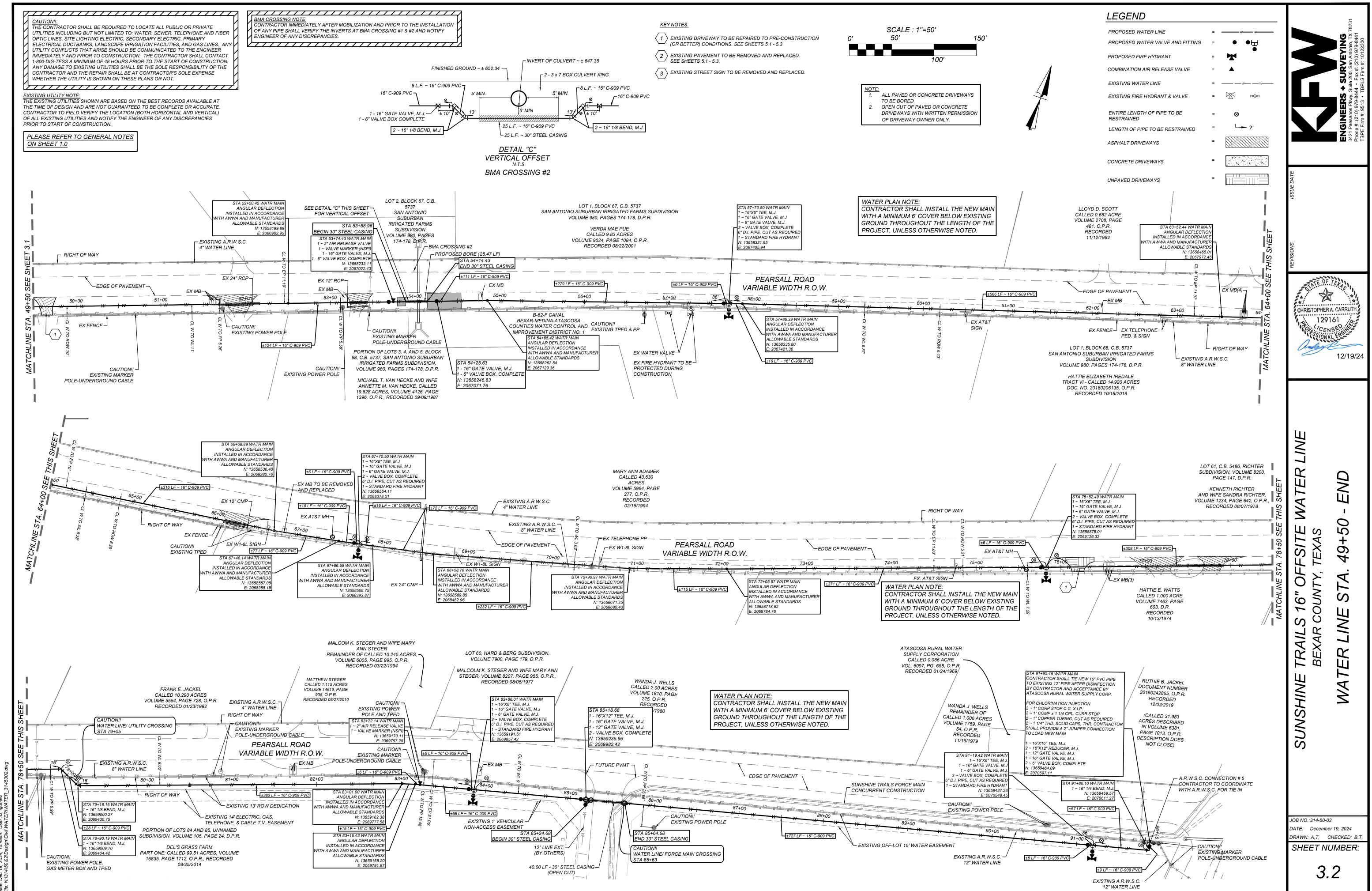


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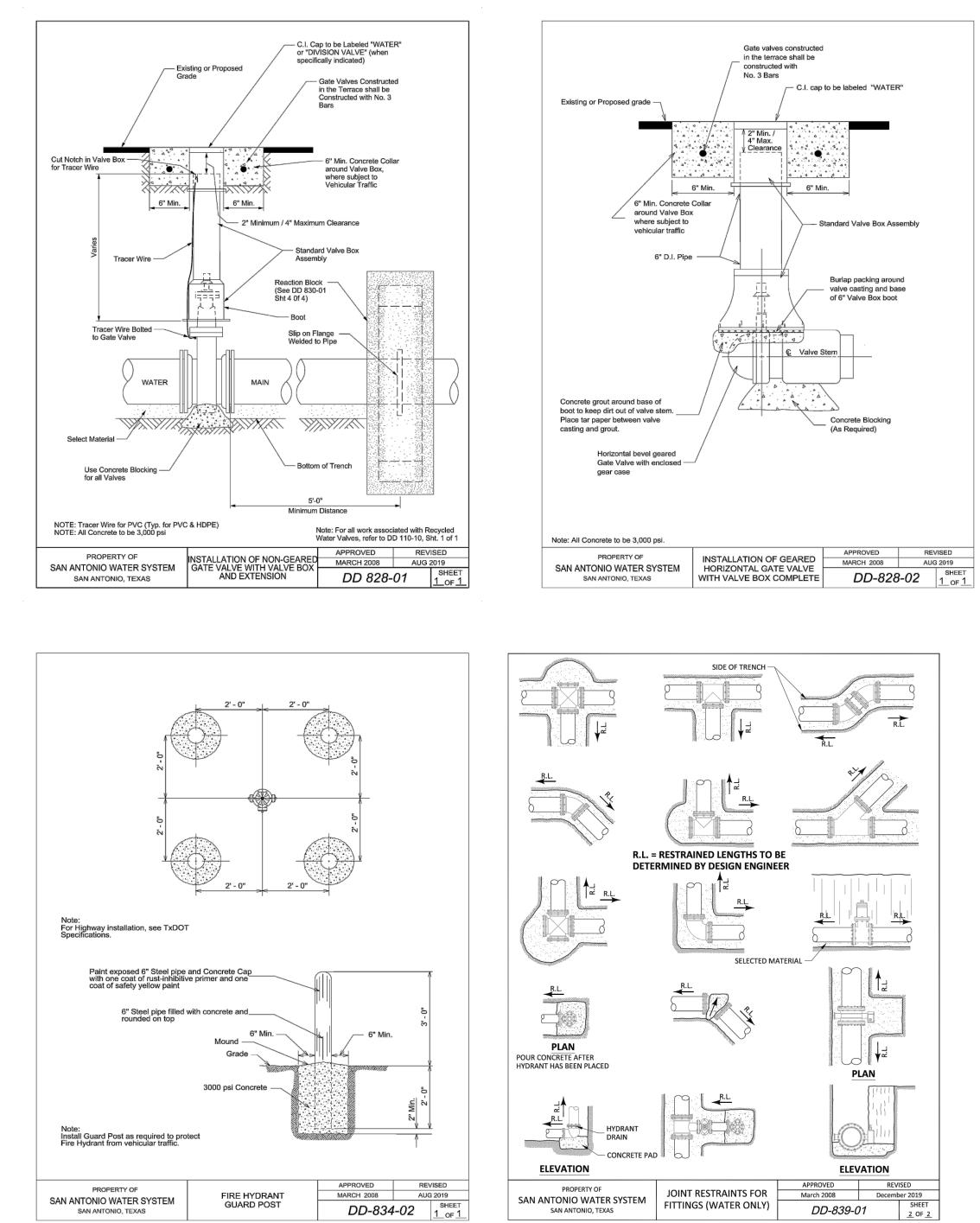


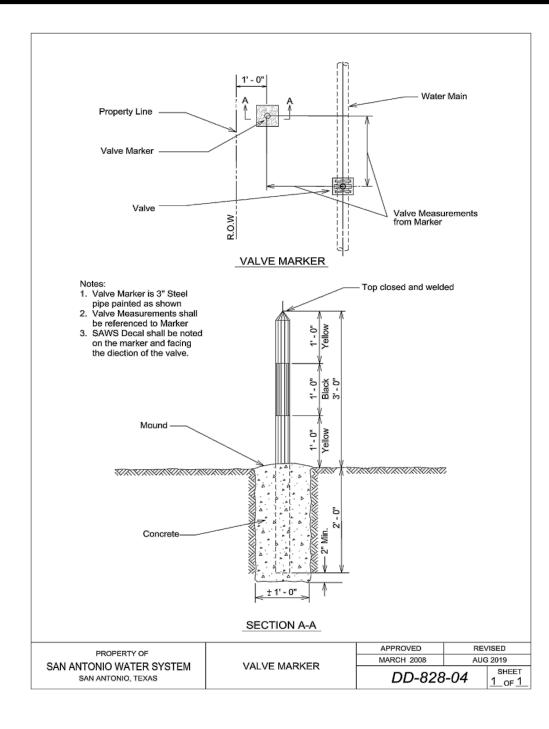
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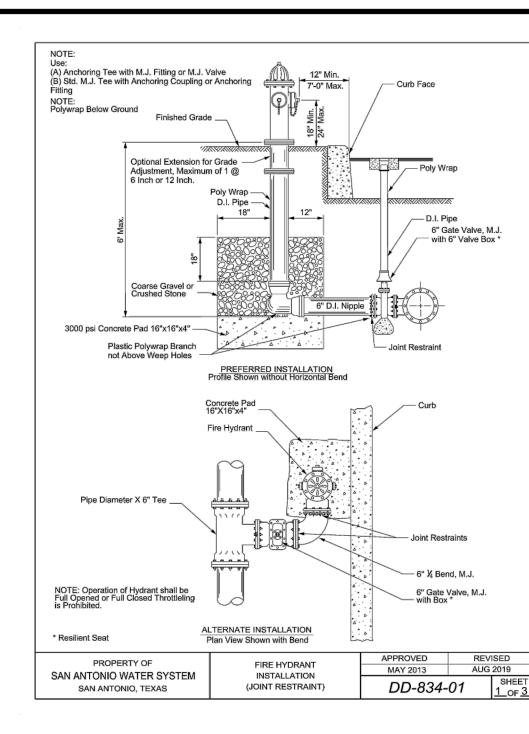




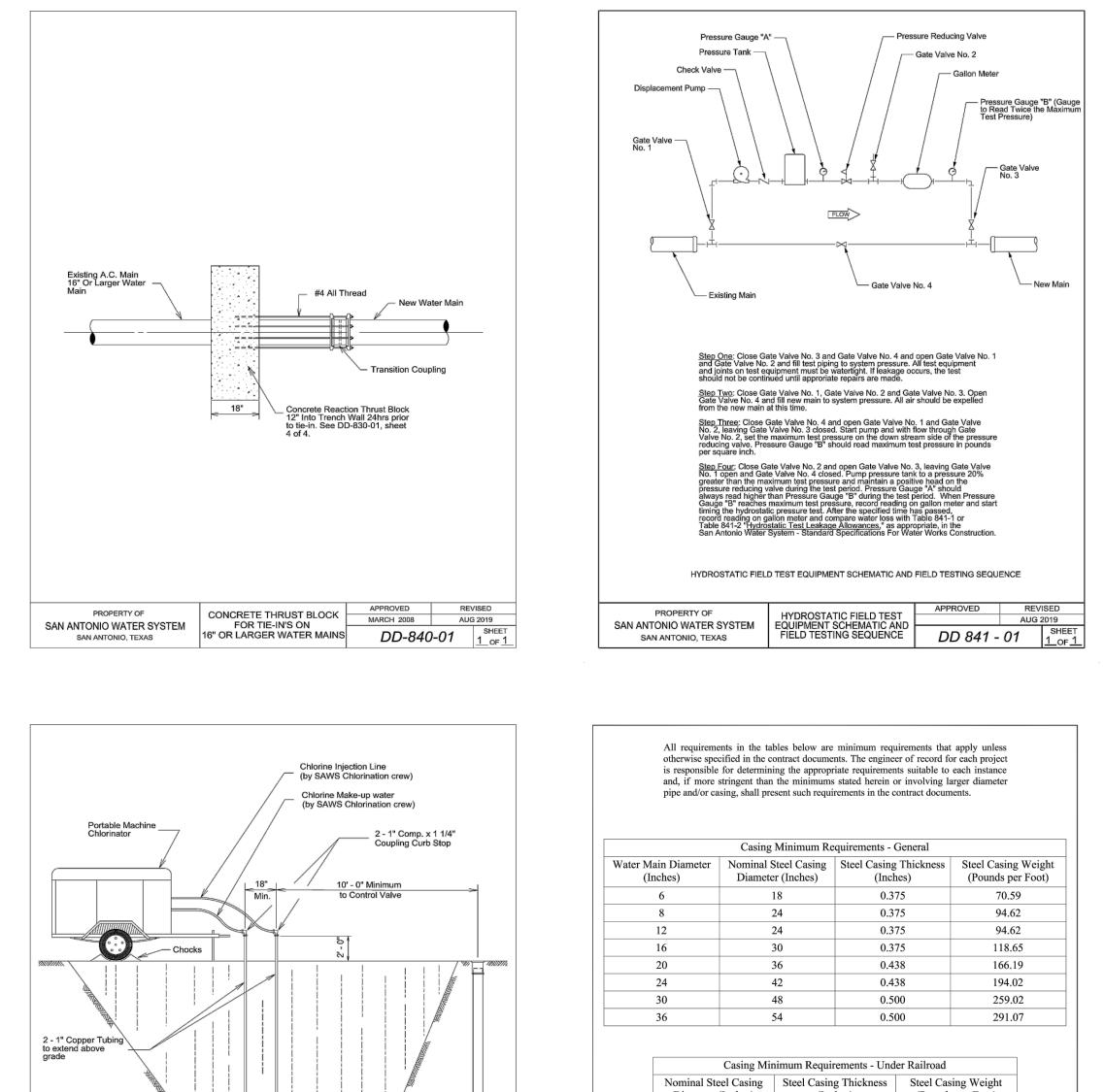
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		ENGINEERS + SURVEYING 3421 Paesanos Pkwy, Suite 200, San Antonio, TX 78231 Phone #: (210) 979-8444 • Fax #: (210) 979-8441 TBPE Firm #: 9513 • TBPLS Firm #: 10122300
REVISIONS ISSUE DATE	TOPHERA 1291 SSIONAL	61
SUNSHINE TRAILS 16" OFFSITE WATER LINE	BEXAR COUNTY, TEXAS	WATER DETAILS - 1 OF 2
	December A,T, Cl	HECKED: B.T.



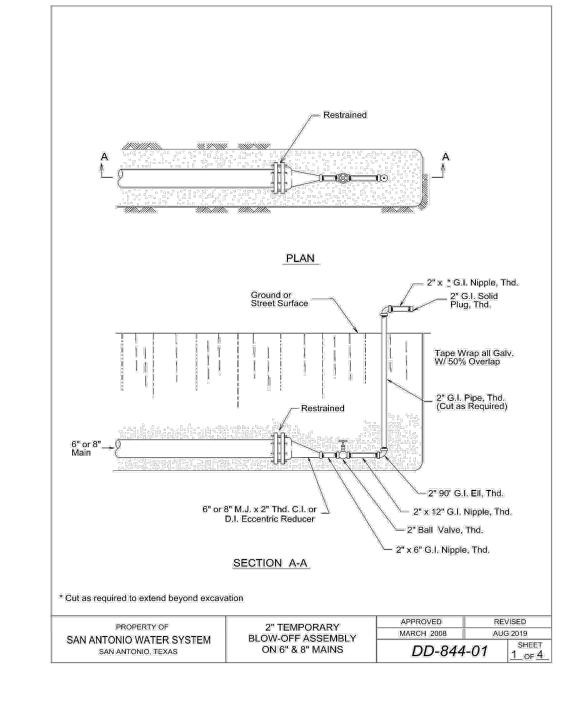
Note:		Gate Valve a Box Comple	ete
2 - 1 1/4" Solid Cap, Thd. to b Corporation Stop after chlorin	e installed on ation		
PROPERTY OF	STANDARD CHLORINATION	APPROVED March 2008	REVISED AUG 2019
SAN ANTONIO WATER SYSTEM SAN ANTONIO, TEXAS	INSTALLATION	DD-847	QUEET

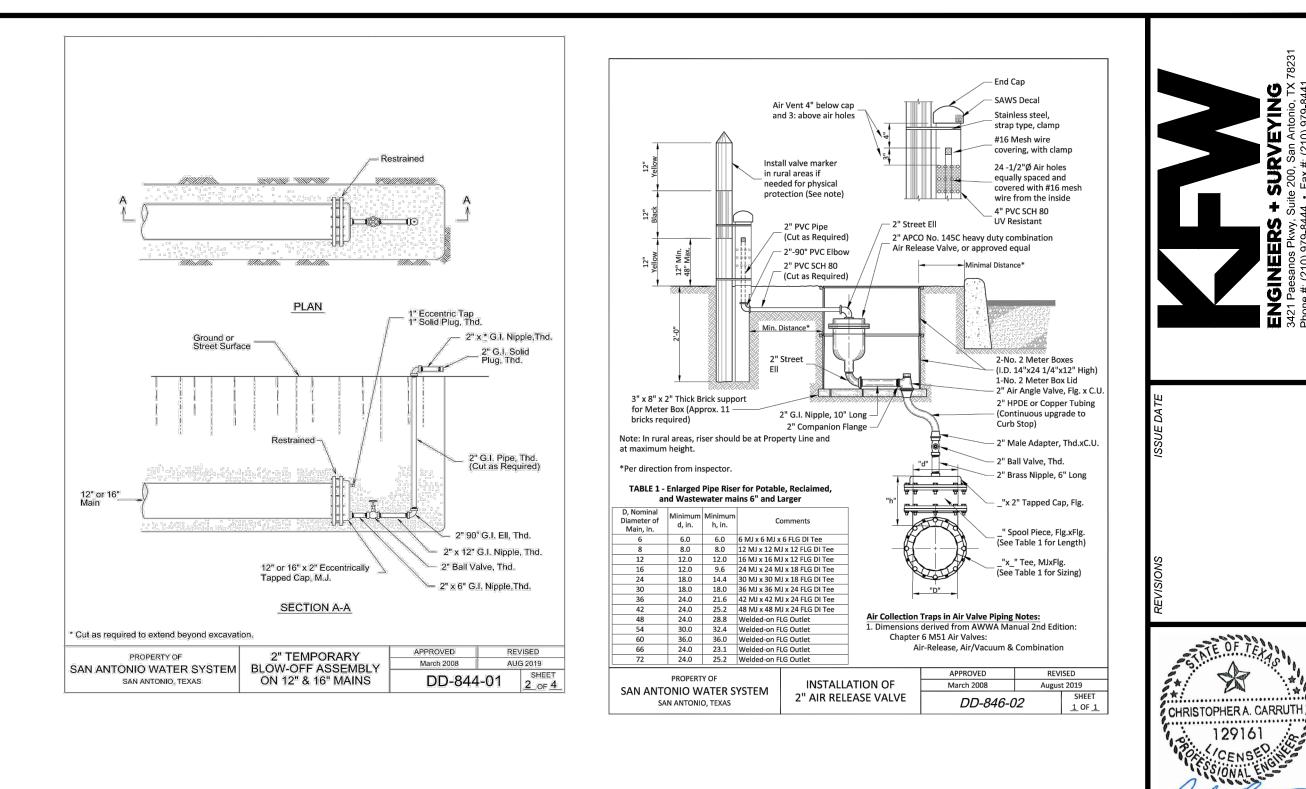
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Nominal Steel Casing Diameter (Inches)	Steel Casing Thickness (Inches)	Steel Casing Weight (Pounds per Foot)
18	0.375	70.59
24	0.438	110.22
30	0.500	157.53
36	0.562	212.70
42	0.625	276.18
48	0.625	316.53

Railroad notes:

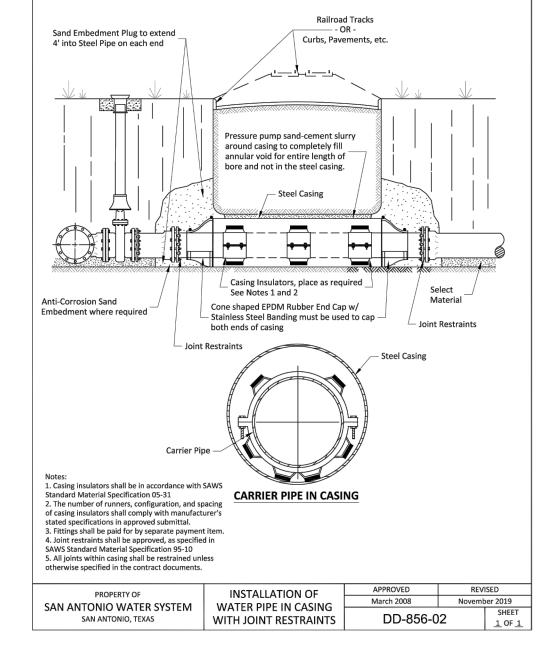
1. Steel casing shall have a minimum yield strength of 35,000 pounds per square inch. 2. Casing pipes larger than 48" diameter or with any portion deeper than 20' shall be submitted to chief engineer of the railroad for approval.

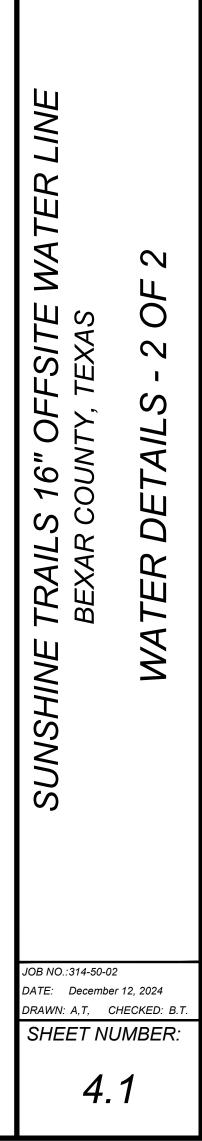
PROPERTY OF		APPROVED	REV	ISED
SAN ANTONIO WATER SYSTEM	INSTALLATION OF PIPE	March 2008	Novemb	oer 2019
	IN CASING		1	SHEET
SAN ANTONIO, TEXAS		DD-856-0	1	<u>1</u> OF <u>1</u>





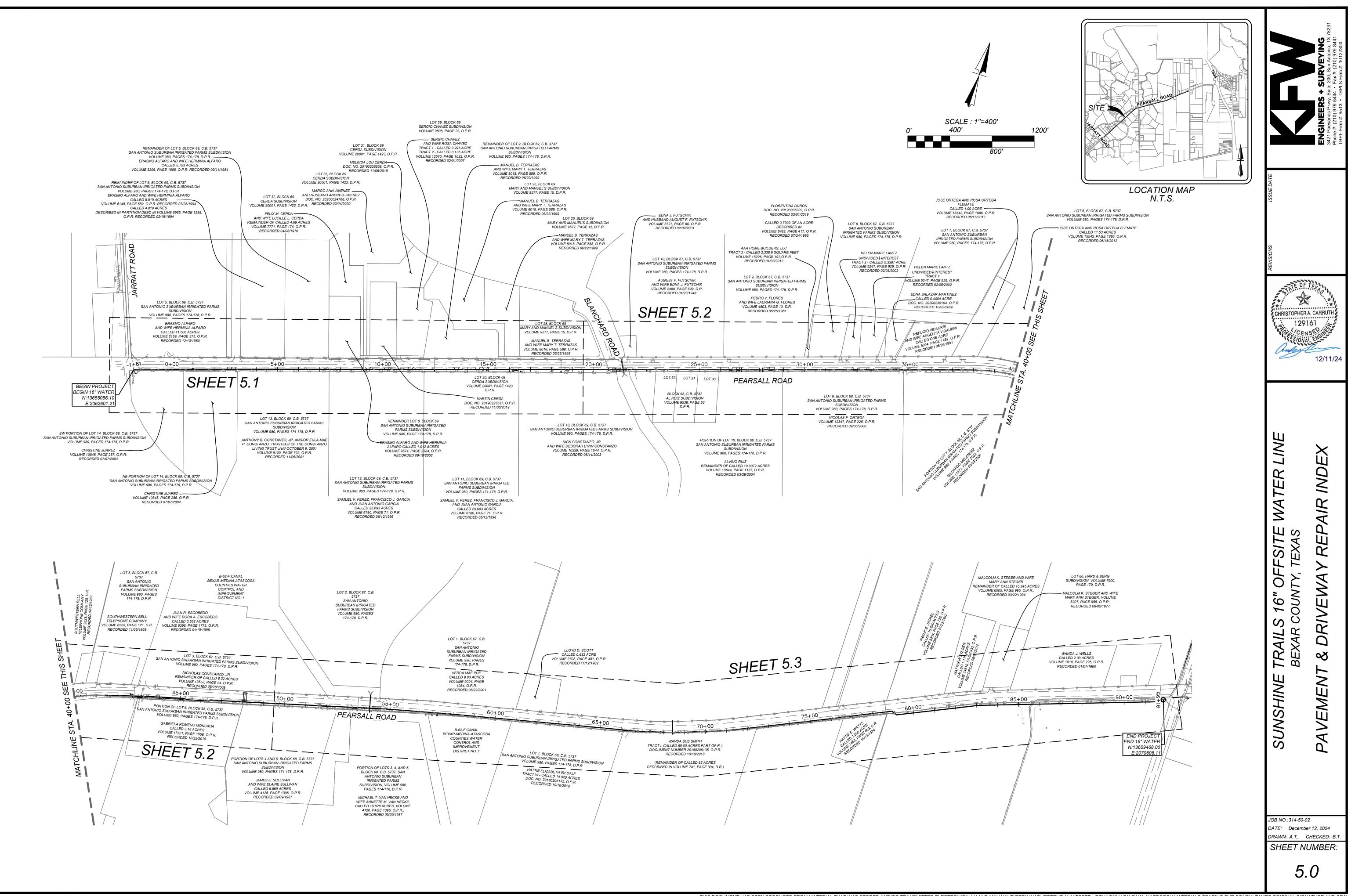
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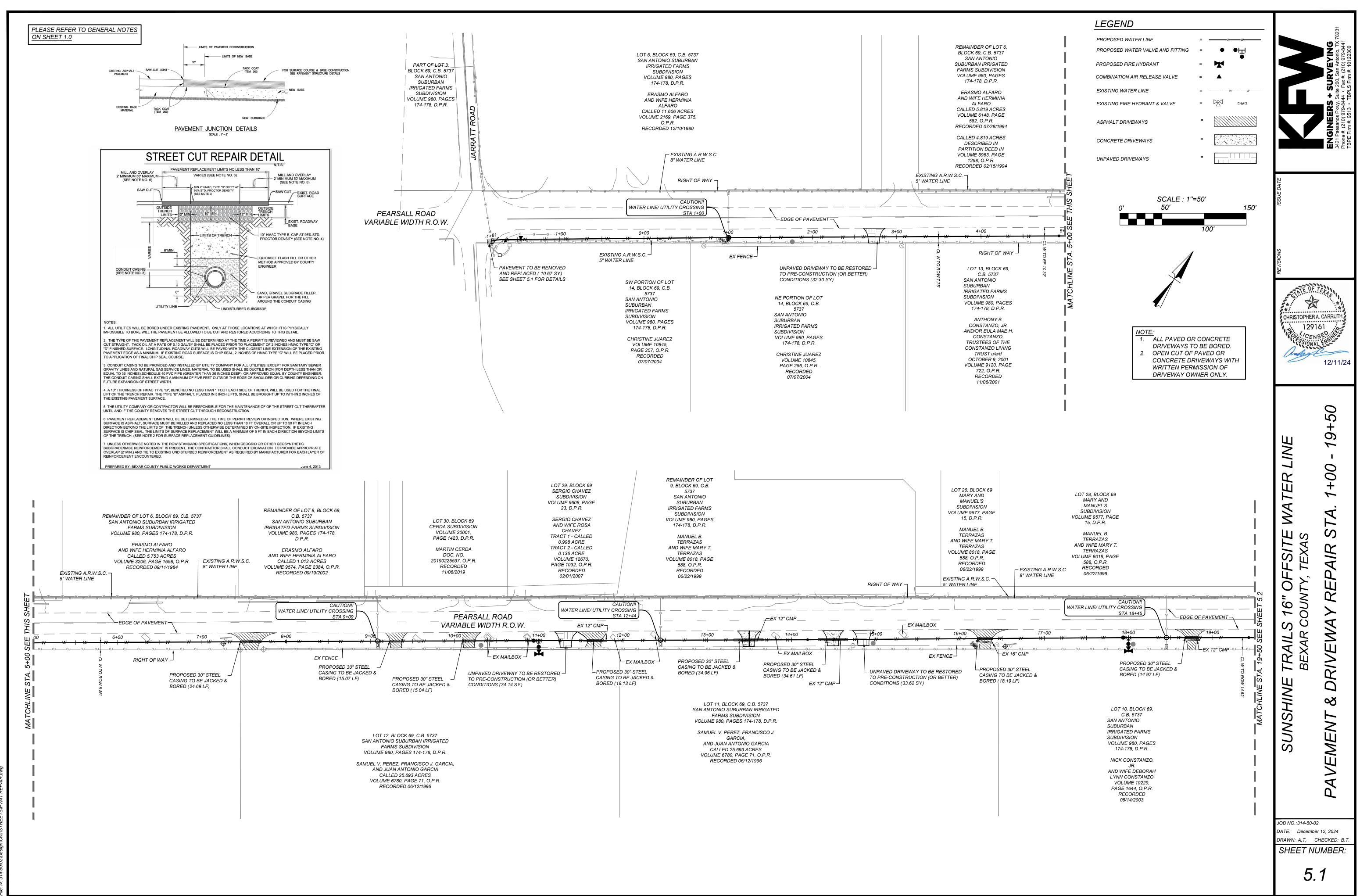


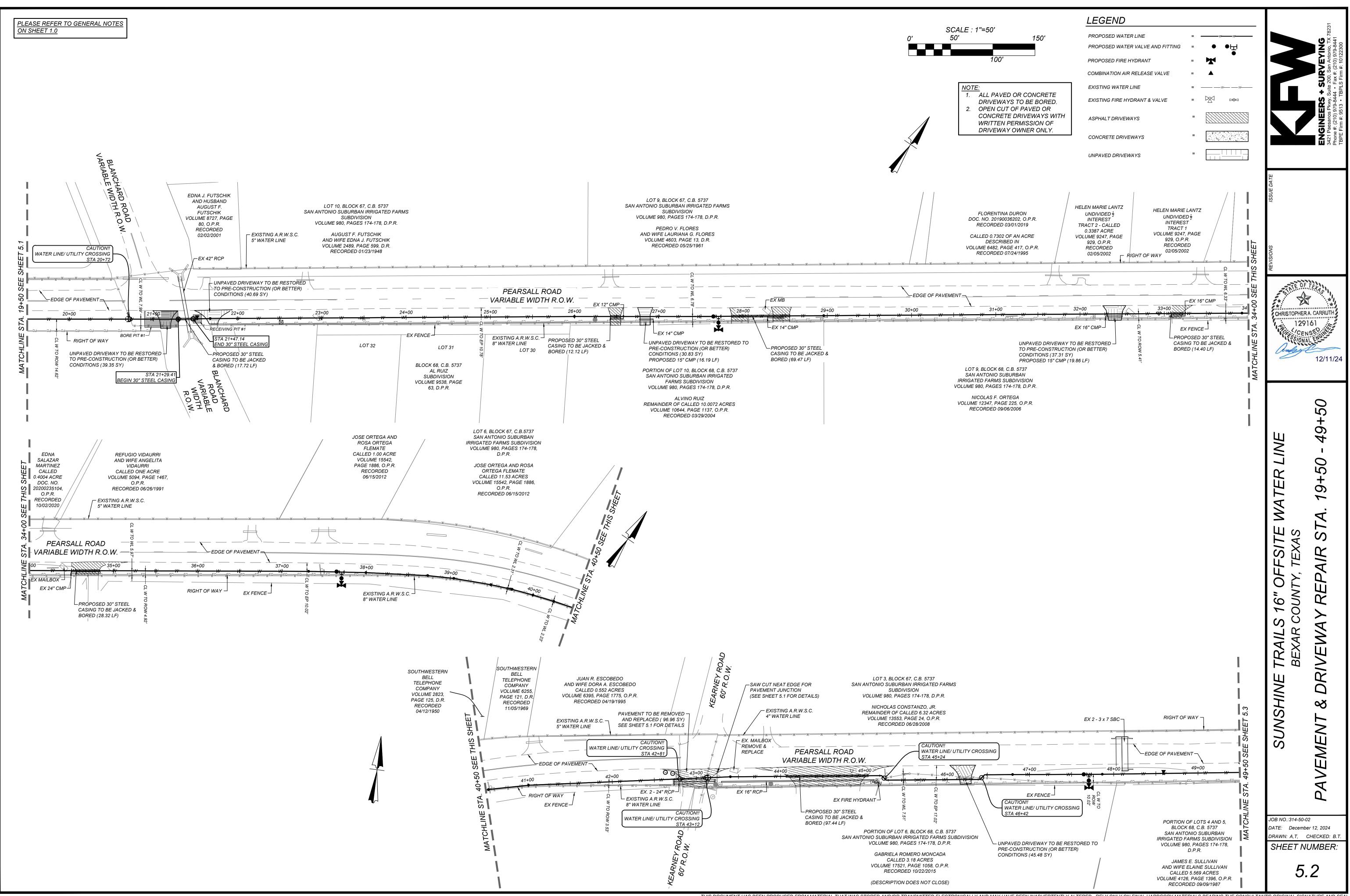


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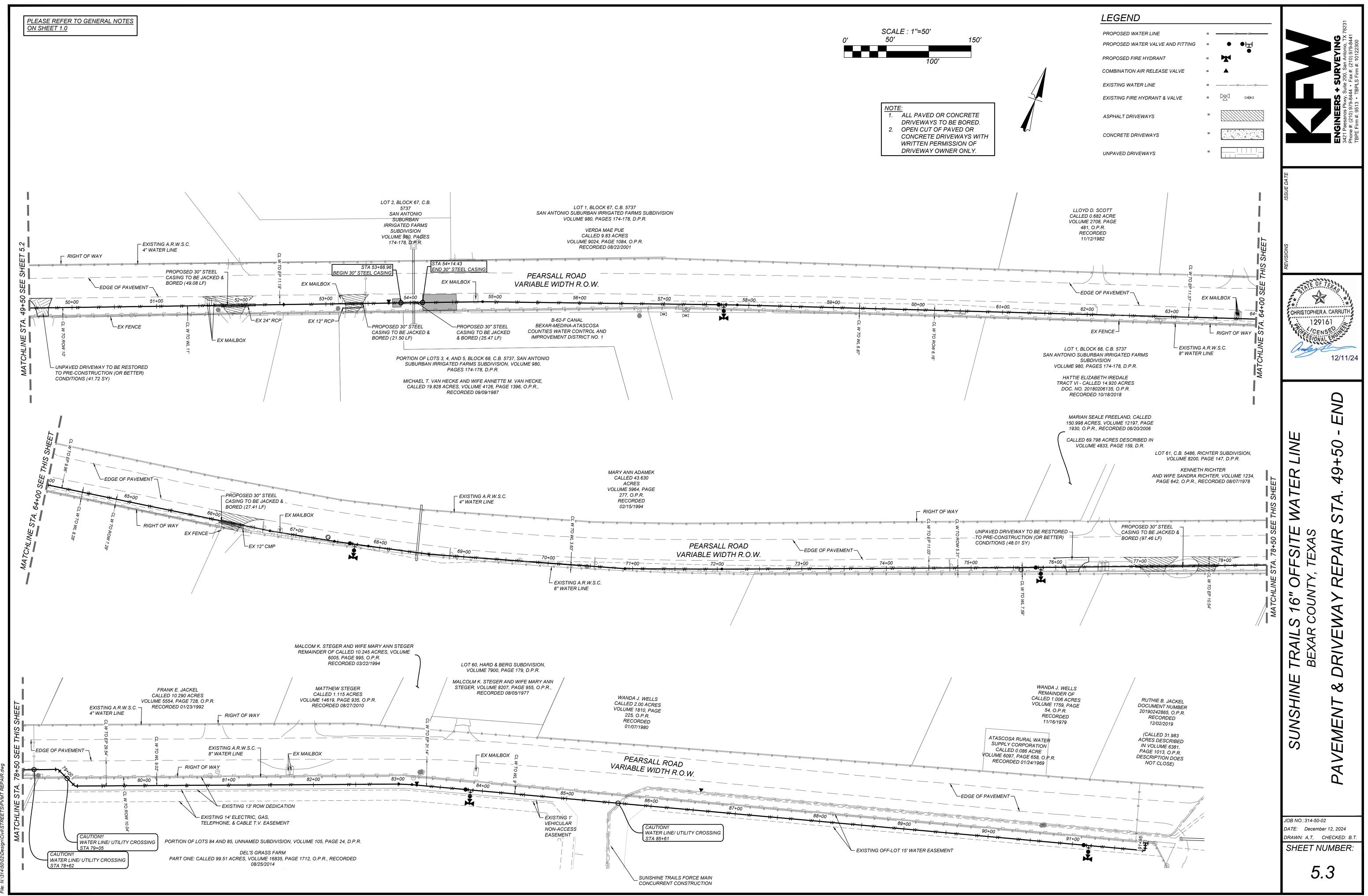
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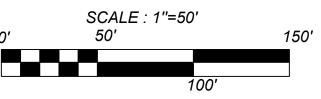


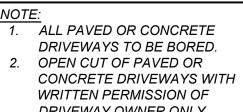




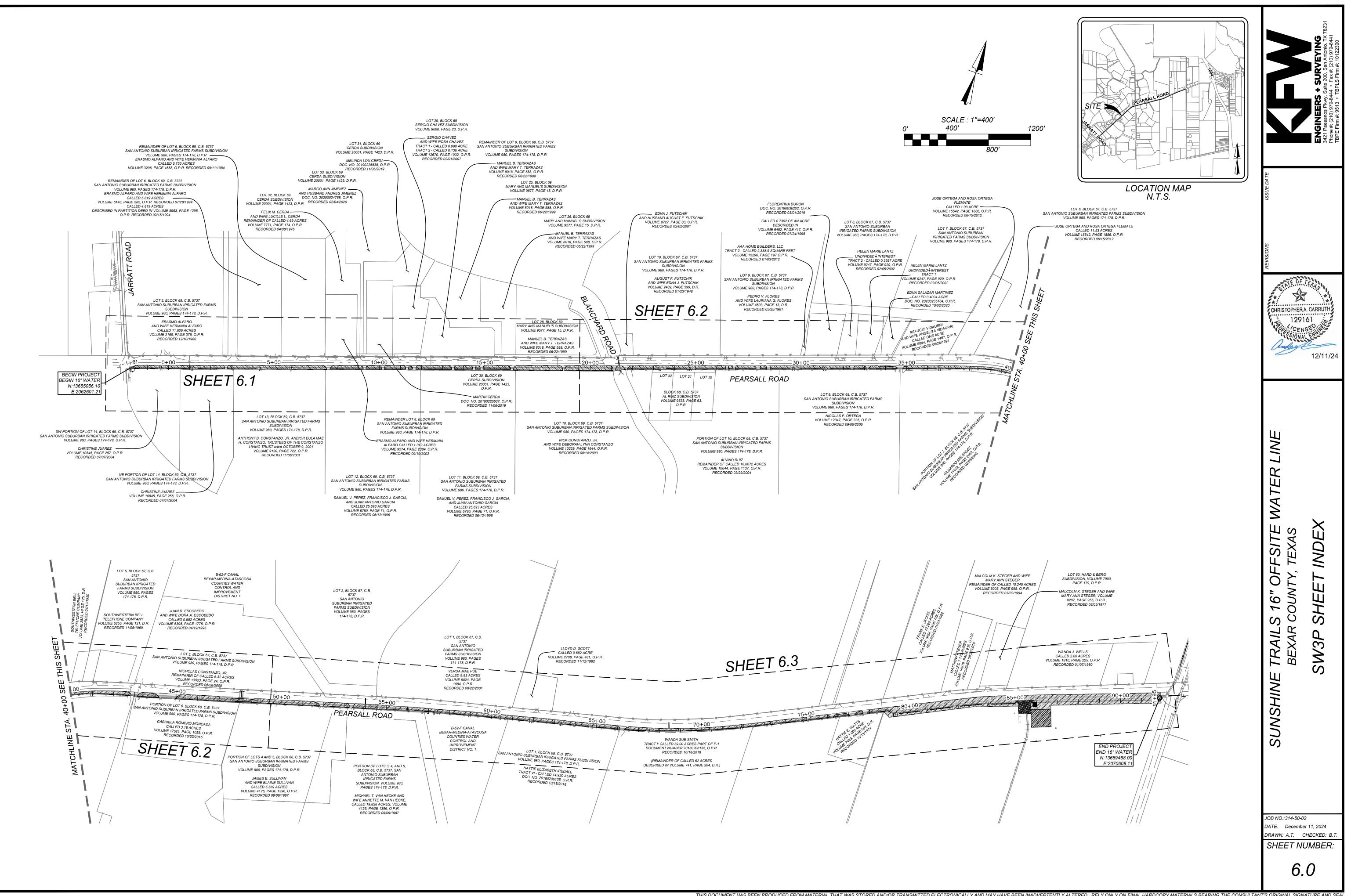
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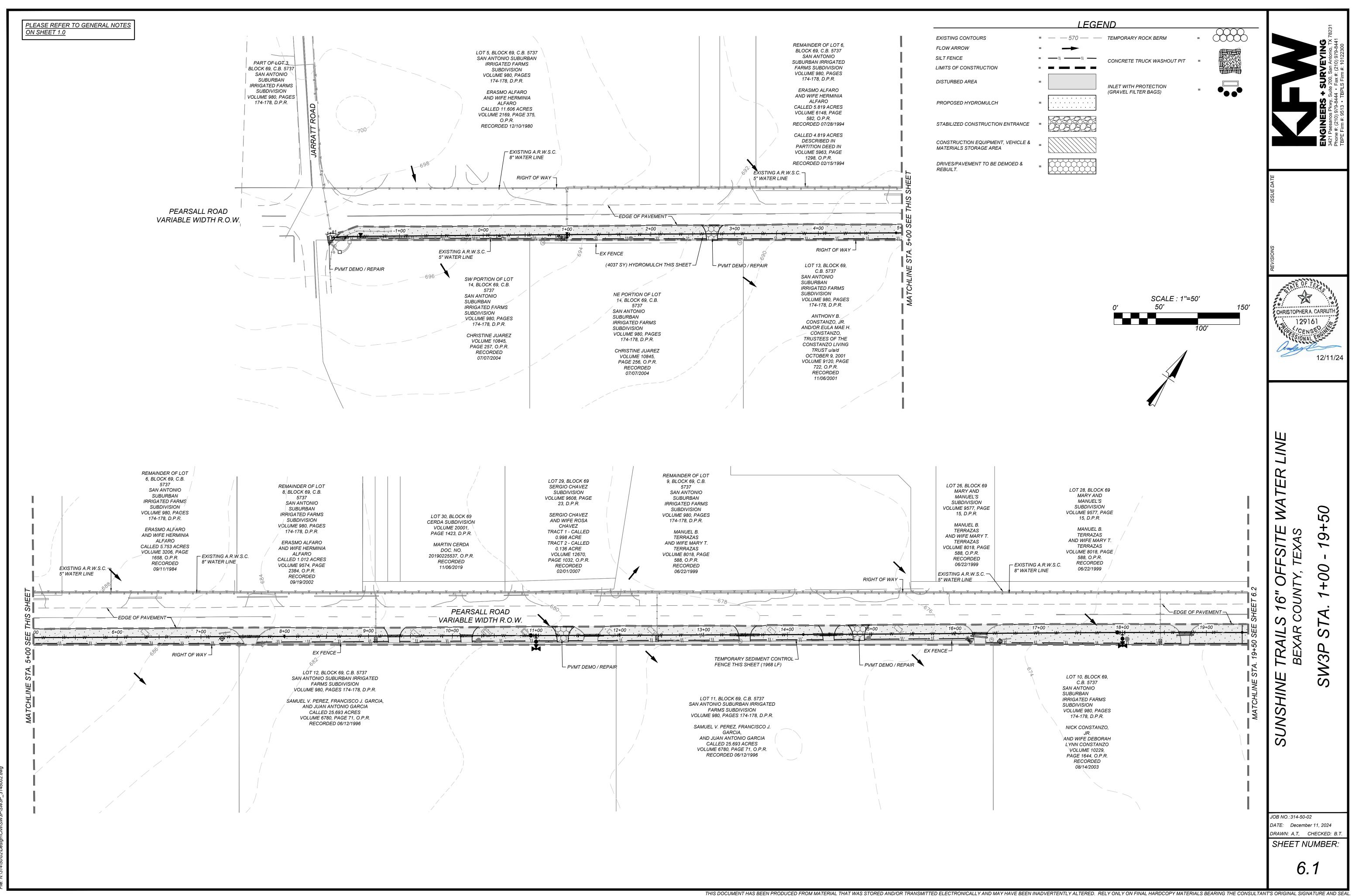


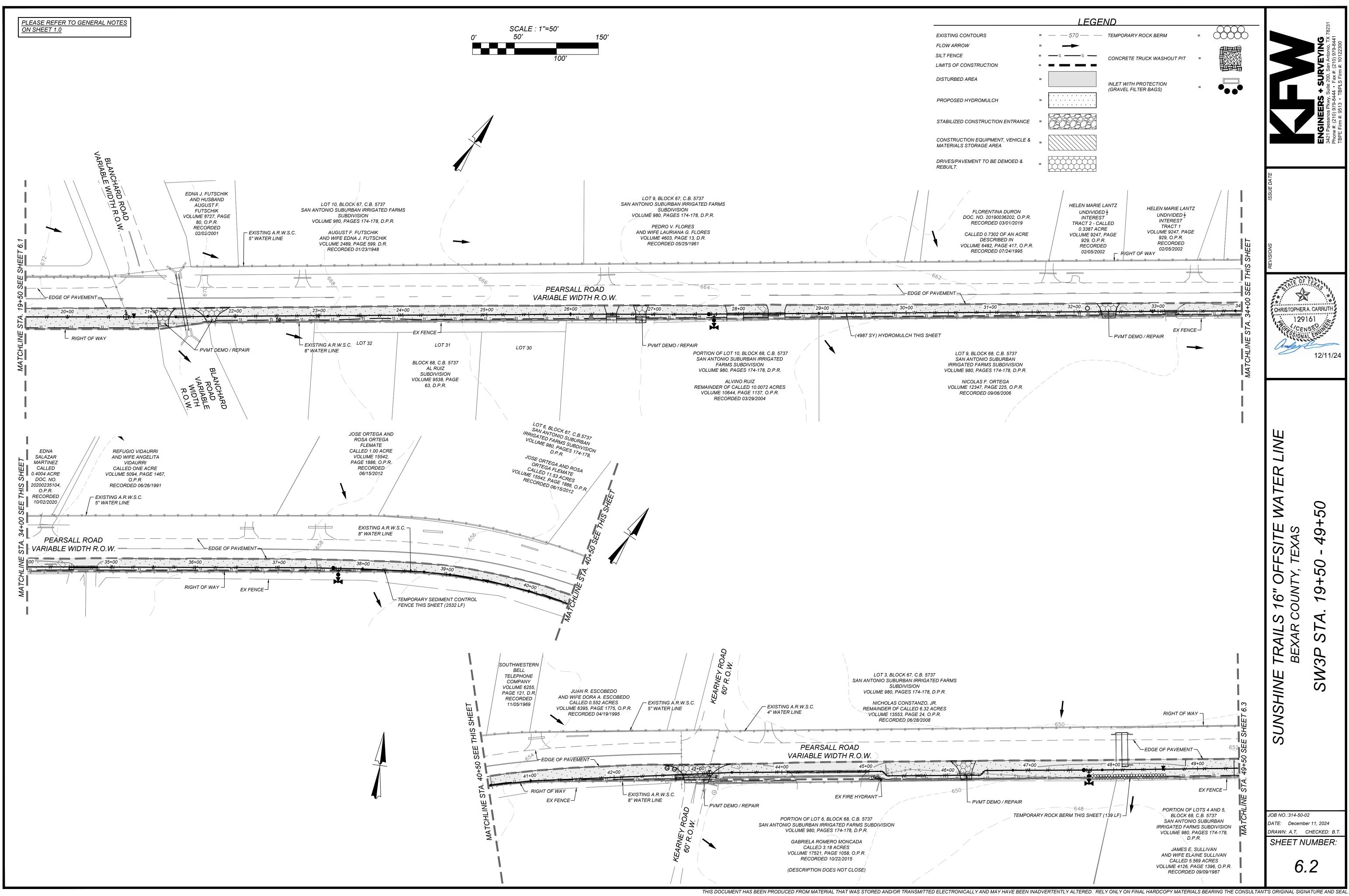


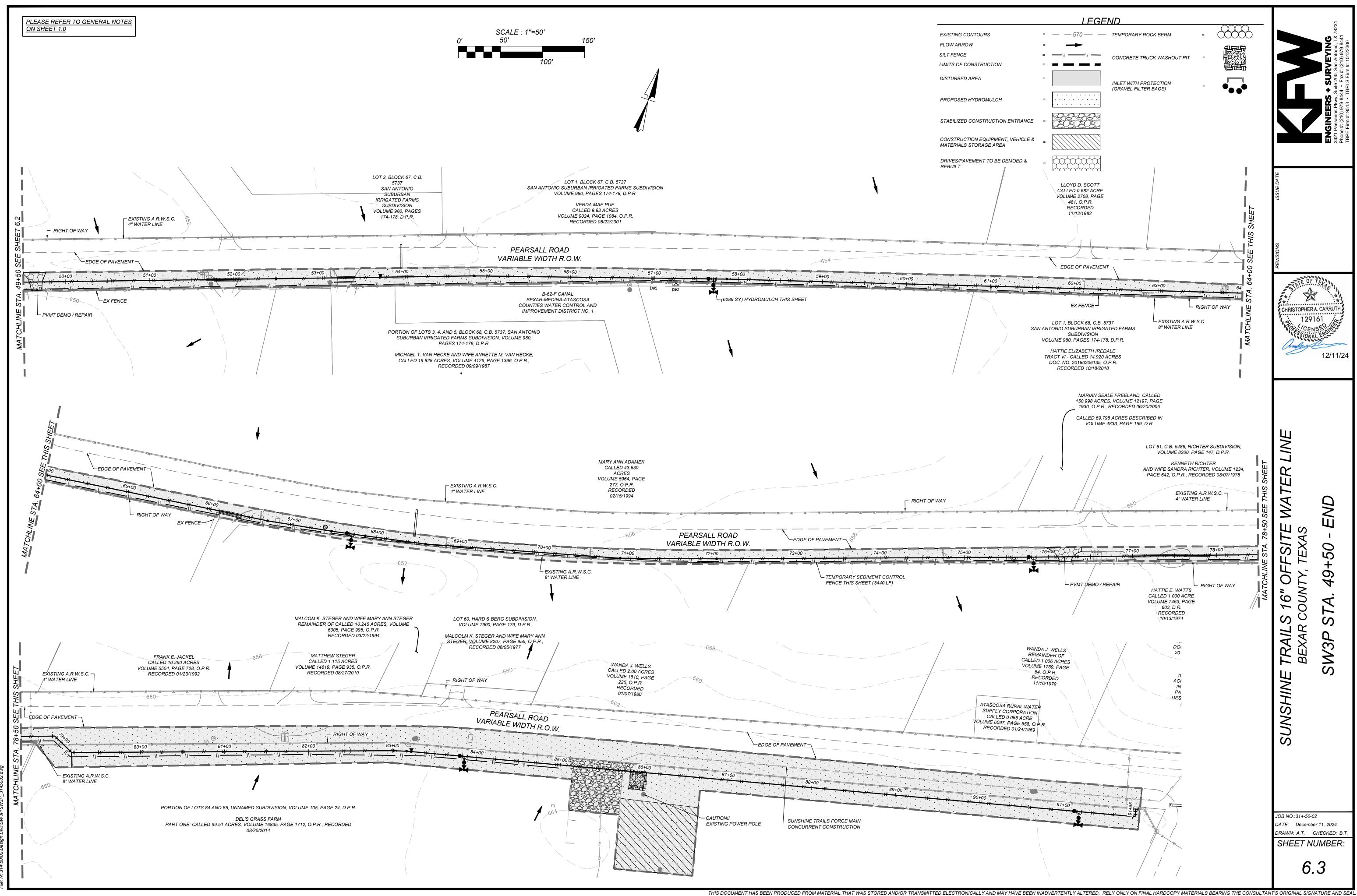


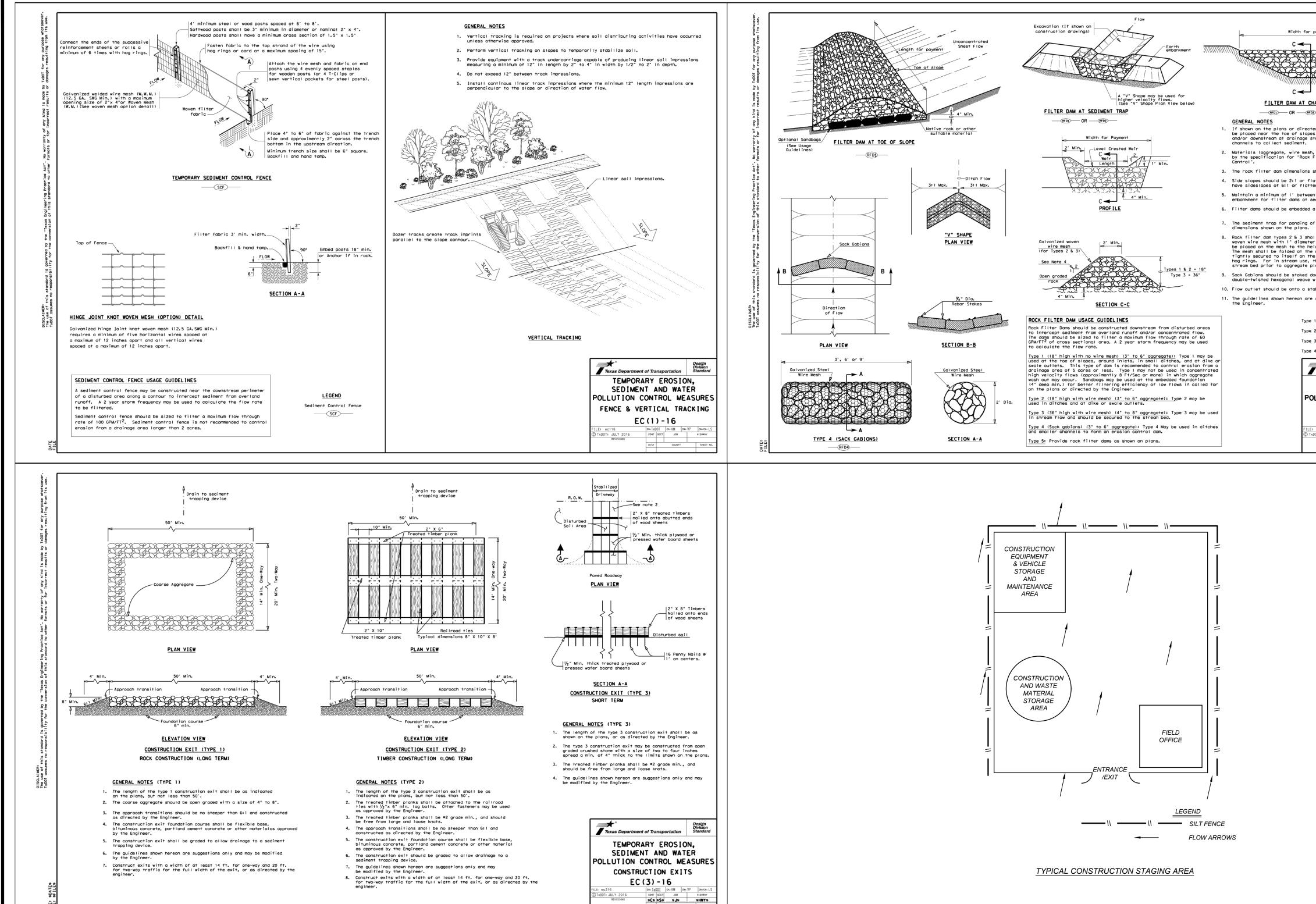
THIS DOCUMENT HAS BEEN PRODUCED FROM MATERIAL THAT WAS STORED AND/OR TRANSMITTED ELECTRONICALLY AND MAY HAVE BEEN INADVERTENTLY ALTERED. RELY ONLY ON FINAL HARDCOPY MATERIALS BEARING THE CONSULTANT'S ORIGINAL SIGNATURE AND SEAL











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Galvanized Woven Wire Mesh (for Types 2 & 3) r payment (for Types 2 & 3) r payment (fo		REVISIONS ISSUE DATE
		SUNSHINE TRAILS 16" OFFSITE WATER LINE BEXAR COUNTY, TEXAS 15/11/2 SW3P DETAILS
	RED. RELY ONLY ON FINAL HARDCORY MATERIALS BEARING THE CONSULTAN	JOB NO.:314-50-02 DATE: December 11, 2024 DRAWN: A,T, CHECKED: B. SHEET NUMBER: 6.4

TRAFFIC CONTROL NARRATIVE

1. GENERAL

- (1) TRAFFIC MUST BE HANDLED THROUGHOUT THE PROJECT DURING CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING A SAFE AND COMFORTABLE PASSAGE FOR VEHICULAR AND PEDESTRIAN TRAFFIC WITH MINIMAL INCONVENIENCE TO THE PUBLIC.
- (2) THE CONTRACTOR MAY PROPOSE/RECOMMEND MODIFICATIONS TO THE SEQUENCE OF WORK FOR CONSIDERATION BY THE ENGINEER. ANY MAJOR RECOMMENDED MODIFICATION BY THE CONTRACTOR SHALL INCLUDE ANY CHANGES TO THE VARIOUS BID ITEMS, IMPACT TO TRAFFIC, EFFECT OF OVERALL PROJECT IN TIME AND COST, ETC. IF THIS PROPOSAL IS IMPLEMENTED, THE CONTRACTOR WILL BE RESPONSIBLE FOR DEVELOPING DETAILED PLAN SHEETS TO BE SEALED BY A LICENSED PROFESSIONAL ENGINEER FOR INCLUSION WITH THE CHANGE ORDER. THE CONTRACTOR CANNOT PROCEED WITH ANY CONSTRUCTION OPERATIONS BASED ON A REVISED PHASE/SEQUENCE UNTIL WRITTEN APPROVAL IS OBTAINED FROM THE ENGINEER. IF AT ANY TIME DURING CONSTRUCTION THE CONTRACTOR'S PROPOSED PLAN OF OPERATION FOR HANDLING TRAFFIC DOES NOT PROVIDE FOR SAFE AND COMFORTABLE MOVEMENT, THE CONTRACTOR WILL IMMEDIATELY CHANGE THEIR OPERATION TO CORRECT THE UNSATISFACTORY CONDITION.
- (3) DO NOT STORE ANY CONSTRUCTION MATERIAL OR EQUIPMENT AT ANY LOCATION THAT WILL CONSTITUTE A HAZARD AND WILL ENDANGER TRAFFIC.
- (4) THE CONTRACTOR WILL PROVIDE PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) PER THE TMUTCD, AT LEAST 72 HOURS IN ADVANCE OF CONSTRUCTION. THE CITY AND/OR COUNTY SHALL APPROVE THE LOCATION AND MESSAGE OF THE PCMS PRIOR TO CONSTRUCTION
- (5) THE CONTRACTOR WILL PROVIDE ADVANCE NOTIFICATION TO THE ENGINEER OF IMPENDING / UPCOMING LANE CLOSURES FOR ALL TEMPORARY AND / OR PERMANENT LANE, RAMP, CONNECTOR, FRONTAGE, SHOULDER, ETC. CLOSURES OR DETOURS. SEE GENERAL NOTES FOR NOTIFICATION REQUIREMENTS.
- (6) ACCESS TO ADJOINING PROPERTY MUST BE MAINTAINED AT ALL TIMES.
- (7) TEMPORARY DRAINAGE IS THE RESPONSIBILITY OF THE CONTRACTOR
- (8) AT NO TIME SHALL TWO CONSECUTIVE INTERSECTING ROADWAYS BE CLOSED AT ONE TIME DURING CONSTRUCTION.
- (9) AT NO TIME SHALL TWO CONSECUTIVE RAMPS BE CLOSED AT ONE TIME DURING CONSTRUCTION OR OVERLAY OPERATIONS.
- (10) UNLESS OTHERWISE DIRECTED BY THE ENGINEER, DAILY LANE CLOSURES SHALL BE LIMITED ACCORDING TO THE FOLLOWING RESTRICTIONS:

NIGHTTIME: SUNDAY THRU THURSDAY 9:00 PM TO 4:00 AM. (WITH UNIFORMED OFF DUTY LAW ENFORCEMENT OFFICERS) WEEKEND CLOSURES WHEN APPROVED BY THE ENGINEER: FRIDAY FROM 9:00 PM TO MONDAY AT 4:00 AM NO LANE CLOSURES WILL BE PERMITTED FOR THE FOLLOWING DATES AND/OR SPECIAL EVENTS: BETWEEN DECEMBER 15 AND JANUARY 1.

FIESTA WEEK AND TAX-FREE WEEKEND. (BEXAR COUNTY ONLY)

WEDNESDAY BEFORE THANKSGIVING THRU THE SUNDAY AFTER THANKSGIVING

SATURDAY AND SUNDAY BEFORE MEMORIAL DAY AND LABOR DAY.

SATURDAY OR SUNDAY WHEN JULY 4 FALLS ON A FRIDAY OR MONDAY.

ELECTION DAYS (BEXAR COUNTY ONLY)

- (11) REMOVAL AND DISPOSAL OF EXISTING ABANDONED UTILITIES (EITHER PREVIOUSLY ABANDONED OR ABANDONED DURING THIS PROJECT) REQUIRED TO SUPPORT THIS PROJECT'S CONSTRUCTION SHALL BE PERFORMED UNDER THE OVERALL PREPARE RIGHT-OF-WAY ITEM (ITEM 101).
- (12) CONTRACTOR SHALL COORDINATE WITH ADJACENT PROJECTS.
- (13) COVER PERMANENT SIGNS IF NOT USED OR IN CONFLICT WITH THE TRAFFIC CONTROL PLANS. THIS IS SUBSIDIARY TO ITEM 530.
- (14) EXCAVATION WITHIN 5 FEET OF AN EXISTING CPS ENERGY POLE WILL REQUIRE POLE BRACING. CONTACT CPS ENERGY UTILITY COORDINATION TO REQUEST POLE BRACING (GLORIA CALDERON, (210) 353-4221). THE ESTIMATED DURATION FOR THE POLE BRACING PROCESS IS APPROXIMATELY 6 TO 8 WEEKS.
- (15) WHERE AN OPEN TRENCH RESULTS IN A DROP GREATER THAN 24" AND IS LESS THAN 3' FROM THE EDGE OF TRAVEL, CONCRETE SAFETY BARRIERS SHALL BE USED.

- (16) IF LEFT OVERNIGHT, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE A 3:1 SAFETY SLOPE FOR EDGE CONDITIONS WITH A DROP OFF GREATER THAN 2" WITHOUT A POSITIVE PHYSICAL BARRIER OR STEEL PLATE THE OPEN AREA.
- (17) CONTRACTOR SHALL MAINTAIN TRAFFIC SIGNAL FUNCTIONALITY DURING ALL PHASES OF CONSTRUCTION. COORDINATE WITH THE CITY OF SAN ANTONIO AND/OR BEXAR COUNTY FOR SIGNAL TIMING REVISIONS, AS NECESSARY.

2. SEQUENCE OF WORK

- (1) THIS PROJECT WILL BE CONSTRUCTED IN ONE PHASE. BEFORE THE COMMENCEMENT OF EACH PHASE, INSTALL ADVANCE WARNING SIGNS, TEMPORARY SIGNS AND BARRICADES AS SHOWN ON THE PLANS AND/OR AS DIRECTED/APPROVED BY THE ENGINEER. DAILY LANE CLOSURES WILL BE USED IN ACCORDANCE WITH STATE TCP STANDARDS. DROP OFF CONDITIONS OF GREATER THAN 2" MUST HAVE A 3:1 SLOPE AT THE END OF EACH DAY, AS WELL AS THROUGHOUT THE PROJECT WHERE ACCESS TO ADJACENT PROPERTIES IS ALLOWED TO DRIVEWAYS AND SIDE STREETS.
- (2) PREPARING ROW / REMOVAL OF EXISTING ITEMS TO BE DONE ONLY IN AREAS WHERE WORK IS OCCURRING, AS PER THE PHASE NOTED BELOW.
- (3) PLANING, SURFACE TREATMENTS AND OVERLAYS SHALL BE PERFORMED IN THE DIRECTION OF TRAFFIC. BEGIN SURFACE CONSTRUCTION ON HIGH SIDE OF ROAD TO AVOID WATER PONDING ISSUES.
- (4) A BRIEF DESCRIPTION OF THE TRAFFIC CONTROL PLAN IS AS FOLLOWS:
- WATER LINE IMPROVEMENTS:
- (1) INSTALL TEMPORARY WARNING SIGNS, BARRICADES AND OTHER TRAFFIC CONTROL DEVICES PERTINENT TO THE SANITARY SEWER LINE BEING CONSTRUCTED. CONTRACTOR SHALL REFERENCE TXDOT TRAFFIC CONTROL DETAILS TCP(1-3)-18 AND TCP(2-3)-18 FOR TRAFFIC SHIFTS ON TWO-LANE ROADS.
- (2) INSTALL TEMPORARY PAVEMENT AND WORK ZONE PAVEMENT MARKINGS AS INDICATED ON THE PLAN SHEETS.
- (3) CONSTRUCT WATER LINE IMPROVEMENTS.
- (4) REMOVE ALL TEMPORARY WARNING SIGNS, BARRICADES, TEMPORARY PAVEMENT MARKINGS AND OTHER TRAFFIC CONTROL DEVICES.
- (5) INSTALL TEMPORARY WARNING SIGNS, BARRICADES AND OTHER TRAFFIC CONTROL DEVICES PERTINENT TO THE PAVEMENT BASE REPAIRS.
- (6) INSTALL PAVEMENT BASE REPAIRS AND PAVEMENT MARKINGS. CONTRACTOR SHALL REFERENCE TXDOT TRAFFIC CONTROL DETAILS TCP(SC-6)-21 AND TCP(SC-7)-21 FOR SEAL COAT OPERATIONS.
- (7) REMOVE ALL TEMPORARY WARNING SIGNS, BARRICADES AND OTHER TRAFFIC CONTROL DEVICES.

3. SAFETY

- (1) THE CONTRACTOR WILL PROVIDE, CONSTRUCT AND MAINTAIN BARRICADES AND SIGNS IN ACCORDANCE WITH STATE STANDARDS. ANY SIGNS REQUIRED THAT ARE NOT DETAILED IN THE STANDARD SHEETS SHALL BE IN CONFORMANCE WITH THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE "STANDARD HIGHWAY SIGN DESIGNS FOR TEXAS."
- (2) BARRICADES AND WARNING SIGNS SHALL BE PLACED AS INDICATED IN THE DETAILS. THIS SHALL BE CONSIDERED THE MINIMUM REQUIRED TO PROVIDE FOR THE SAFETY OF TRAFFIC DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN OTHER SUCH BARRICADES AND SIGNS DEEMED NECESSARY BY THE ENGINEER OR AS DIRECTED BY FIELD CONDITIONS, TO PROVIDE FOR THE PASSAGE OF TRAFFIC IN SAFETY AT ALL TIMES.
- (3) THE CONTRACTOR SHALL PROVIDE AND MAINTAIN FLAGGERS AS DIRECTED/APPROVED BY THE ENGINEER, AT SUCH POINTS, AND FOR SUCH PERIODS OF TIME AS MAY BE REQUIRED, TO PROVIDE FOR THE SAFETY OF THE TRAVELING PUBLIC AND THE CONTRACTOR'S PERSONNEL
- (4) THE CONTRACTOR SHALL KEEP THE ROADWAY CLEAN AND FREE OF DIRT OR OTHER MATERIALS DURING HAULING OPERATIONS. IF THE CONTRACTOR DOES NOT MAINTAIN A CLEAN ROADWAY, THEY SHALL CEASE ALL CONSTRUCTION OPERATIONS, WHEN DIRECTED BY THE ENGINEER, TO CLEAN THE ROADWAY TO THE SATISFACTION OF THE ENGINEER.

4. HAULING EQUIPMENT

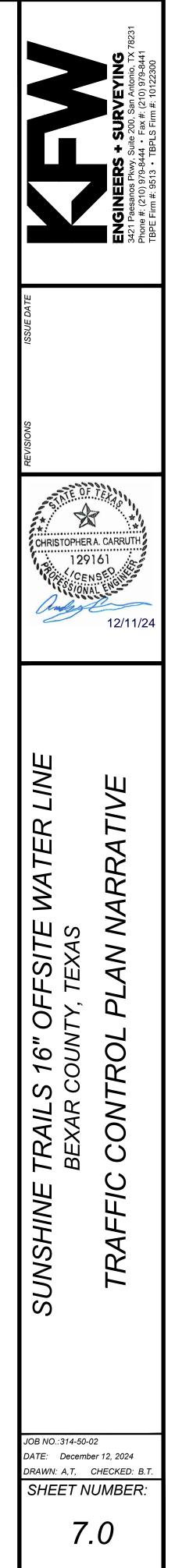
- (1) THE USE OF RUBBER-TIRED EQUIPMENT WILL BE REQUIRED FOR MOVING DIRT OR OTHER MATERIALS ALONG OR ACROSS PAVEMENTED SURFACES. WHERE THE CONTRACTOR DESIRES TO MOVE ANY EQUIPMENT NOT LICENSED FOR OPERATION ON PUBLIC HIGHWAYS, ON OR ACROSS PAVEMENT. THEY SHALL PROTECT THE PAVEMENT FROM DAMAGE AS DIRECTED / APPROVED BY THE ENGINEER
- (2) THROUGHOUT CONSTRUCTION OPERATIONS, THE CONTRACTOR WILL BE REQUIRED TO CONDUCT THEIR HAULING OPERATIONS IN A MANNER SUCH THAT VEHICLES WILL NOT HAUL OVER PREVIOUSLY RECOMPACTED SUBGRADE OR COMPACTED BASE MATERIAL, EXCEPT IN SHORT SECTIONS FOR DUMPING MANIPULATIONS.

5. FINAL CLEAN UP

UPON COMPLETION OF THE WORK AND BEFORE FINAL ACCEPTANCE AND FINAL PAYMENT IS MADE, THE CONTRACTOR SHALL CLEAR AND REMOVE FROM THE SITE ALL SURPLUS AND DISCARDED MATERIALS AND DEBRIS OF EVERY KIND AND LEAVE THE ENTIRE PROJECT IN A SMOOTH, NEAT AND SIGHTLY CONDITION.

6. PAYMENT

ALL BARRICADES, SIGNS, AND FLAGGERS SHALL BE SUBSIDIARY TO ITEM 530 BARRICADES, SIGNS AND TRAFFIC HANDLING. ALL EROSION AND SEDIMENT CONTROL DEVICES WILL BE PAID FOR UNDER ITEM 540 TEMPORARY EROSION, SEDIMENTATION, AND ENVIRONMENTAL CONTROLS. ALL WORK ZONE PAVEMENT MARKINGS WILL BE PAID FOR UNDER ITEM 530 WORK ZONE PAVEMENT MARKINGS. ALL OTHER WORK AND MATERIALS SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS UNLESS OTHERWISE INDICATED IN THE PLANS.



<u>NOTES:</u>

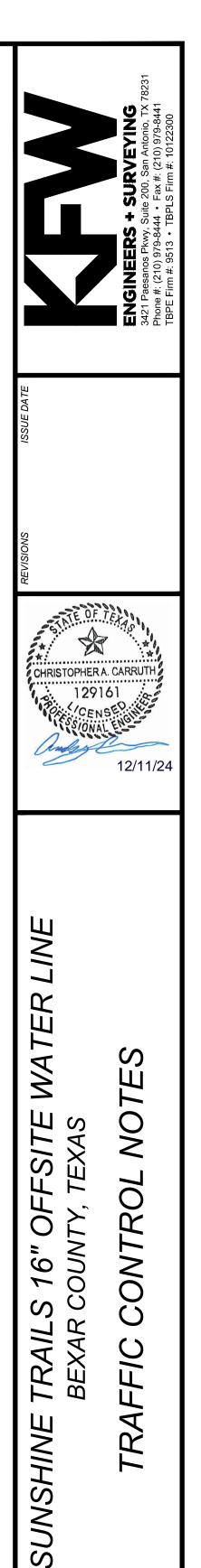
- 3. COORDINATE OPEN TRENCH WITH PROPERTY OWNERS IN ADVANCE. IF TRENCH REMAINS OPEN IN RESIDENTIAL DRIVEWAY, COVER TRENCH WITH STEEL PLATE TO MAINTAIN PROPERTY ACCESS 4. EXISTING DRIVEWAYS TO BE REPAIRED/RECONSTRUCTED TO "AS-IS" OR BETTER CONDITION AFTER CONSTRUCTION.
- 5. ROAD CROSSINGS MUST BE COMPLETED OVER A WEEKEND AND AN OFF DUTY OFFICER MUST BE PRESENT DURING CONSTRUCTION. ONE (1) LANE MUST BE KEPT OPEN AT ALL TIMES.
- 7. CONTRACTOR SHALL REFERENCE TXDOT BARRICADE AND CONSTRUCTION NOTES AND DETAILS FOR CONSTRUCTION PAVEMENT MARKINGS AND DETAILS.
- 8. AT TRAFFIC TRANSITIONS BLACK OUT EXISTING STRIPING AND USE BUTTONS TO DELINEATE NEW LANES OF TRAFFIC SHIFTS.
- 9. AT CONSTRUCTION COMPLETION, CONTRACTOR SHALL RESTRIPE ORIGINAL PAVEMENT MARKINGS IN AREAS OF LANE TRANSITION AND REMOVE TEMPORARY LANE BUTTON MARKINGS.
- 10. 2 ~ 10' LANES REQUIRED AT ALL TIMS IF THERE IS A BARRIER ON ONE SIDE OR 2 ~ 11' LANES IF NO BARRIER WITHIN CONSTRUCTION ZONE.
- 11. CONTRACTOR SHALL PROVIDE, CONSTRUCT AND MAINTAIN BARRICADES AND SIGNS IN ACCORDANCE WITH THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD).
- 13. CONTRACTOR SHALL REFERENCE TXDOT TRAFFIC CONTROL PLAN DETAILS SHEET TCP(SC-6)-21 AND TCP(SC-7)-21 FOR SEAL COAT OPERATIONS.
- 14. CONTRACTOR SHALL MAINTAIN ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS AT ALL TIMES.
- 15. CONTRACTOR SHALL FIELD ADJUST ANY TRAFFIC CONTROL SIGNAGE THAT IS IN CONFLICT WITH EXISTING COMMERCIAL/RESIDENTIAL DRIVEWAYS.
- 16. ANY CHANGES MADE TO THIS TRAFFIC CONTROL PLAN SHALL BE REVIEWED BY THE ENGINEER AND THE CITY OF SAN ANTONIO OR BEXAR COUNTY.
- CHANNELIZING DEVICES AND SIGN SPACING NOTES: 1. THE POSTED SPEED LIMIT ON LOOP 1604 IS 45 MPH & PEARSALL RD. IS 35 MPH.
- 2. CONTRACTOR SHALL VERIFY CHANNELIZING DEVICE SPACING AND SIGN SPACING WITH THE TMUTCD AND TXDOT TCP STANDARDS.

- 4. CHANNELIZING DEVICES ON A TANGENT SHALL BE SPACED A MAXIMUM OF 90 FEET APART. 5. TEMPORARY TRAFFIC CONTROL SIGNS SHALL BE SPACED 320 FEET APART.

- 1. SAWCUT NEAT EDGE FOR PAVEMENT JUNCTION AS NOTED IN PAVEMENT REPAIR PLANS (5.0-5.3). 2. SEE PAVEMENT REPAIR PLANS (5.0-5.3) FOR TRENCH AND ASPHALT REPAIR DETAILS.
- 6. UTILITY TRENCH MUST BE BACKFILLED WITH FLOWABLE FILL AT EACH ROAD CROSSING.

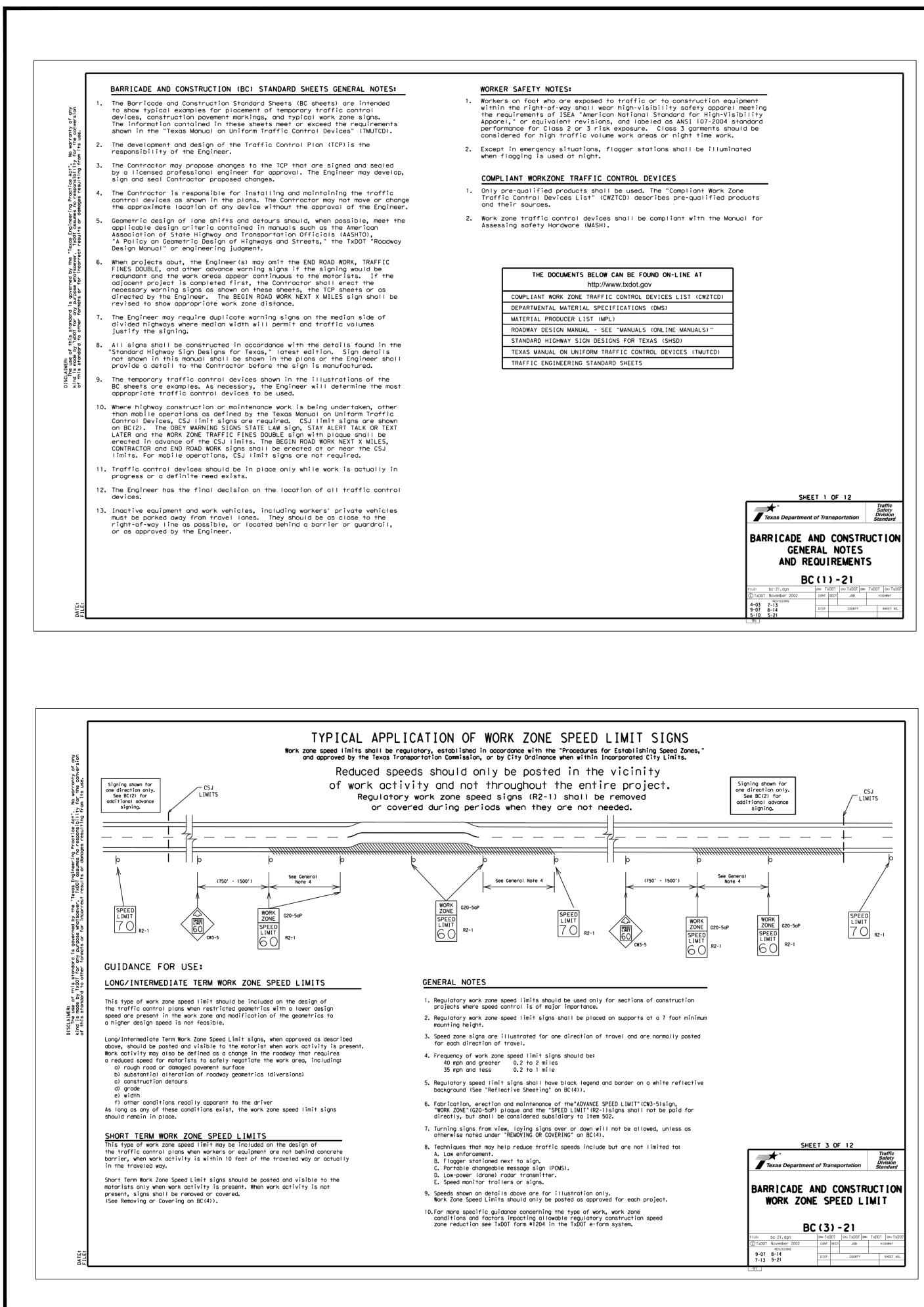
12. CONTRACTOR SHALL REFERENCE TXDOT TRAFFIC CONTROL PLAN DETAILS SHEET TCP(1-3)-18 AND TCP(2-3)-18 FOR TRAFFIC SHIFTS ON TWO-LANE ROADS.

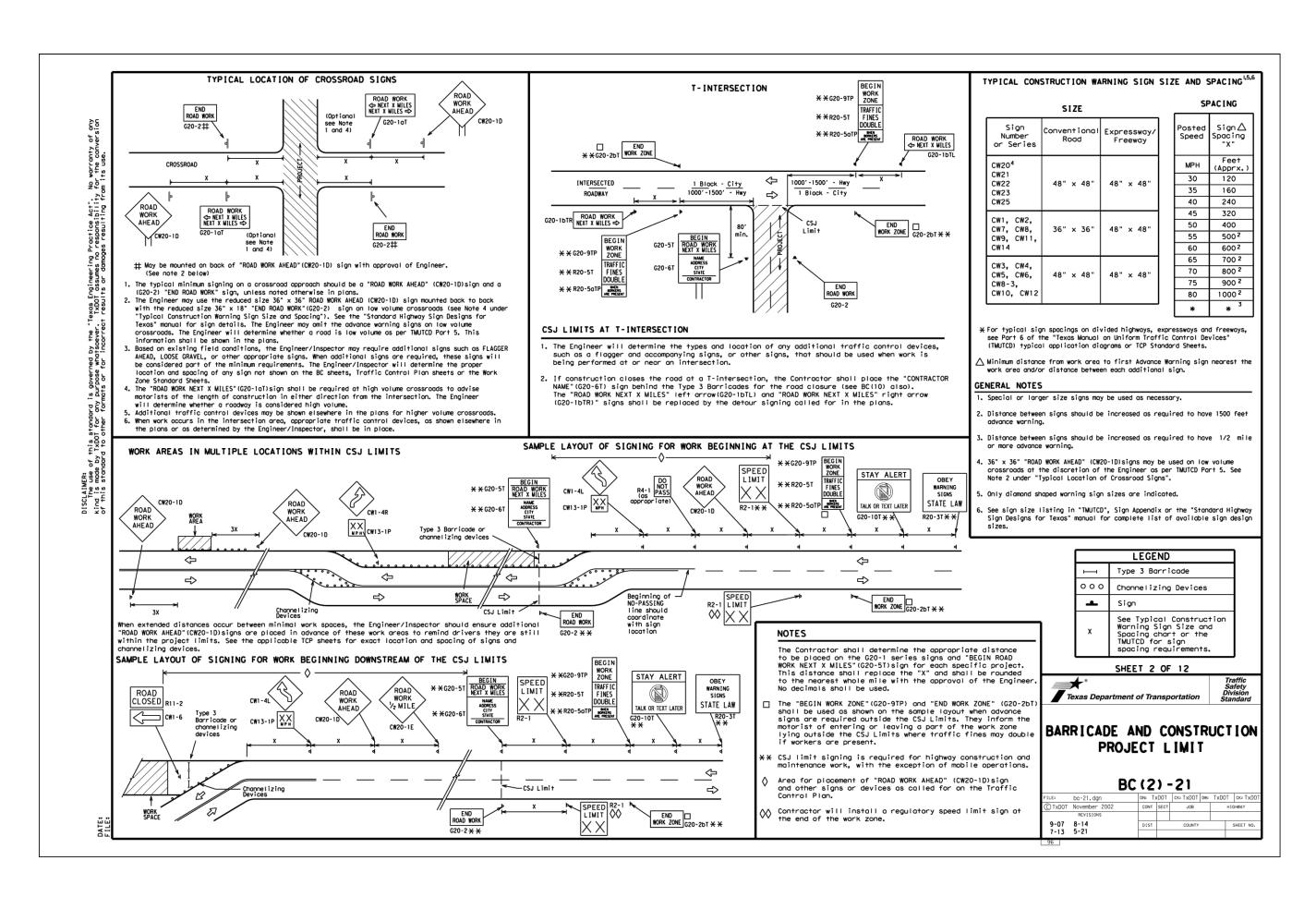
3. CHANNELIZING DEVICES ON A TAPER SHALL BE SPACED A MAXIMUM OF 45 FEET APART.

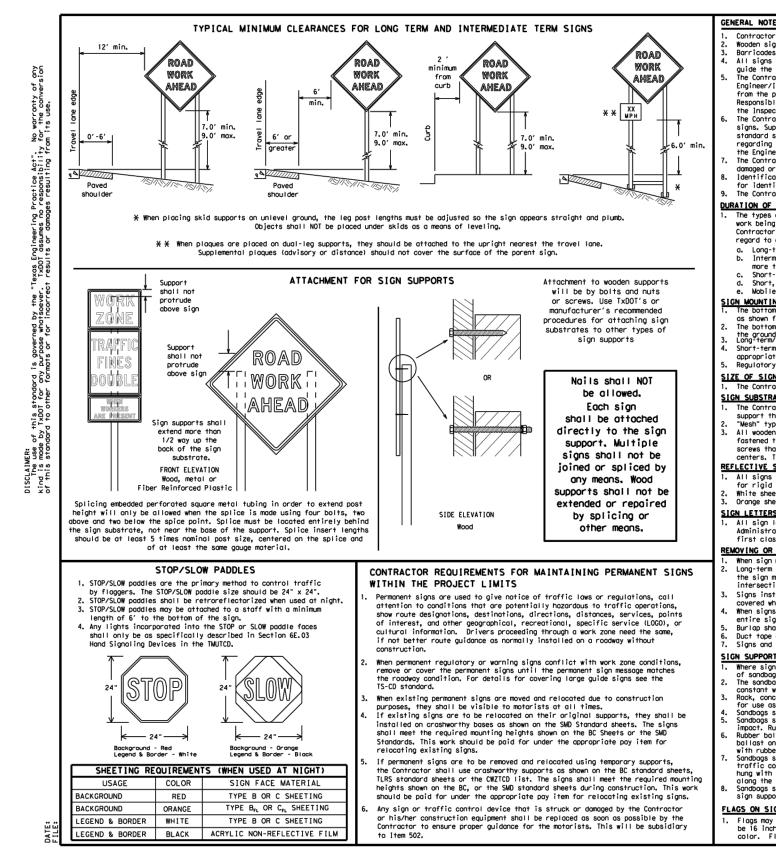


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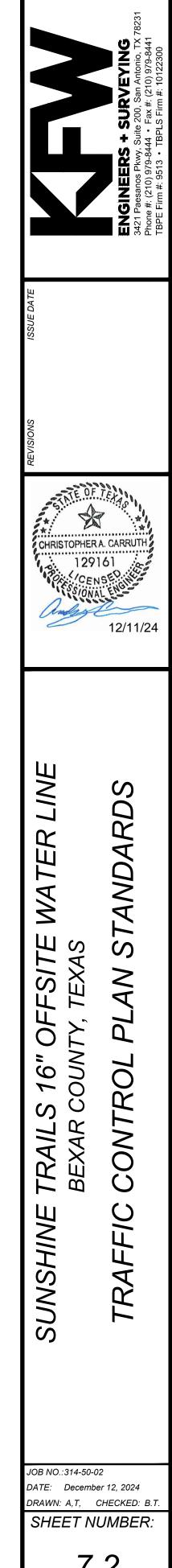
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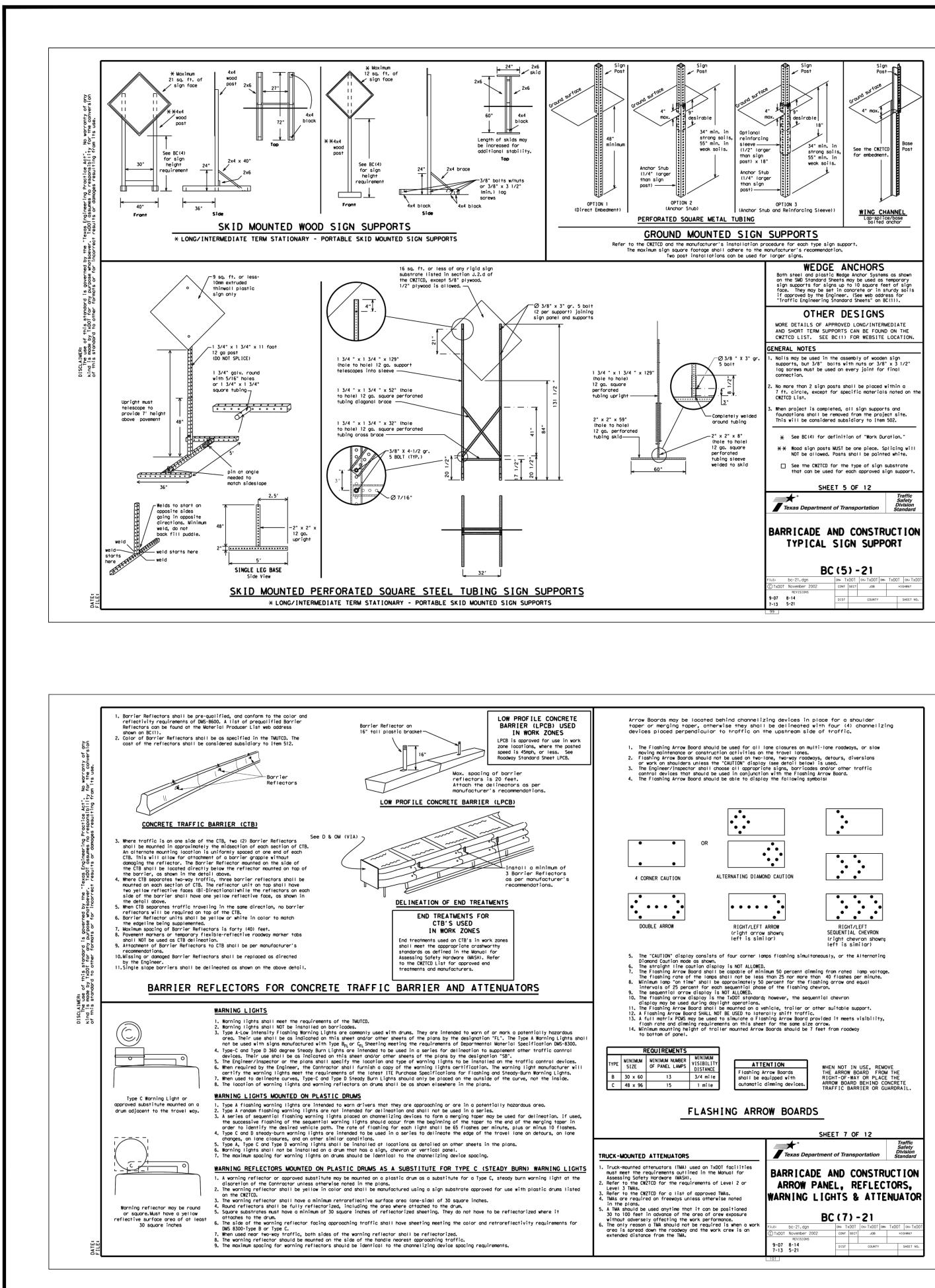


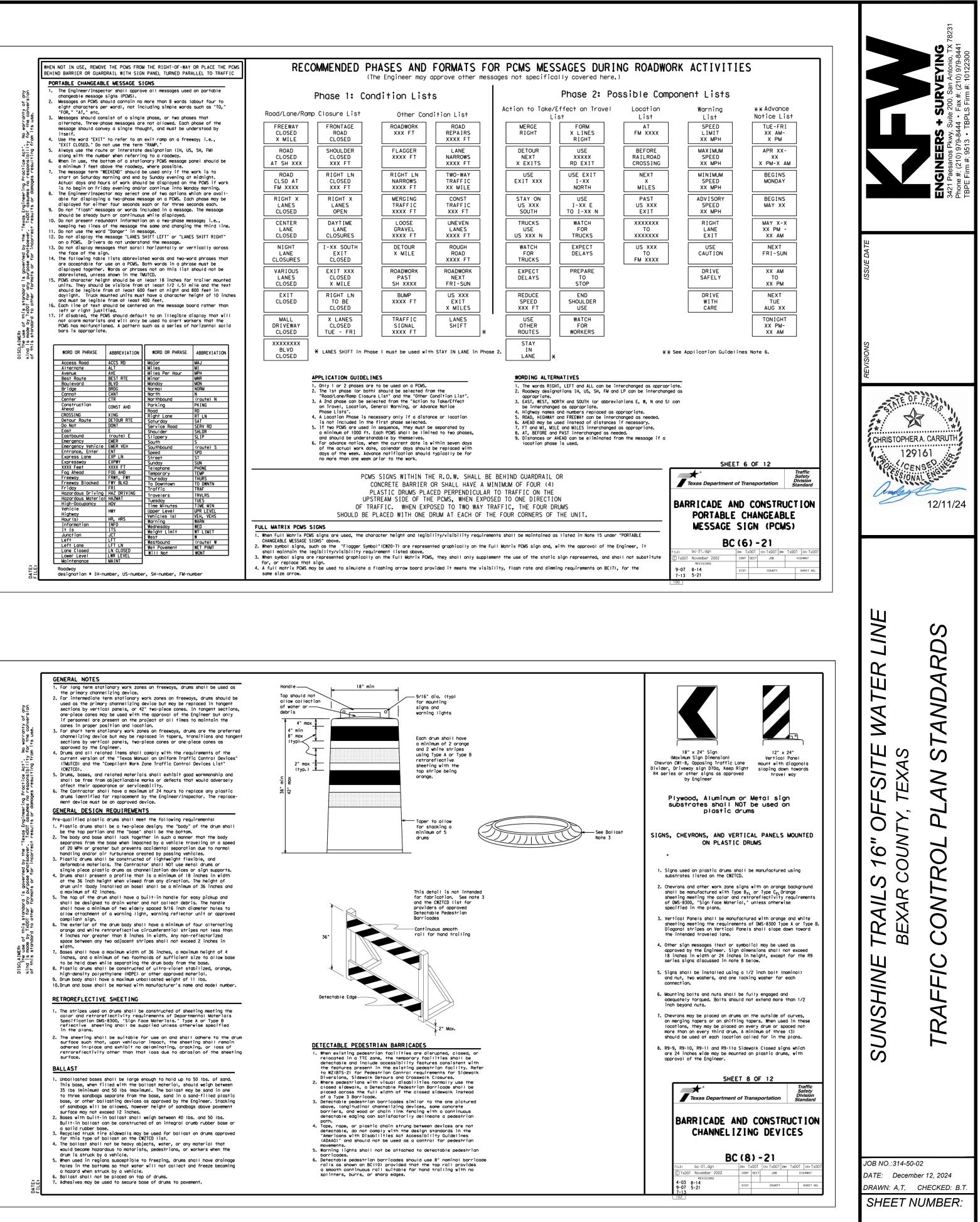


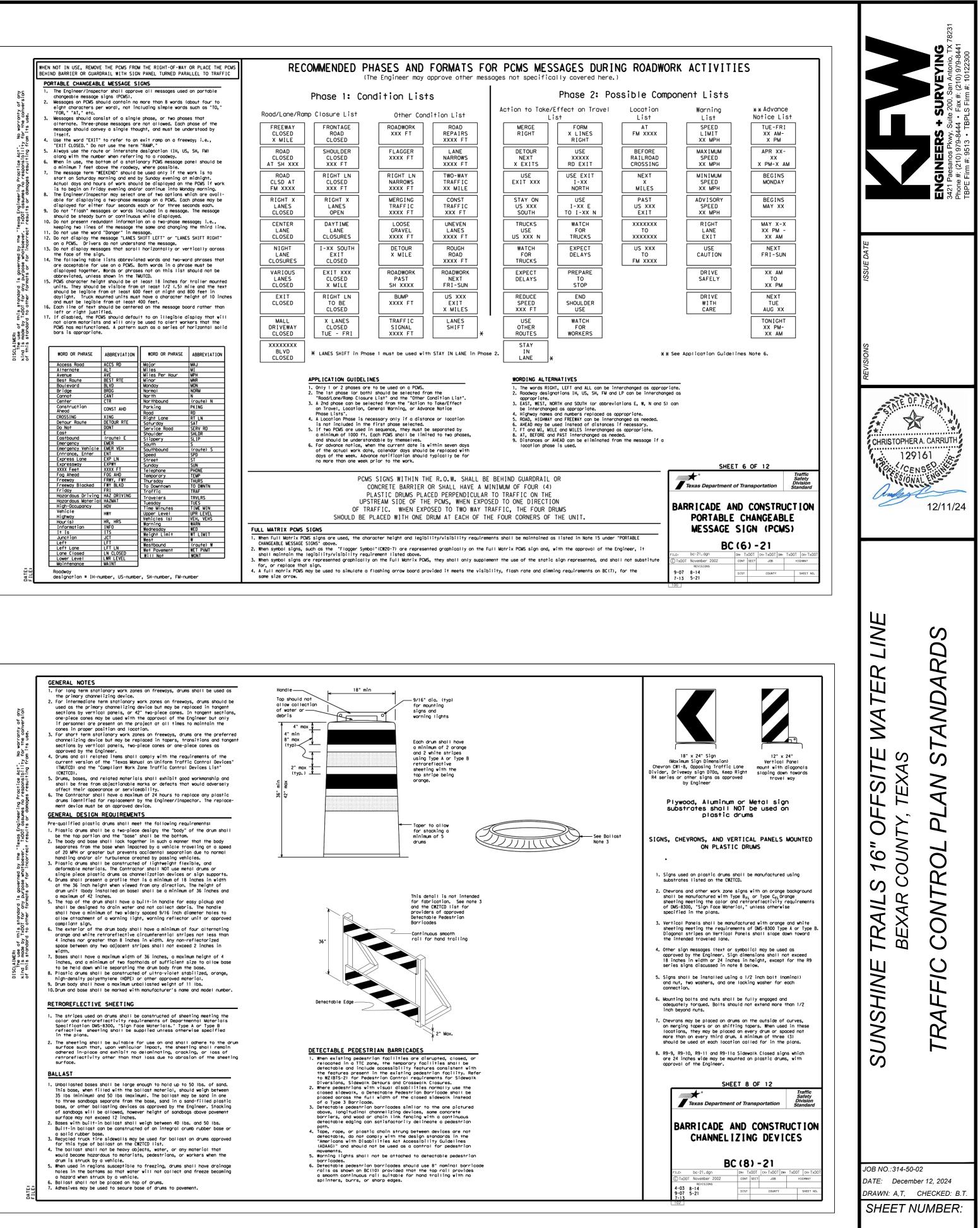


S FOR WORK ZONE SIGNS	
shall install and maintain signs in a straight and plumb condition and/on posts shall be painted white.	or as directed by the Engineer.
shall NOT be used as sign supports. shall be installed in accordance with the plans or as directed by the Englished by the Englished by the state of	gineer. Signs shall be used to regulate, worn, and
rraveling public safely through the work zone, ctor may furnish either the sign design shown in the plans or in the "Sta spector may require the Contractor to furnish other work zone signs tha ans, Any variation in the plans shall be documented by written agreemen	t are shown in the TMUTCD but may have been omitted
Person. All changes must be documented in writing before being implement or's Ix00T diary and having both the Inspector and Contractor initial ar tor shall furnish sign supports listed in the "compliant Work Zone Traf.	nted. This can include documenting the changes in nd date the agreed upon changes.
orts for temporary lorge roadside signs shall meet the requirements det eets.The Contractor shall install the sign support in accordance with th stallation procedures, the Contractor shall furnish the Engineer a copy	ailed on the Temporary Large Roadside Signs (TLRS) he manufacturer's recommendations. If there is a question
r can verify the correct procedures are being followed. tor is responsible for installing signs on approved supports and replac morred reflective sheeting as directed by the Engineer/Inspector. ion markings may be shown only on the back of the sign substrate. The m ication sholl be 1 inch.	
Norshing and by the "Texas Monual on Uniform Traffic Con	
is sign supports, sign mounting height, the size of signs, and the type of performed. The Engineer is responsible for selecting the appropriate siz is responsible for ensuring the sign support, sign mounting height and s rashworthiness and duration of work requirements.	f sign substrates can vary based on the type of ze sign for the type of work being performed. The
rm stationary - work that occupies a location more than 3 days, diate-term stationary - work that occupies a location more than one day an one hour.	
erm stationary - daytime work that occupies a location for more than 1 l duration - work that occupies a location up to 1 hour. - work that moves cantinuously or intermittently (stopping for up to app	
3 HEIGHT of Long-term/intermediate-term signs shall be at least 7 feet, but not n supplemental plaques mounted below other signs	more than 9 feet, above the paved surface, except
r supplemental plaques mounted below other signs, of Short-term/Short Duration signs shall be a minimum of 1 foot above th accordinate term Signs mou be used to live of Short-term/Chart Duration	
https://doi.org/10.1000/000000000000000000000000000000	moved at the end of the workday or raised to
signs shall be mounted at least 7 feet, but not more than 9 feet, above	-
for shall furnish the sign sizes shown on BC (2) unless otherwise shown ES chall approve the size substrate is installed in considered with the	
tor shall ensure the sign substrate is installed in accordance with the t is being used. The CWZICD lists each substrate that can be used an the materials are NOT an approved sign substrate, regardless of the tightne individual sign panels fabricated from 2 or more pieces shall have one o the back of the sign and extending fully across the sign. The cleat shu do not penetrate the face of the sign panel. The screws shall be placed	e different types and models of sign supports. ess of the weave. or more plywood cleat, 1/2" thick by 6" wide, all be attached to the back of the sign using wood
e Engineer may approve other methods of splicing the sign face. HEETING	
hall be retroreflective and constructed of sheeting meeting the color and igns or DMS-8310 for roll-up signs. The web address for DMS specificativing, meeting the requirements of DMS-8300 Type A, shall be used for signifing, meeting the requirements of DMS-8300 Type B_{FL} or Type C_{FL} , shall t	ons is shown on BC(1). ns with a white background.
ters and numbers shall be clear, and open rounded type uppercase alphad on (FHMA) and as published in the "Standard Highway Sign Design for Te: workmanship in accordance with Department Standards and Specifications. OVERING	xas" manual. Signs, letters and numbers shall be of
sortented essages may be confusing or do not apply, the signs shall be removed or tationary or intermediate stationary signs installed on square metal tul ssage is not applicable. This technique may not be used for signs inst ns where the sign may be seen from approaching traffic. Iled on wooden skids shall not be turned at 90 degree angles to the road	bing may be turned away from traffic 90 degrees when alled in the median of divided highways or near any
n not required. nre covered, the material used shall be apaque, such as heavy mil black face and maintain their opaque properties under automobile headlights (plastic, or other materials which will cover the
I NOT be used to cover signs. r other adhesive material shall NOT be affixed to a sign face.	
chor stubs shall be removed and holes backfilled upon completion of wor NEIGHTS	SHEET 4 OF 12
supports require the use of weights to keep from turning over, the use with dry, cohesionless sand should be used. s will be tied shut to keep the sand from spilling and to maintain a ight.	Traffic Safety Division
uit, ie, iron, steel or other solid objects shall not be permitted sign support weights. uid weigh a minimum of 35 lbs and a maximum of 50 lbs.	Texas Department of Transportation Standard
bill be made of a durable material that tears upon vehicular ber (such as tire inner tubes) shall NOT be used. sist designed for channelizing devices should not be used for bortable sign supports. Sign supports designed and monufactured bases may be used when shown on the CWZICD list. all only be placed along or laid over the base supports of the	BARRICADE AND CONSTRUCTION TEMPORARY SIGN NOTES
trol device and shall not be suspended above ground level or ope, wire, chains or other fasteners. Sandbags shall be placed ength of the skids to weigh down the sign support.	PC (4) = 21
ali NOT be placed under the skid and shall not be used to level ts placed on slopes. *	BC (4) - 21 FILE: bc-21.dgn DN: TXD0T CK: TXD0T DN: TXD0T CK: TXD0T
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e used to draw attention to warning signs. When used, the flag shall s square or larger and shall be orange or fluorescent red-orange in	9-07 8-14 DIST COUNTY SHEET NO.

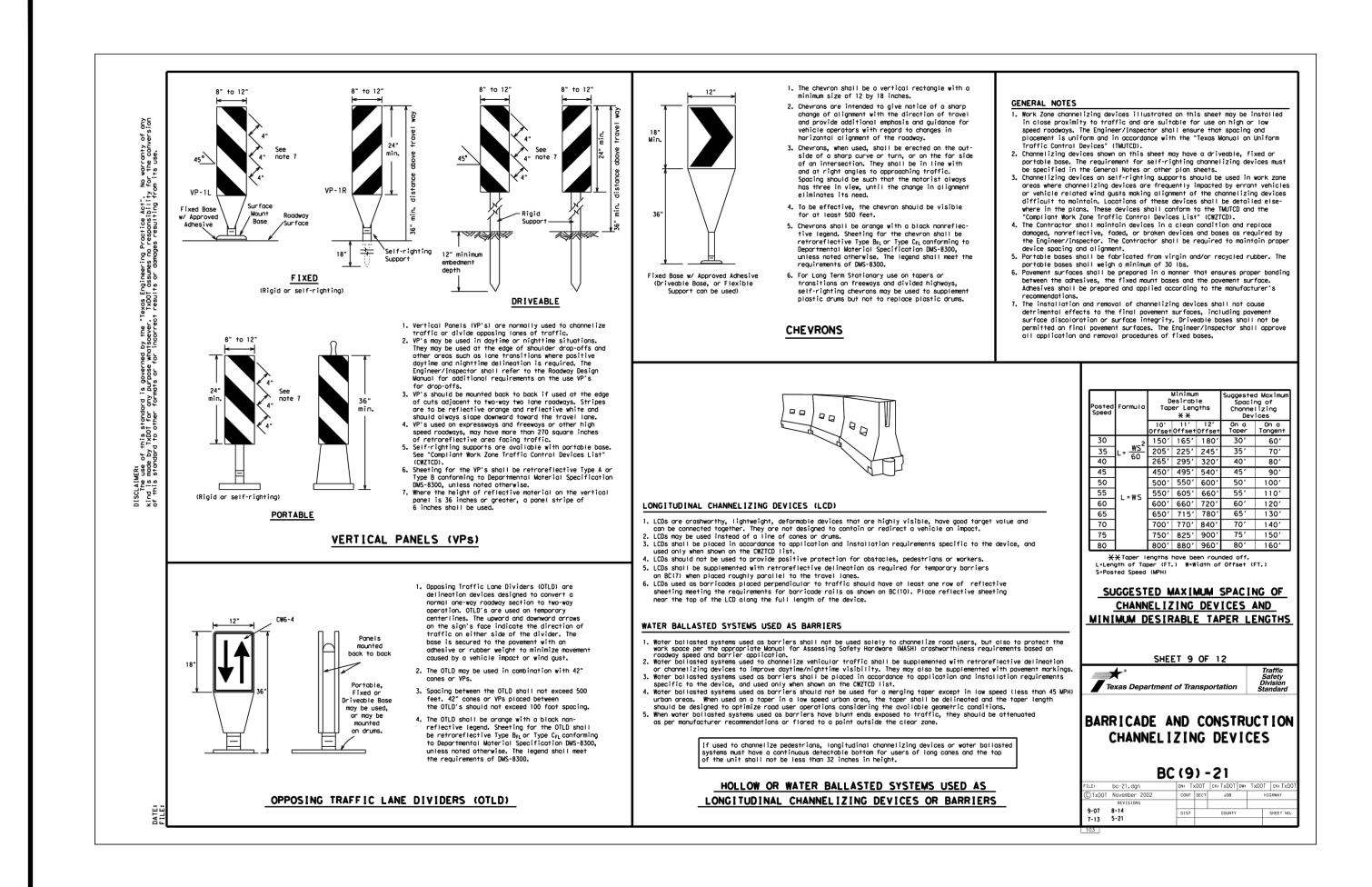


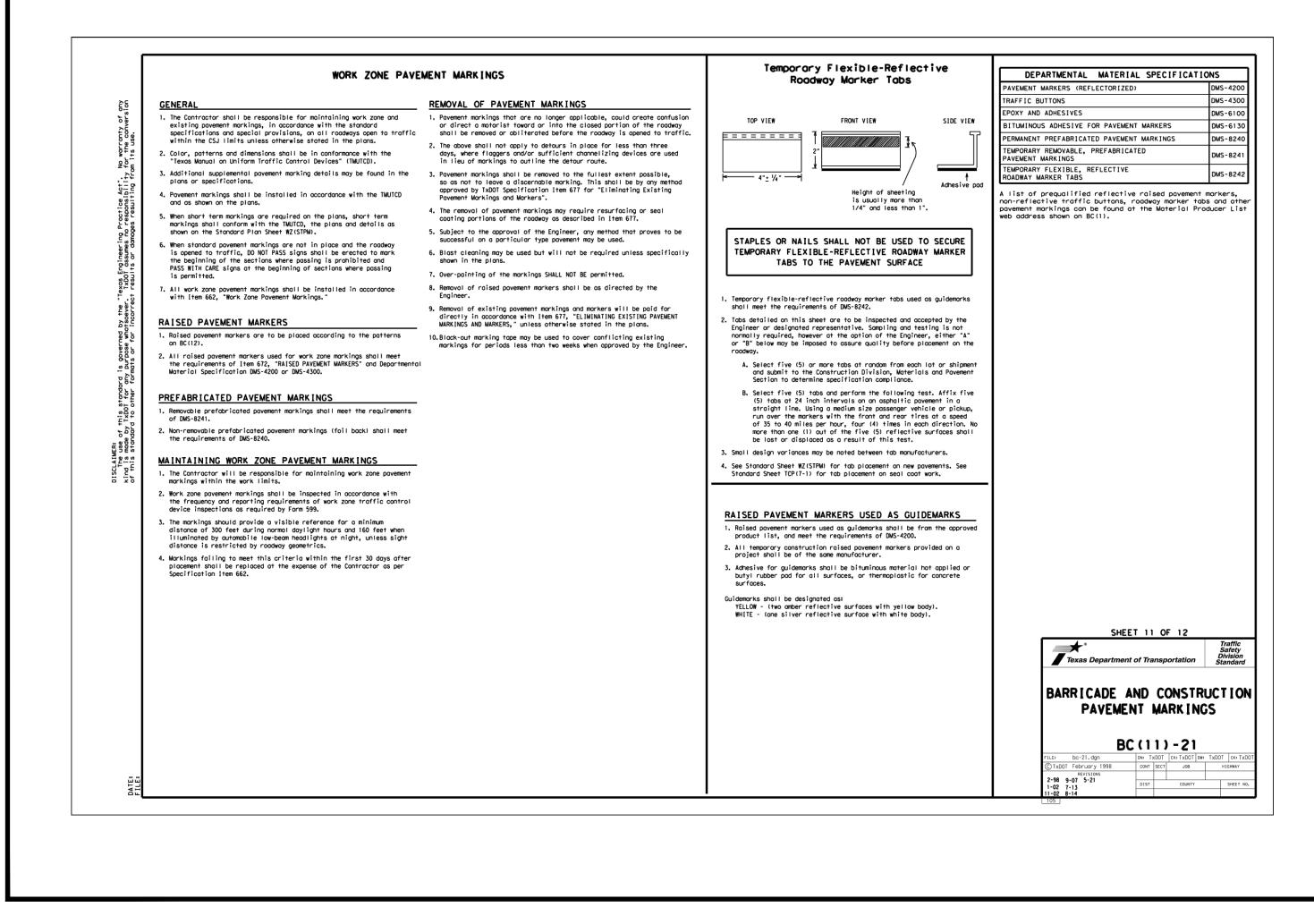


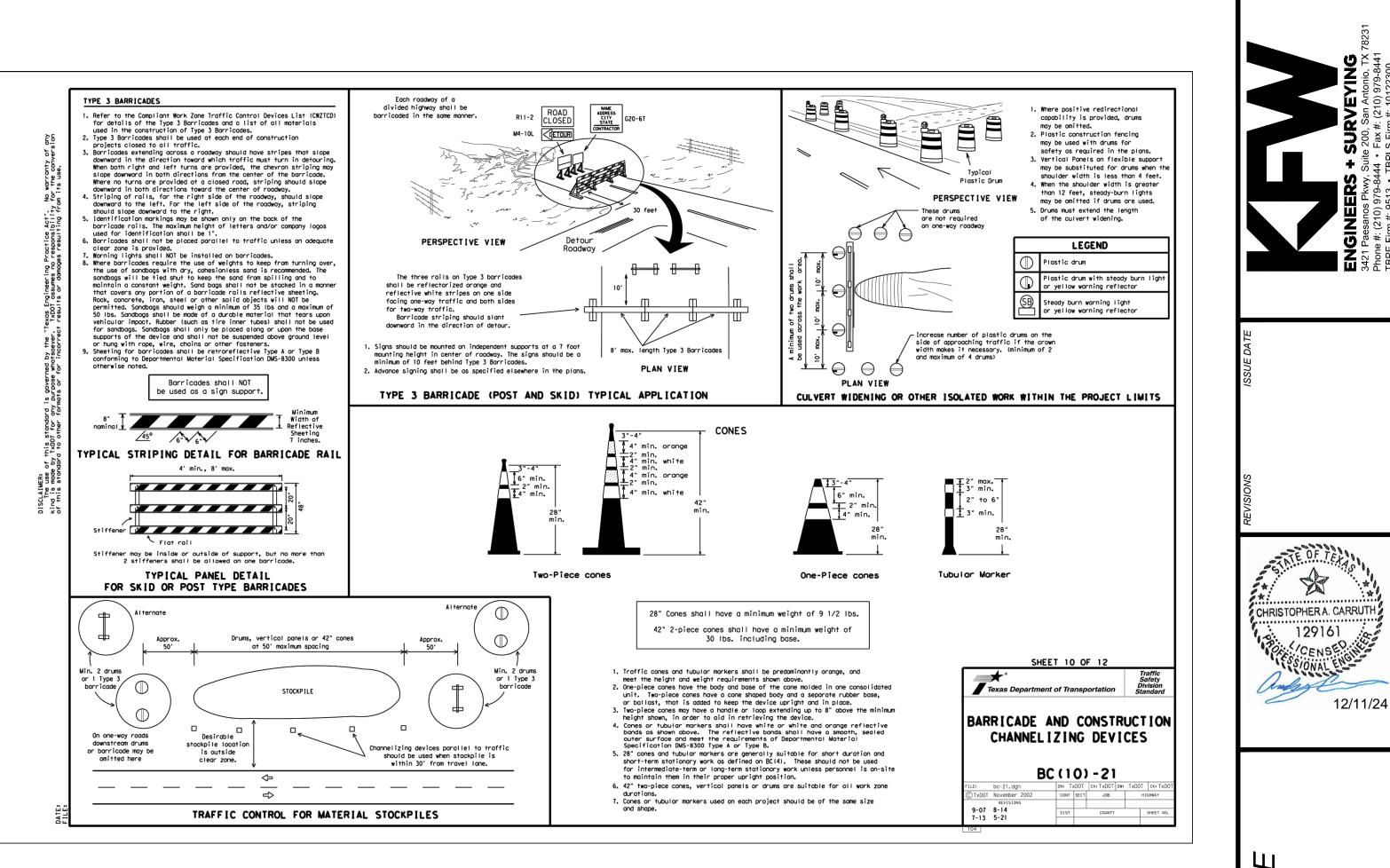




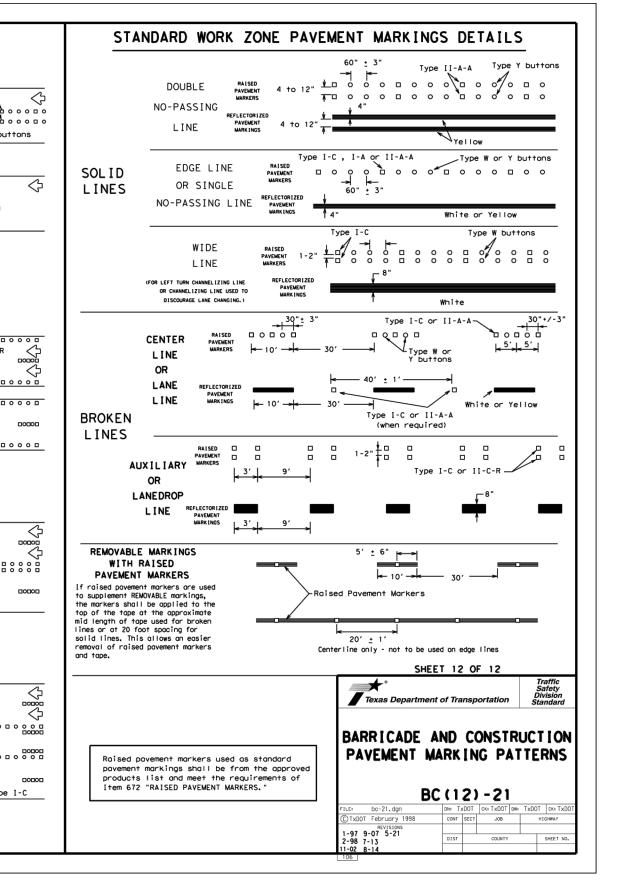
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REFLECTORIZED PAVEMENT MARKINGS - PATTER	
offern A is the TXDOT Standard, however Pattern B may be used if refabricated markings may be substituted for reflectorized pavematic center LINE & NO-PASSING Z	opproved by the Engineer. ant morkings.
	Type I-C
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White Yellow -	
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Prefabricated markings may be substituted for reflectorized pavem	LANE LINES FOR DIVIDED HIGHWAY
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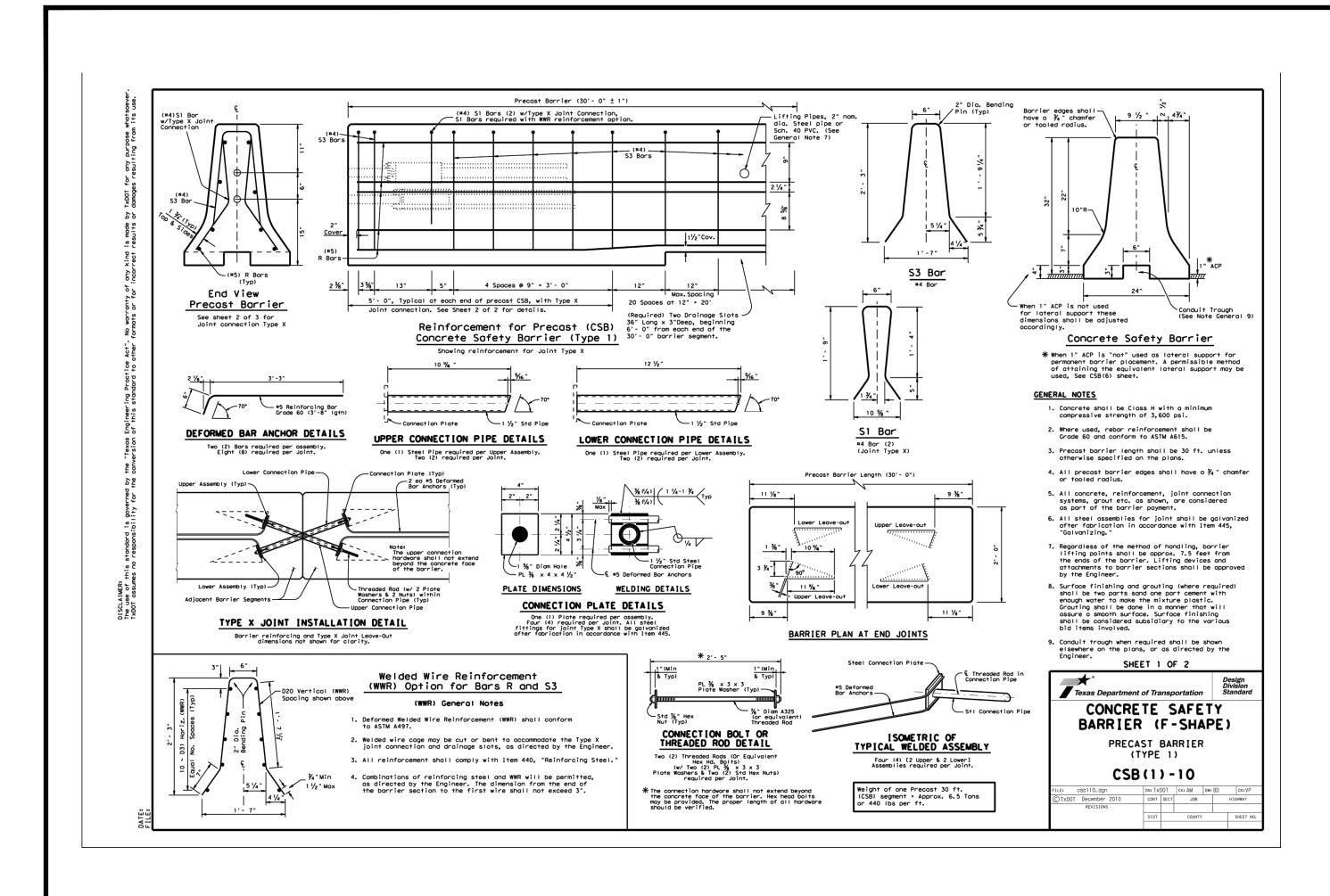


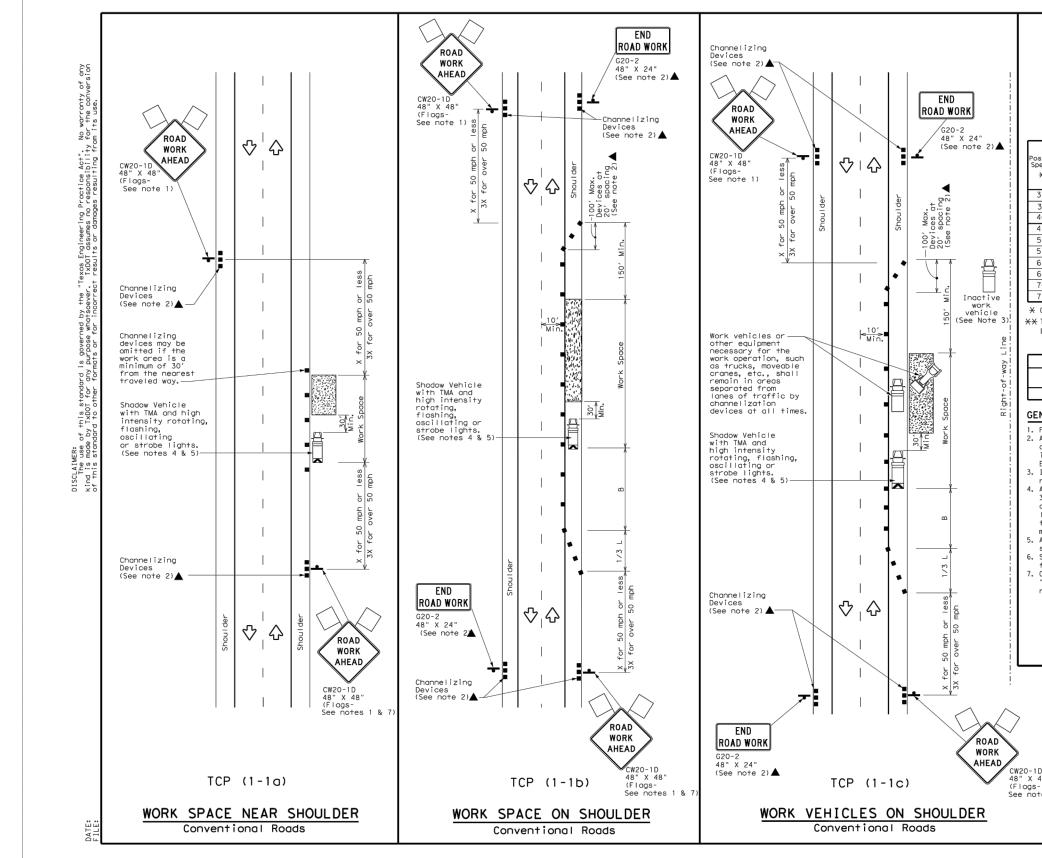


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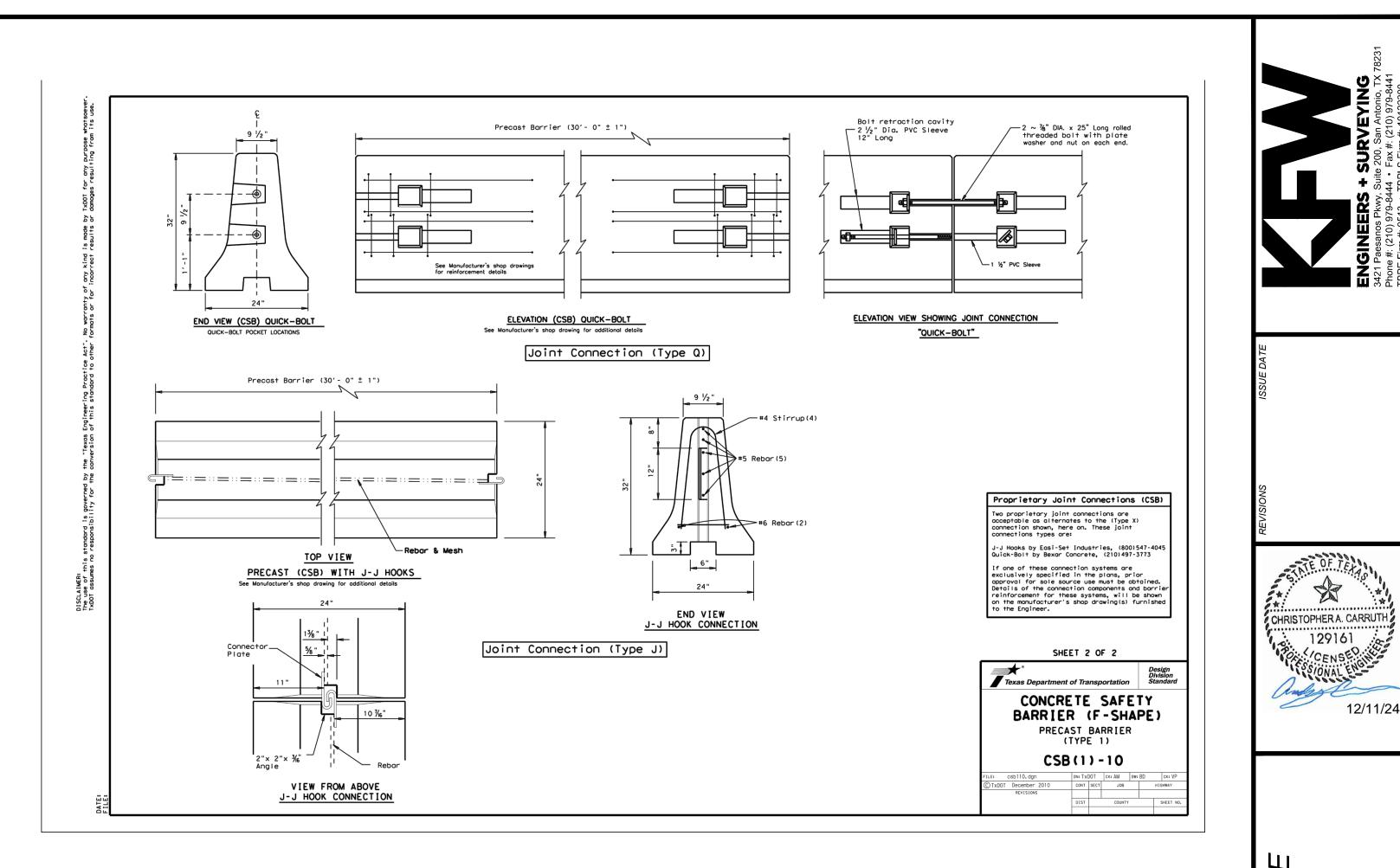
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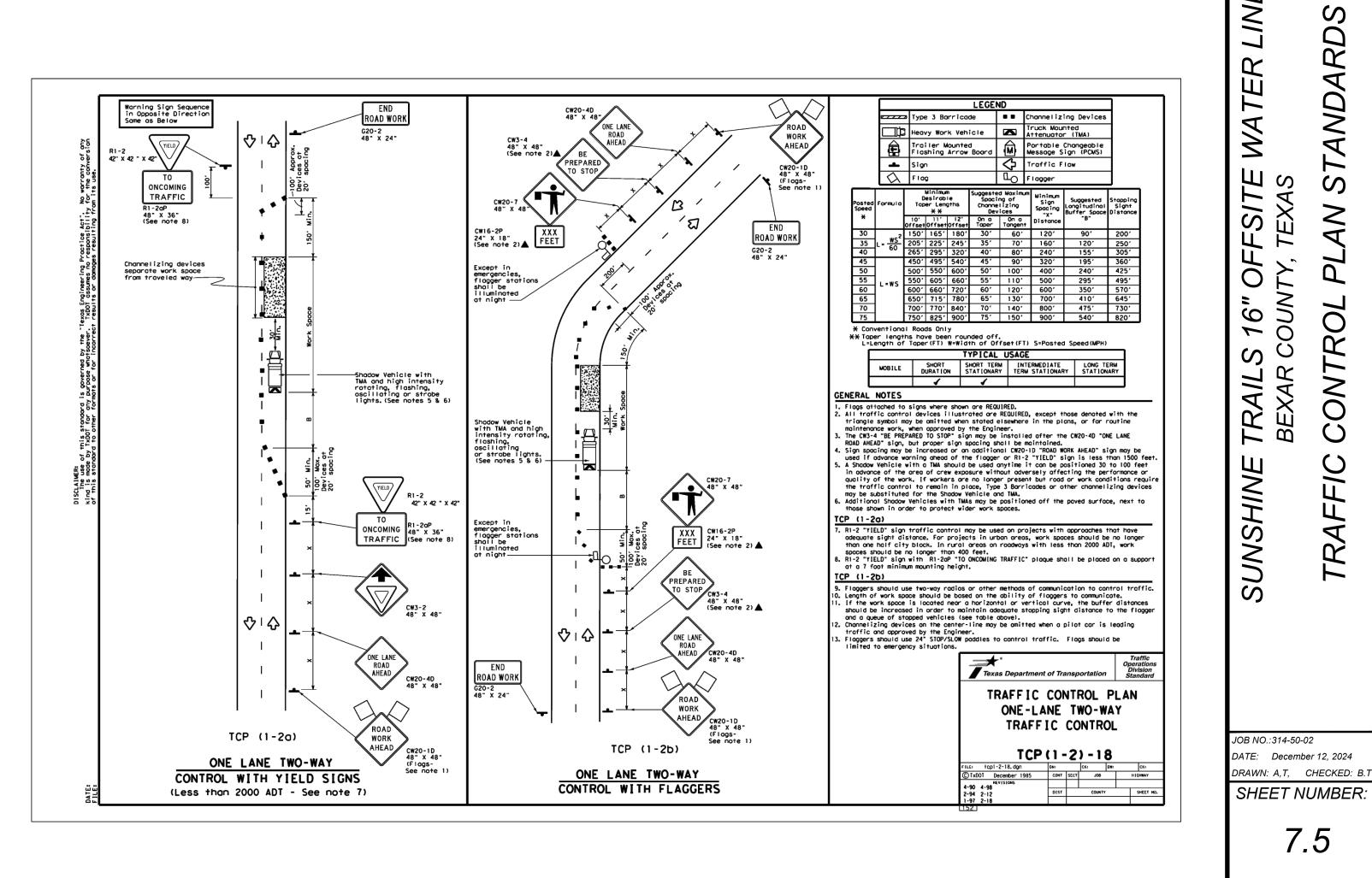
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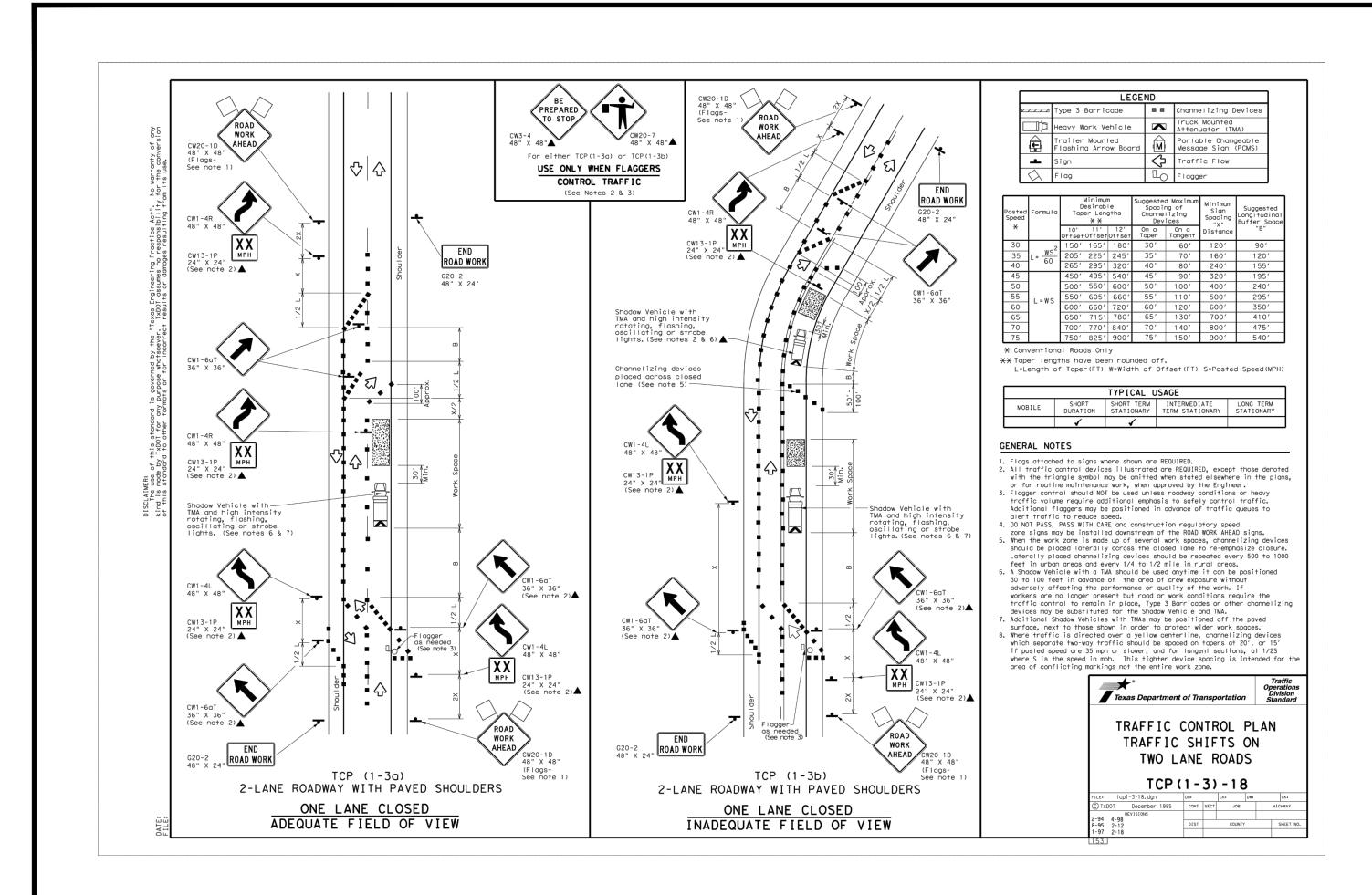
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45				450'	495'	540'	45		90'	320'	195'
50				500'	550'	600′	50		100′	400'	240'
55		L = W	s	550'	605′	660′	55'	'	110′	500'	295'
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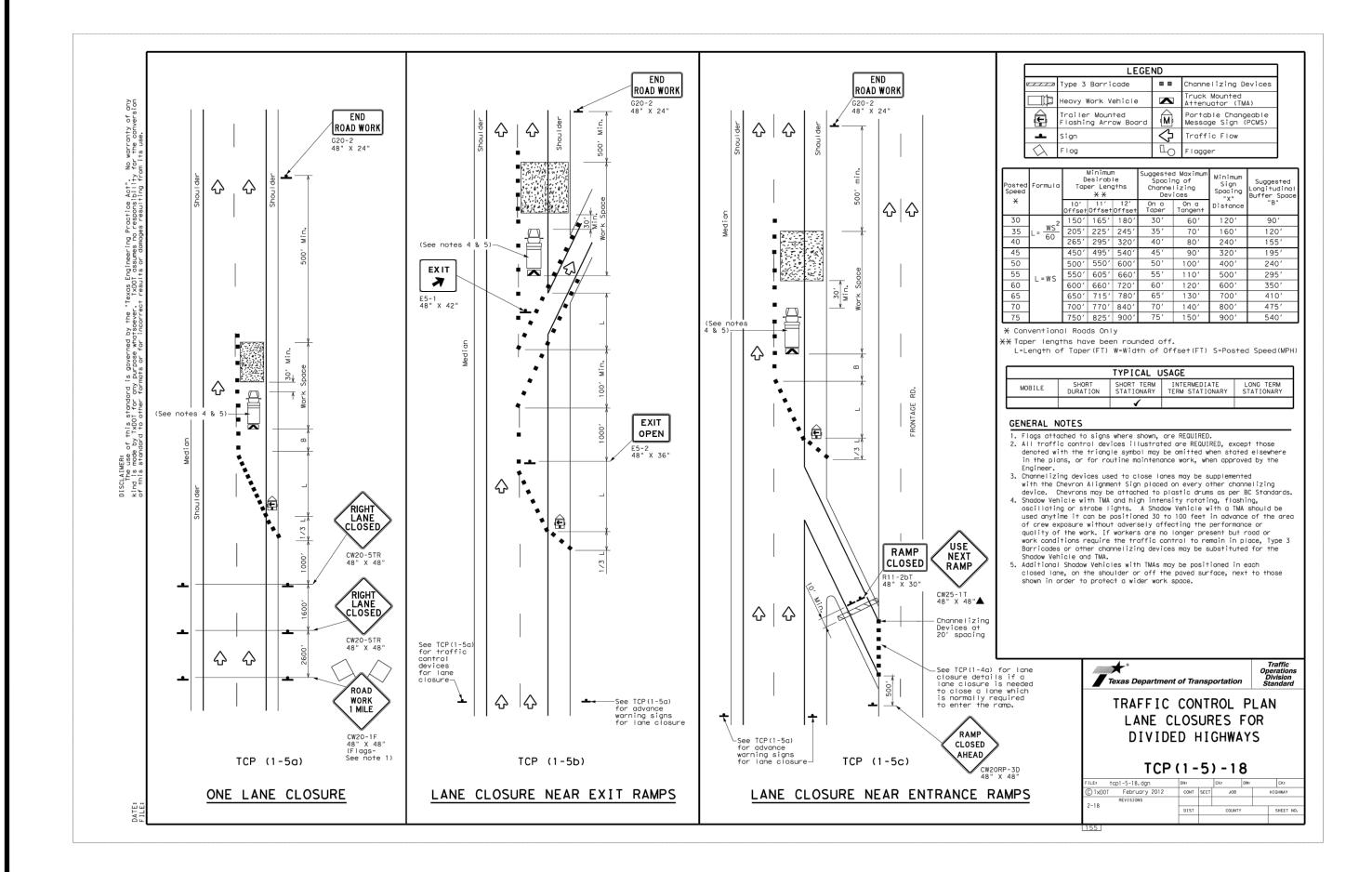




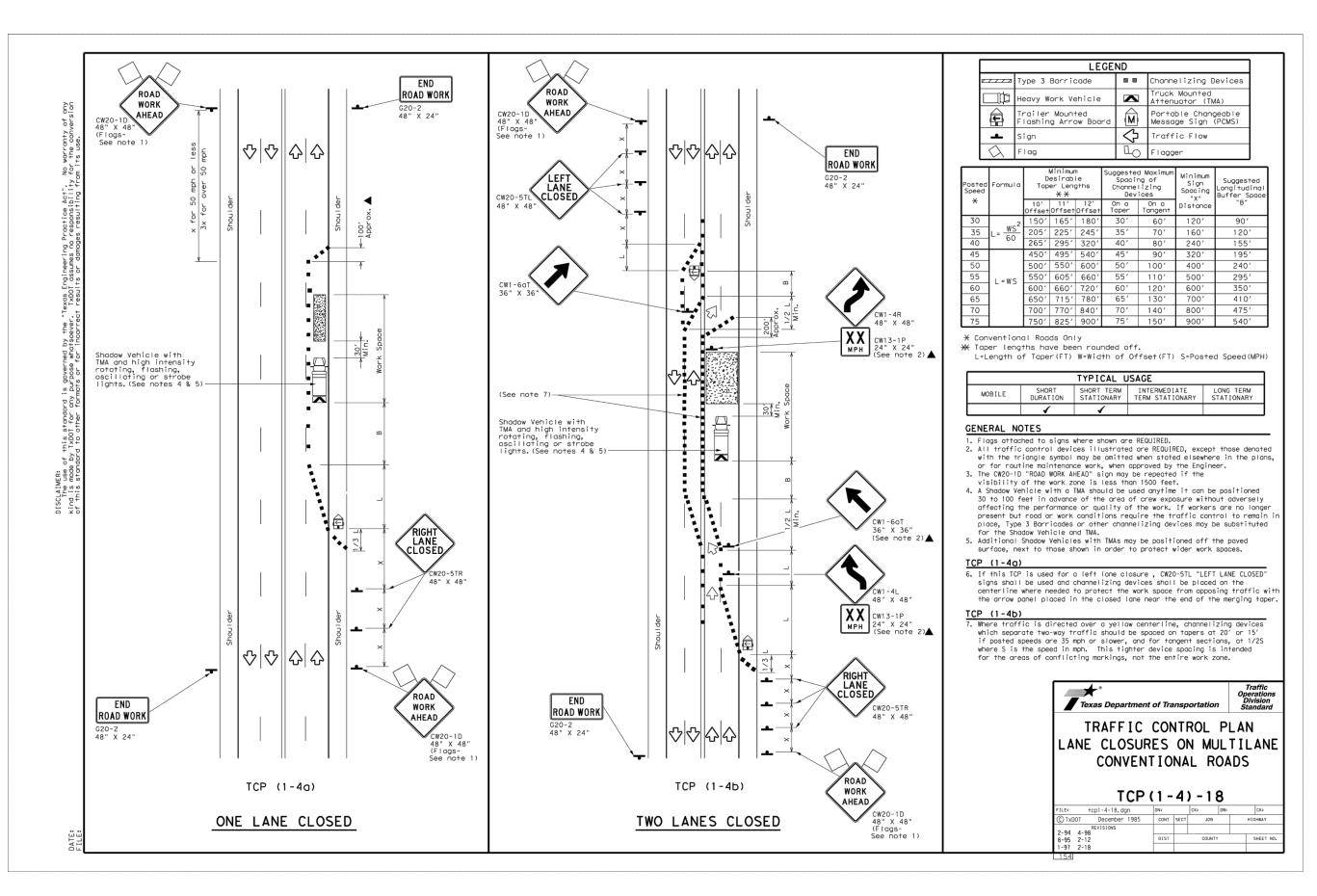
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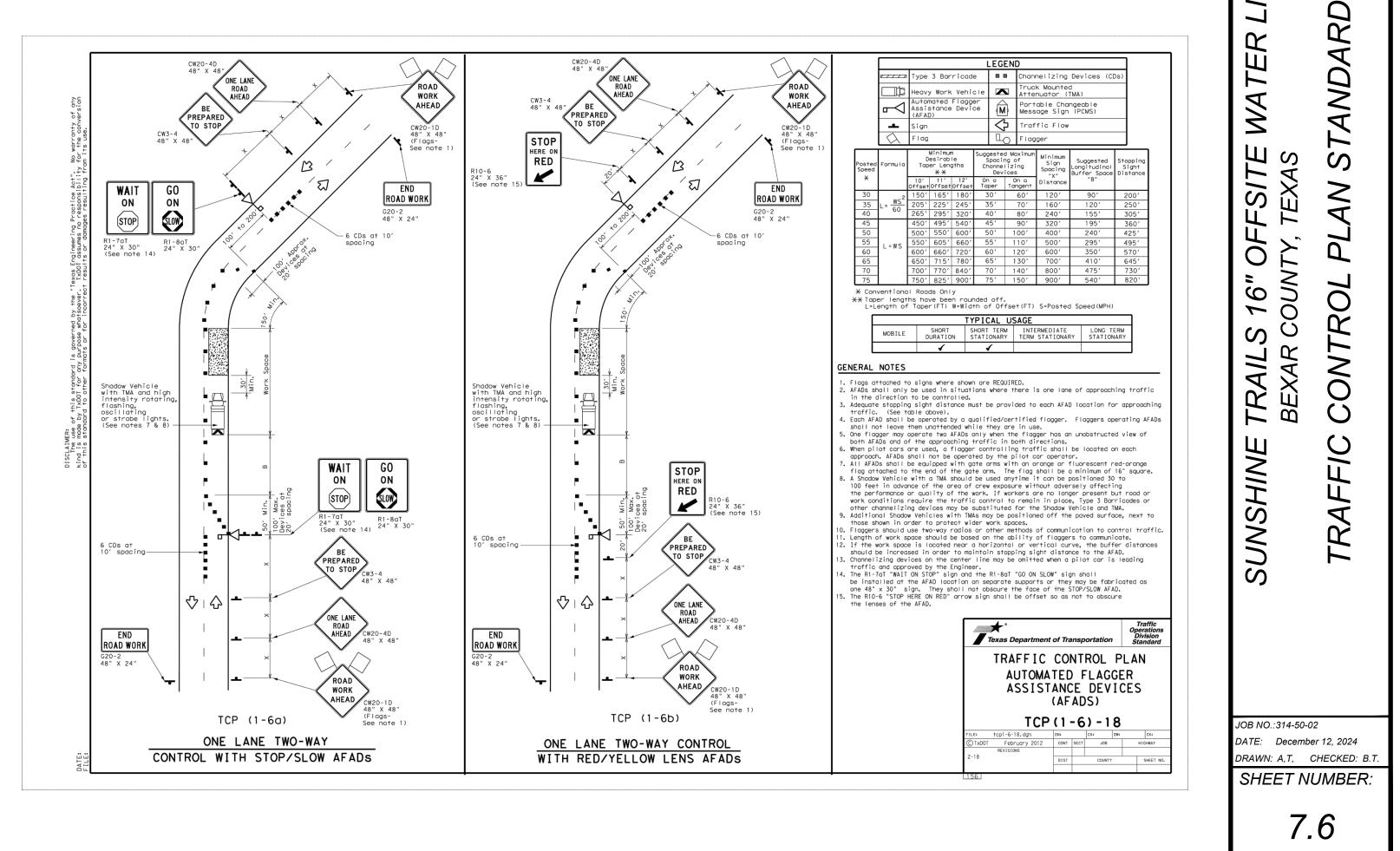
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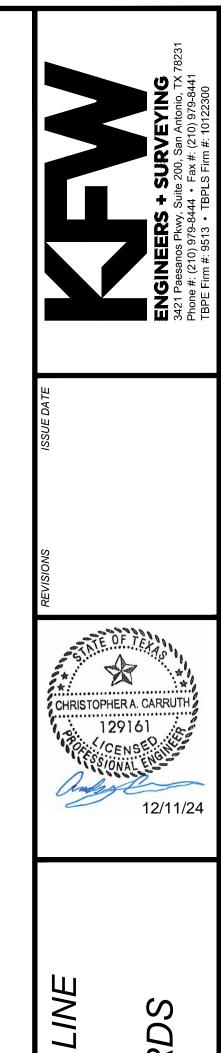


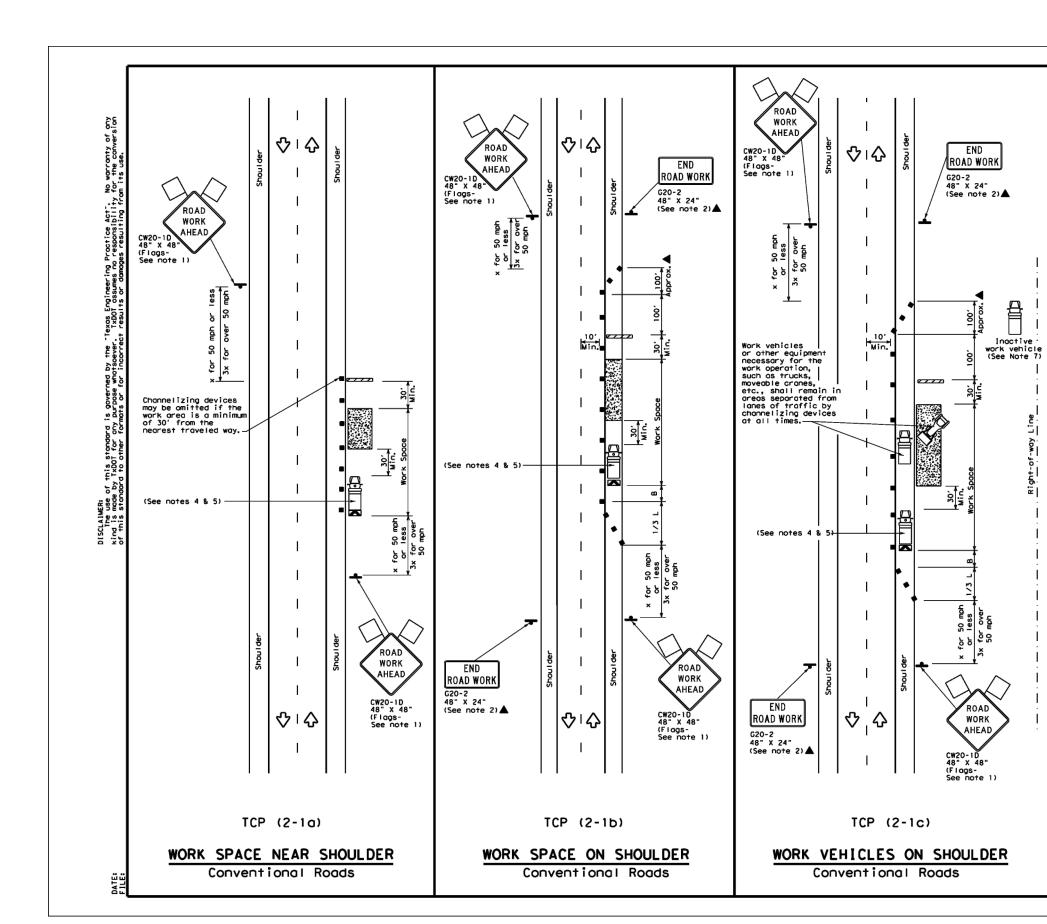


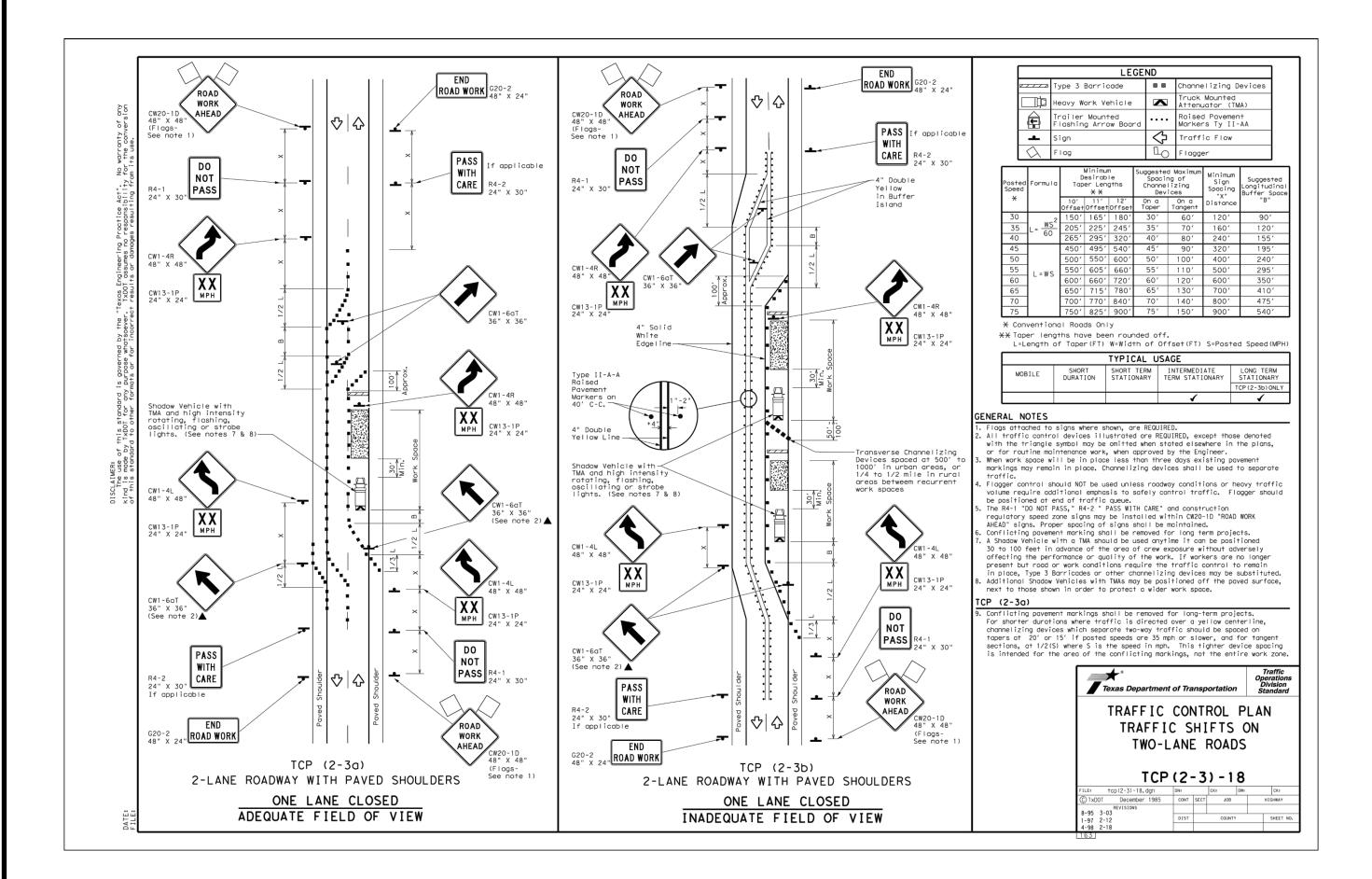
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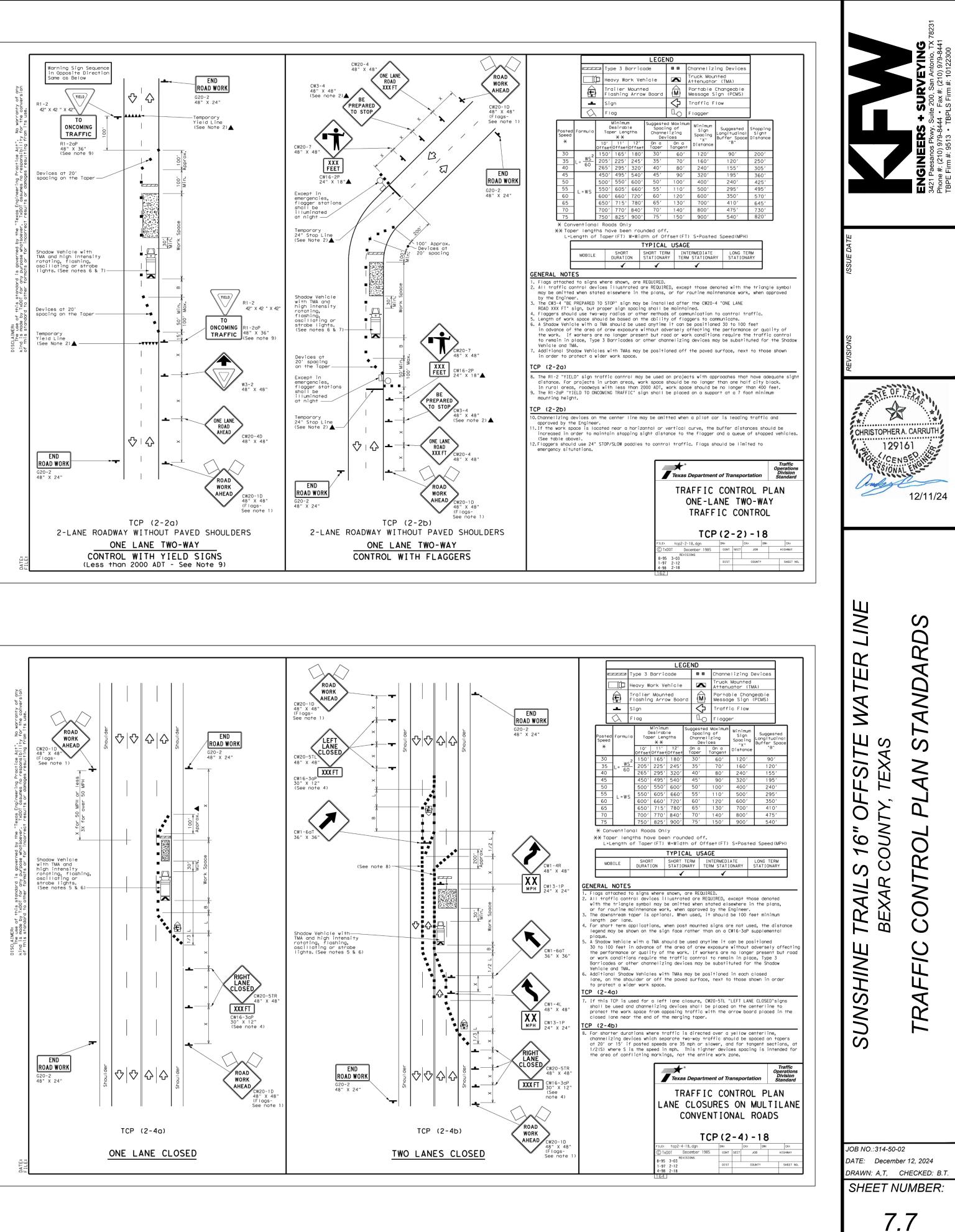


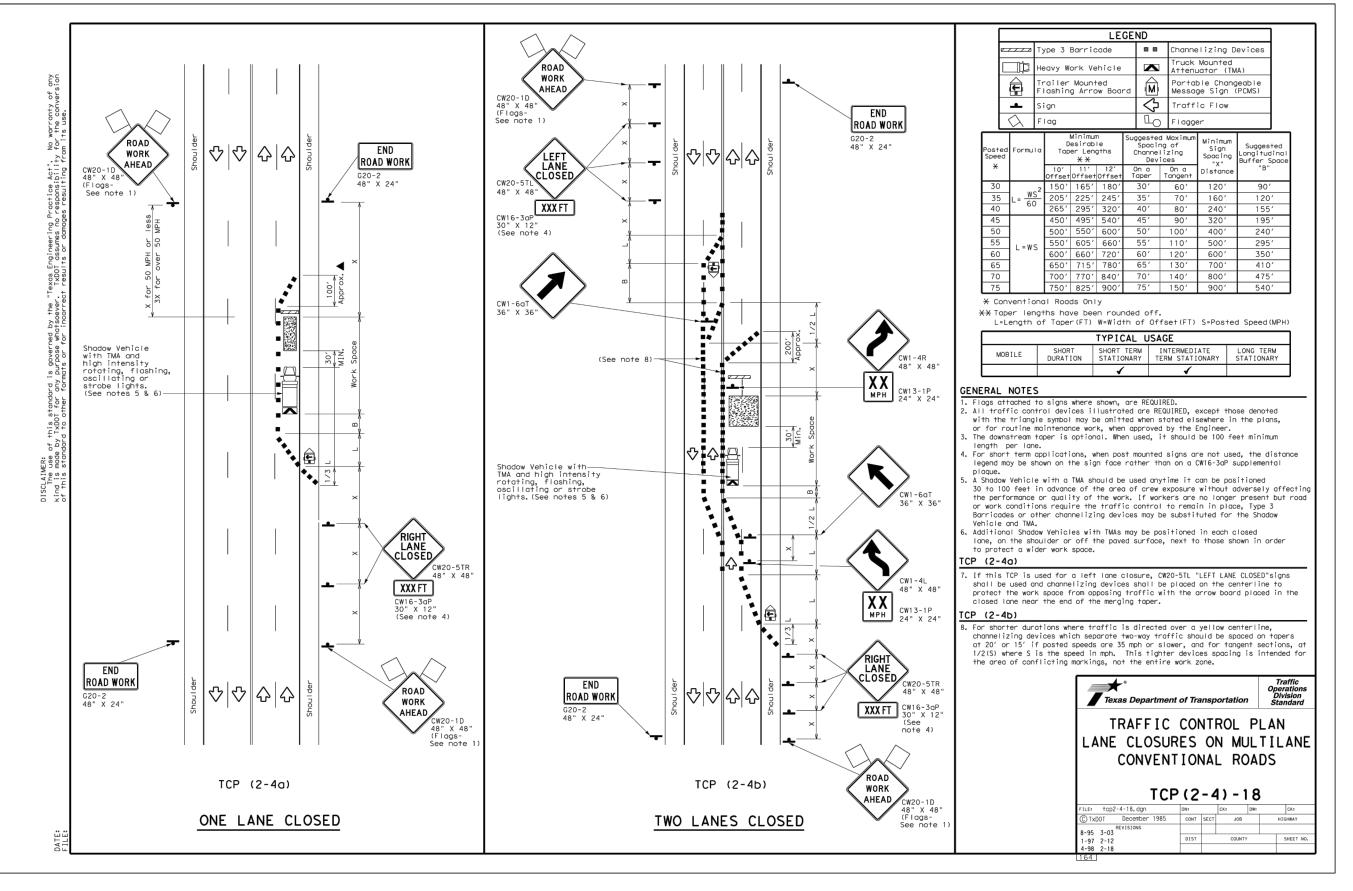


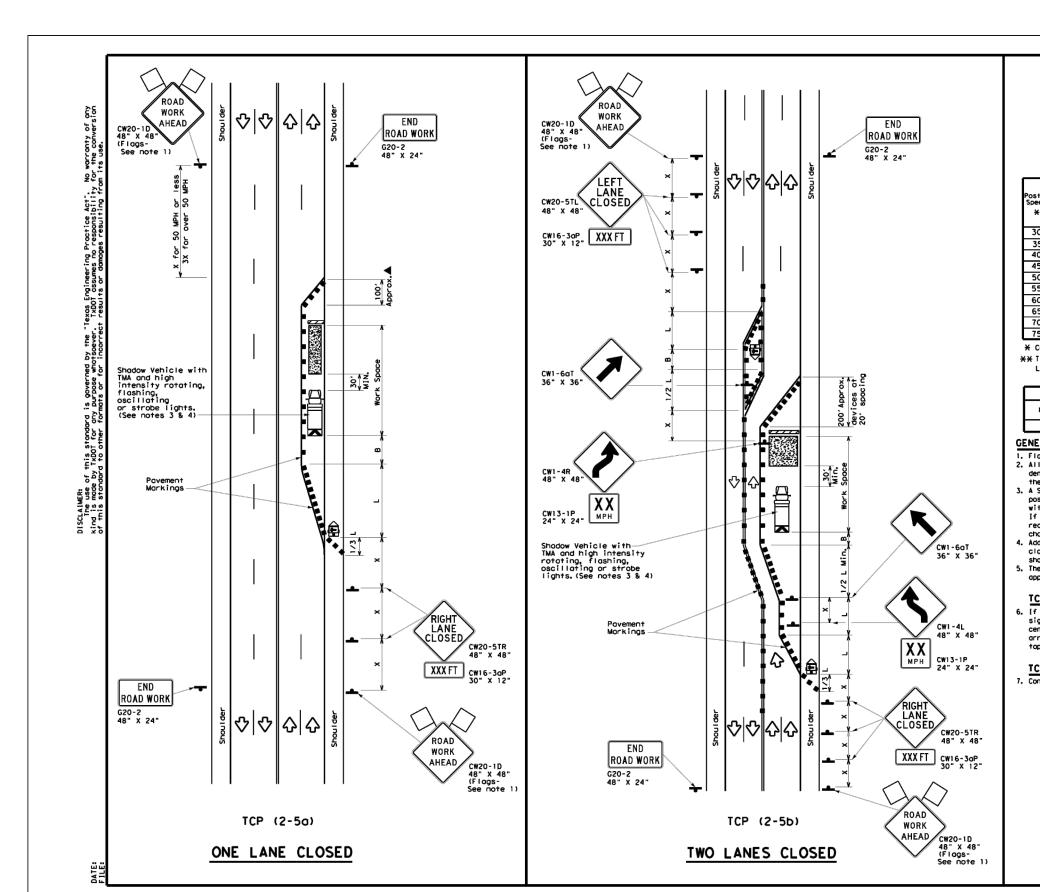


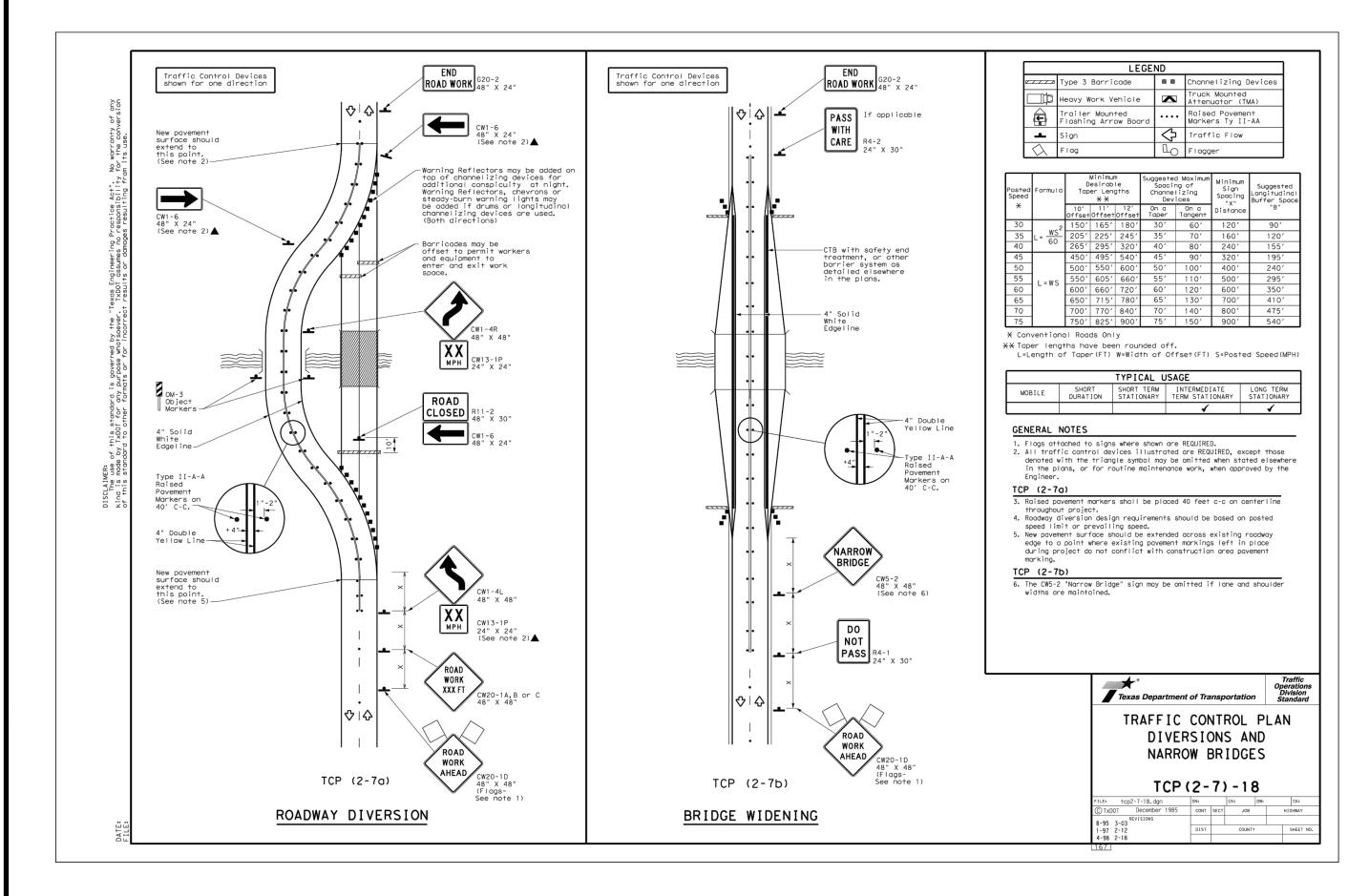


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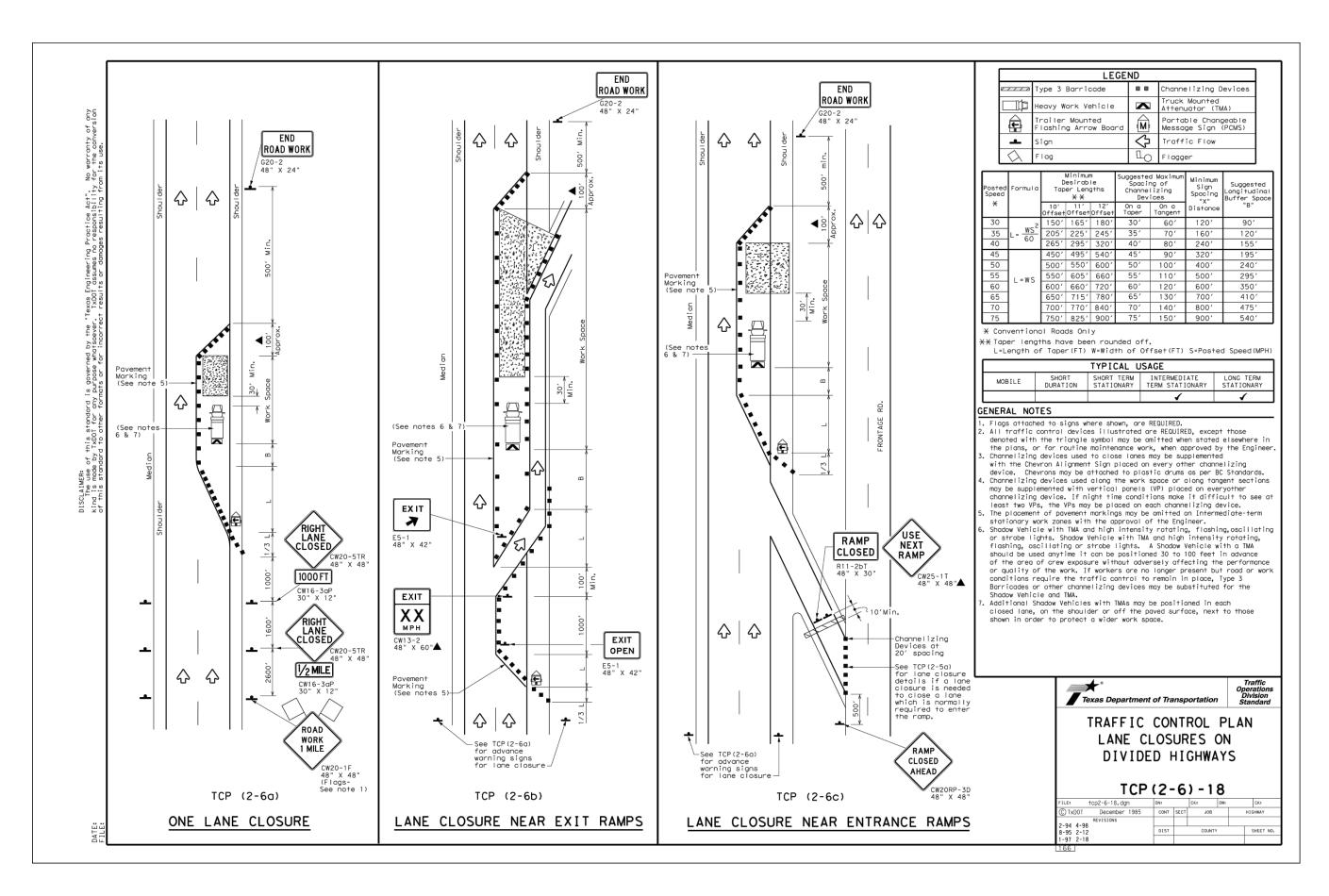


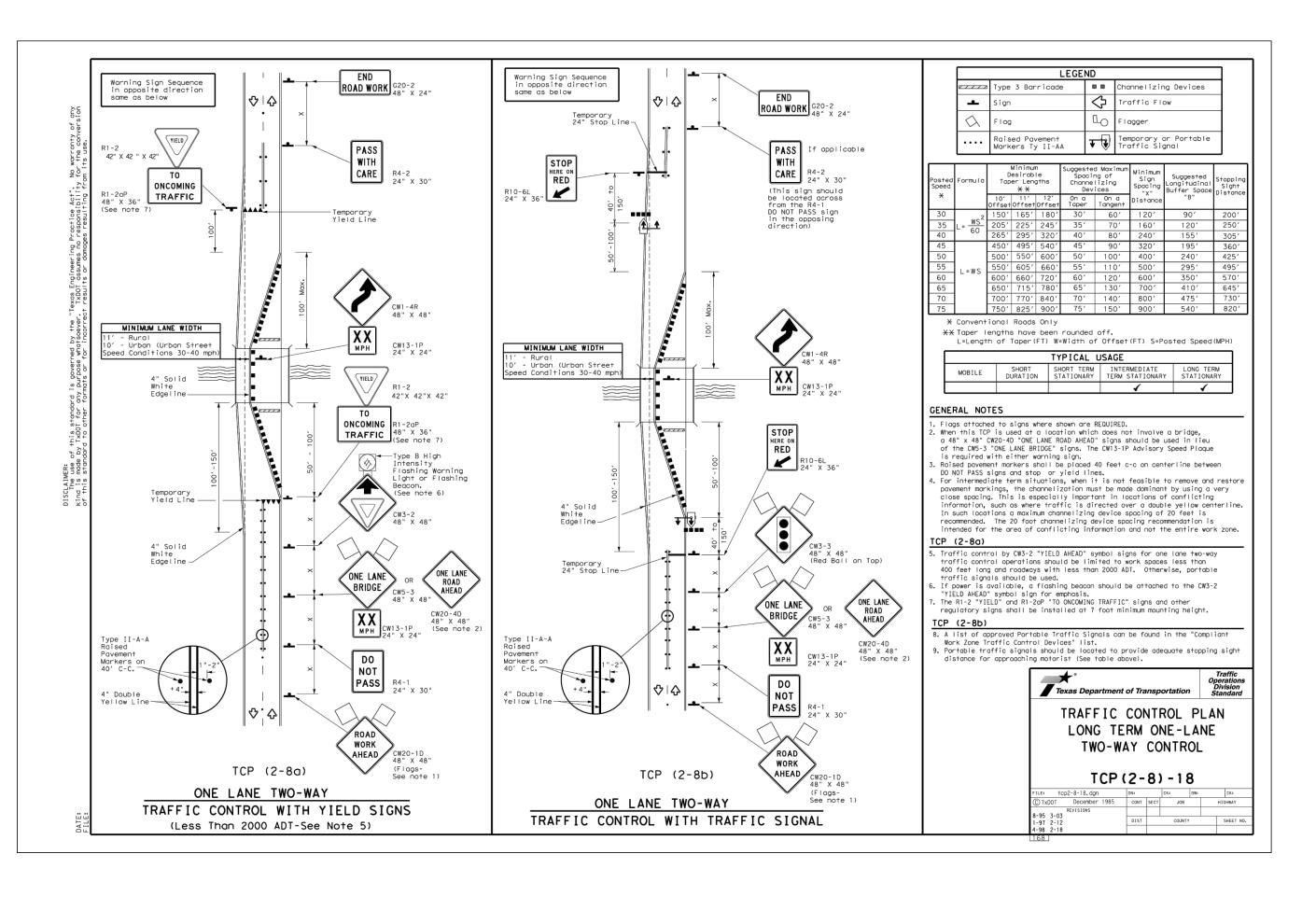
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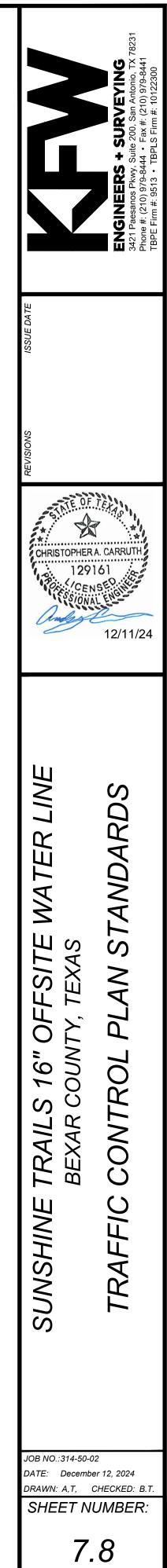
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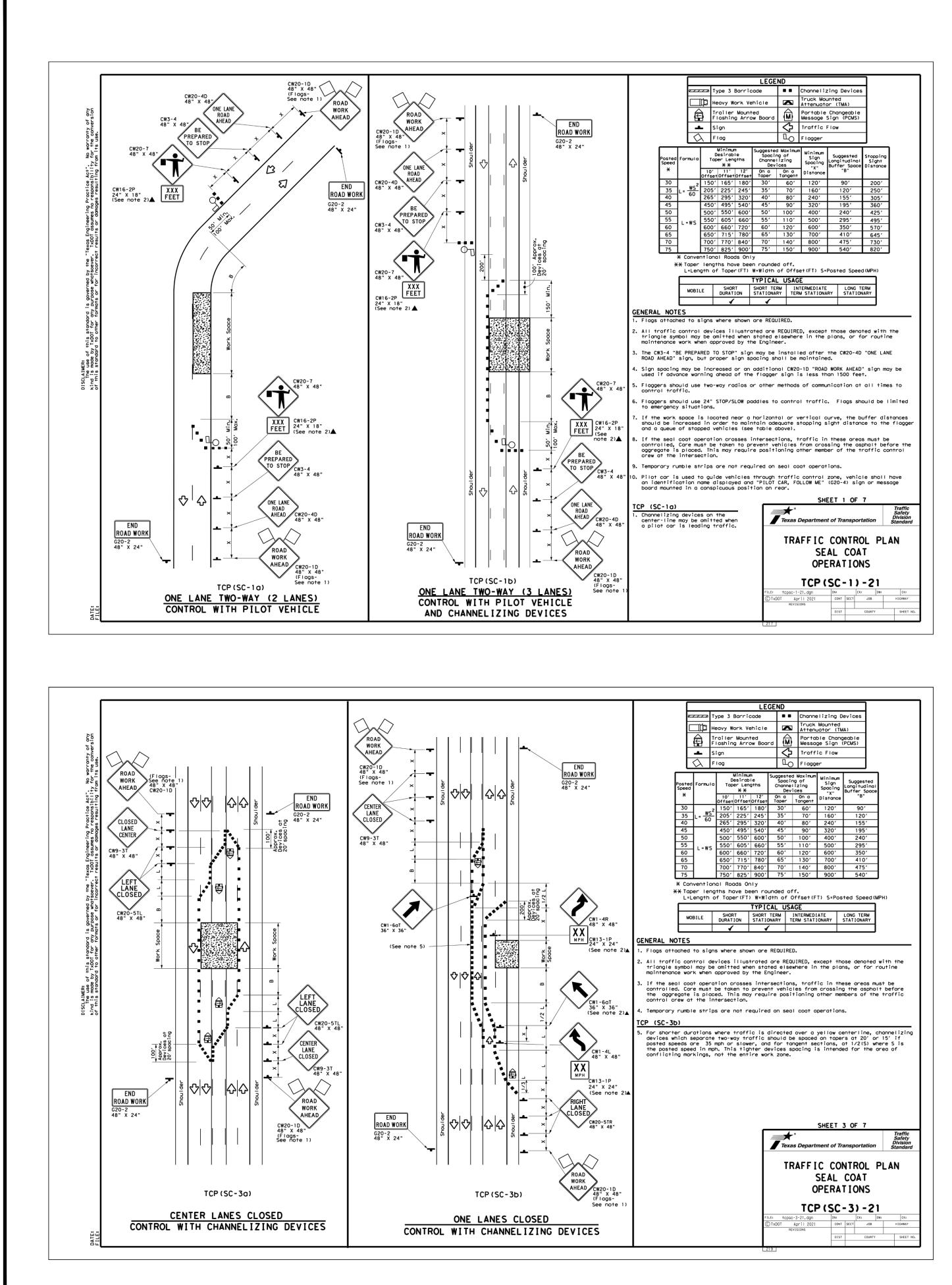
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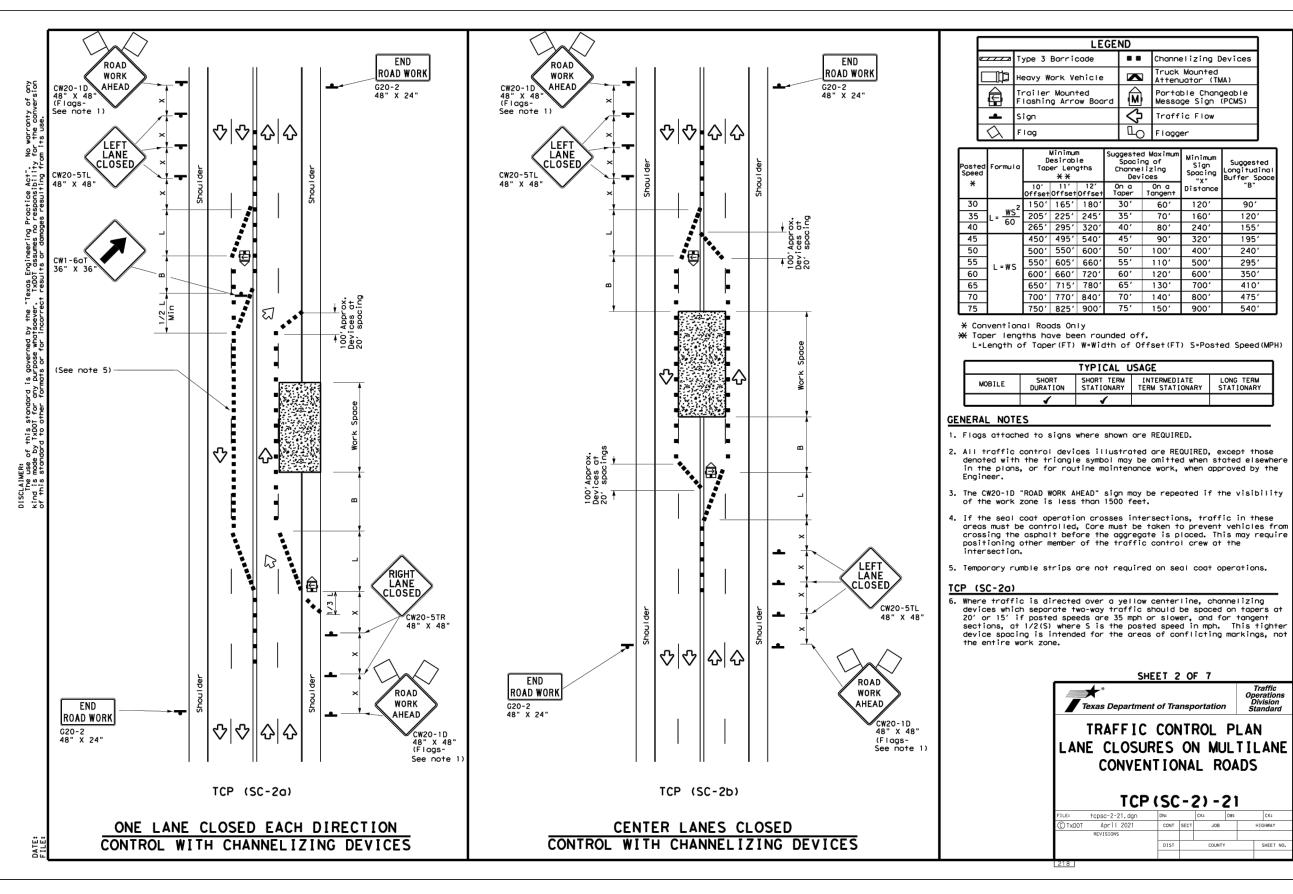
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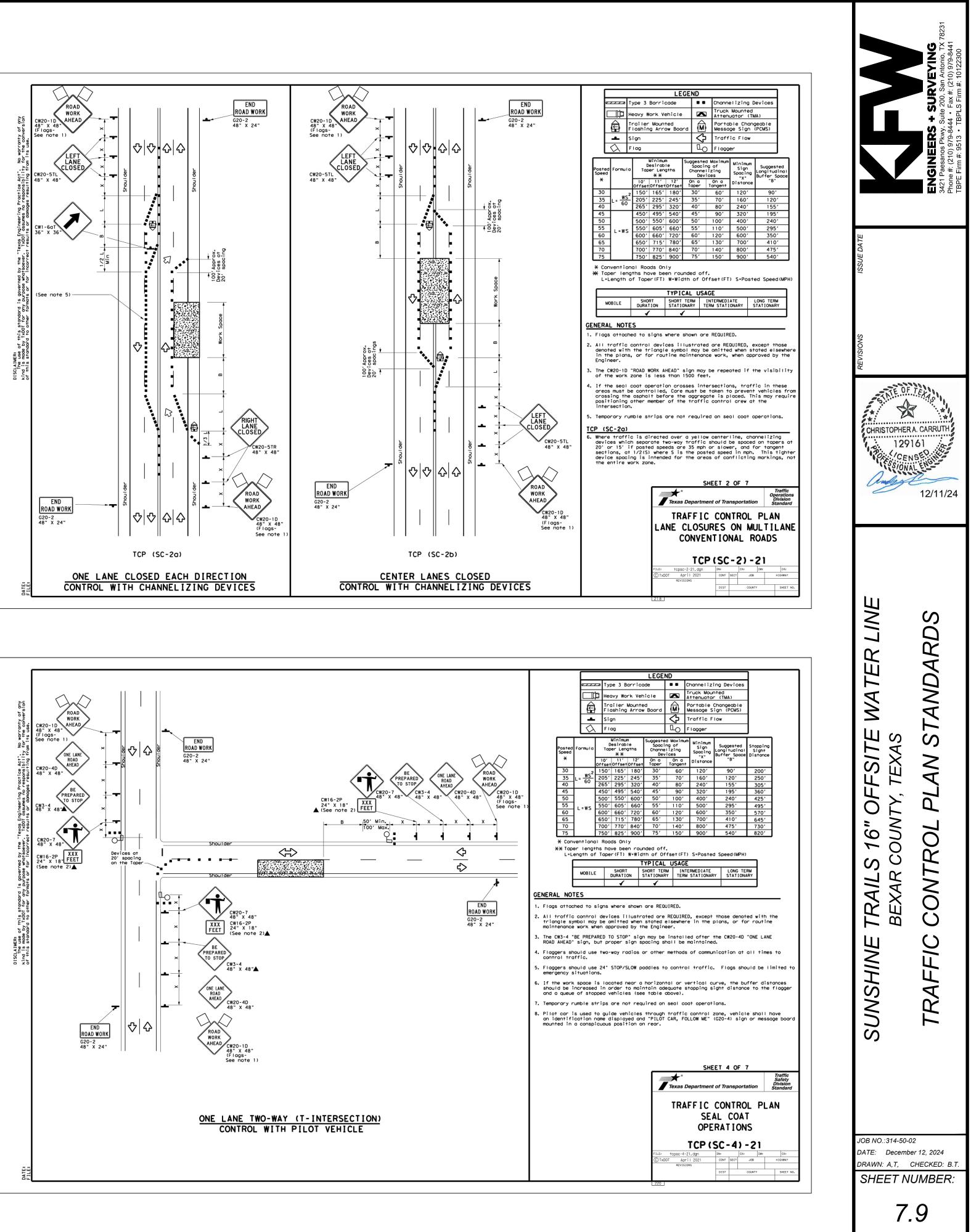


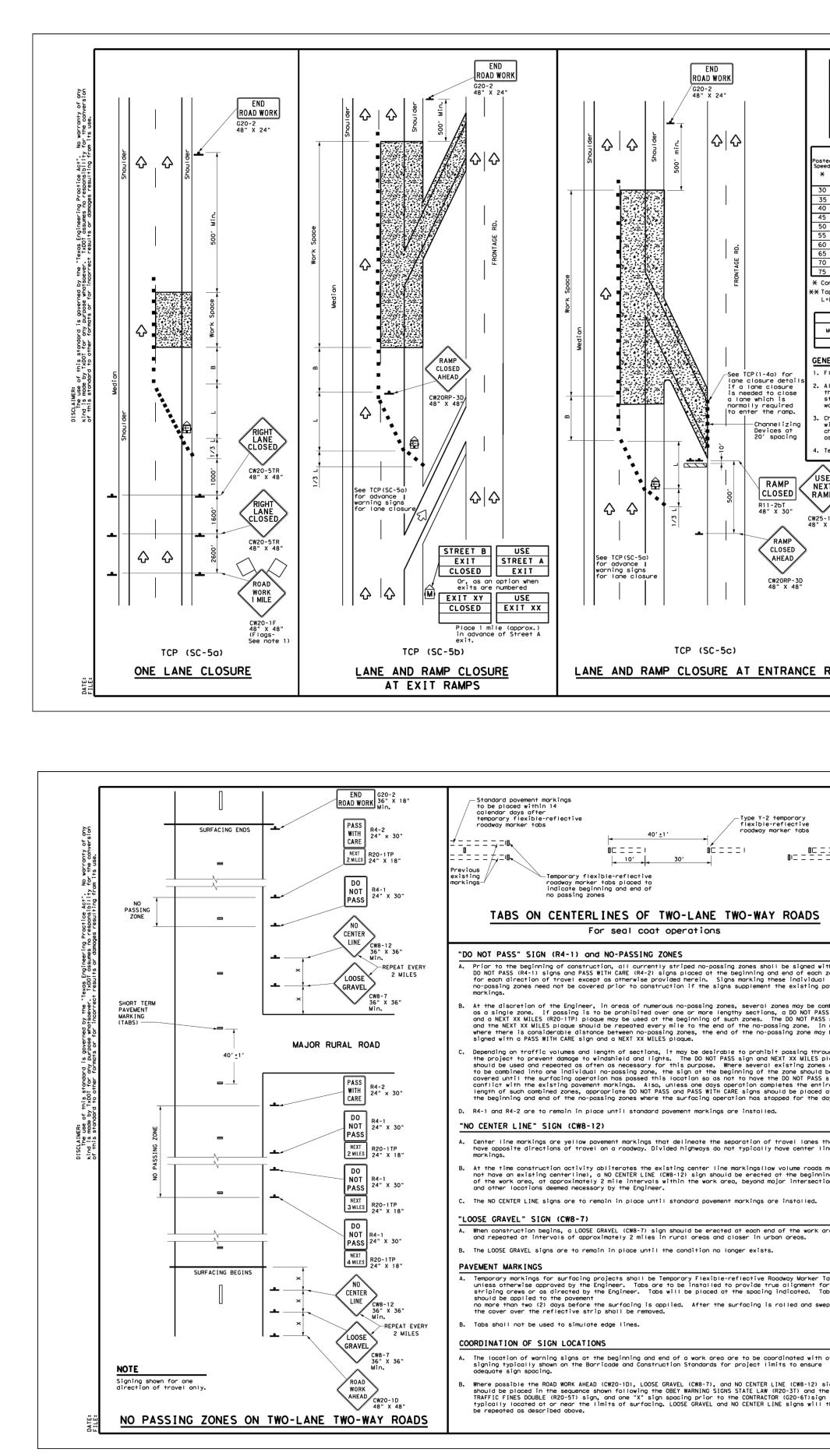












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THIS DOCUMENT HAS BEEN PRODUCED FROM MATERIAL THAT WAS STORED AND/OR TRANSMITTED ELECTRONICALLY AND MAY HAVE BEEN INADVERTENTLY ALTERED. RELY ONLY ON FINAL HARDCOPY MATERIALS BEARING THE CONSULTANT'S ORIGINAL SIGNATURE AND SEAL.